

# Renaissance

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## SPECIFIC PLAN



RENAISSANCE SPECIFIC PLAN  
NOVEMBER 23, 2010

CITY OF RIALTO

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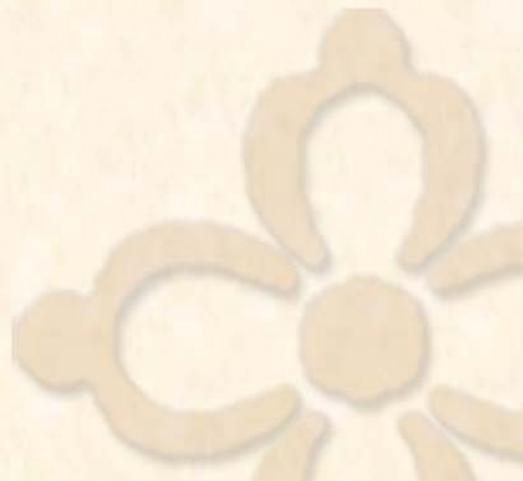
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# Introduction

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## SECTION ONE



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## SECTION 1: INTRODUCTION

### Project Summary

Renaissance is designed as a master-planned community on 1,439 acres that will contain a variety of logistics, employment, business, shopping, as well as residential, and community facilities that are integrated by open spaces and amenities.

The Renaissance Specific Plan serves as the long-range plan for the development of Renaissance and the guide to all future development proposals within the Specific Plan area. This Specific Plan provides detailed text and exhibits describing the various land uses, amenities, and infrastructure improvements envisioned for the community. This document will guide the build-out of the property in a manner consistent with City and state policies and standards to assure that the various community elements identified in the Specific Plan will be developed in a coordinated manner.

#### *Project Location*

As shown in Figure 1-1, *Regional Location*, Renaissance is situated in the western-central portion of the City of Rialto. Rialto lies in the western San Bernardino Valley, 60 miles east of Los Angeles and 103 miles north of San Diego. The City is bordered on the northeast by unincorporated areas of San Bernardino County, Lytle Creek, and the Cities of San Bernardino and Colton; on the west by the City of Fontana and unincorporated San Bernardino; on the south by the City of Colton and unincorporated San Bernardino County.

As shown in Figure 1-2, *Local Vicinity*, the project site is generally bounded on the north by Casmalia Street, on the south by Baseline Road, on the east by Ayala Drive, and on the west by Tamarind Avenue.

Renaissance is ideally situated to take advantage of regional transportation facilities, namely State Route 210 (SR-210), which runs through the northern portion of the project site. Primary access to Renaissance will be from SR-210 interchanges at Alder and Ayala Drives. Baseline Road, Miro Way, Walnut Street, Easton Street, and Casmalia Street provide the main east–west access within the site. Alder, Laurel, Locust, Maple, and Linden Avenues, and Ayala Drive provide the main north–south access.

### **Purpose of the Specific Plan**

As provided for in state law, a specific plan is a regulatory document that provides standards and criteria for the development of a particular geographic area. California Government Code, Sections 65450 through 65454, establishes the authority to adopt a specific plan, identifies the required contents of a specific plan, and mandates consistency with the general plan. In addition to the state requirements, the City of Rialto Municipal Code, Chapter 18.78 Specific Plans, describes the purpose, requirements, regulations, and procedures for preparation of a specific plan in the City.

The Renaissance Specific Plan establishes the planning concept, design and development guidelines, administrative procedures, and implementation measures necessary to achieve the orderly and compatible development of Renaissance. It is also intended to maintain consistency with and carry out the goals, objectives, and policies of the City of Rialto General Plan.

### ***Relationship to Other Plans***

#### ***General Plan***

The California Government Code states that a “specific plan shall include a statement of the relationship of the Specific Plan to the General Plan, and further, that it may not be adopted or amended unless found to be consistent with the General Plan.” In addition, according to Section 18.78.070 of the Municipal Code, all specific plans must be in conformance with the various elements, goals, objectives, and policies of the City’s General Plan, addressed in Appendix B of this document.

As a part of the adoption of the Renaissance Specific Plan, the General Plan map and text will be amended to include the Specific Plan Overlay land use designation. The Specific Plan overlay is a prefix that identifies the existence of a specific plan and allows the base land use designation to closely conform to the existing General Plan land use categories, yet accommodates differences in density and land use types. The overlay allows the land uses to be tailored to reflect the unique characteristics of the site and expand upon the City’s General Plan land use categories.

Future subdivisions, building permits, and public works projects must be consistent with the Specific Plan (Government Code Sections 65455, 66473.5, 65860, and 65401). All projects that are found to be consistent with this Specific Plan will likewise be deemed consistent with the City’s General Plan.

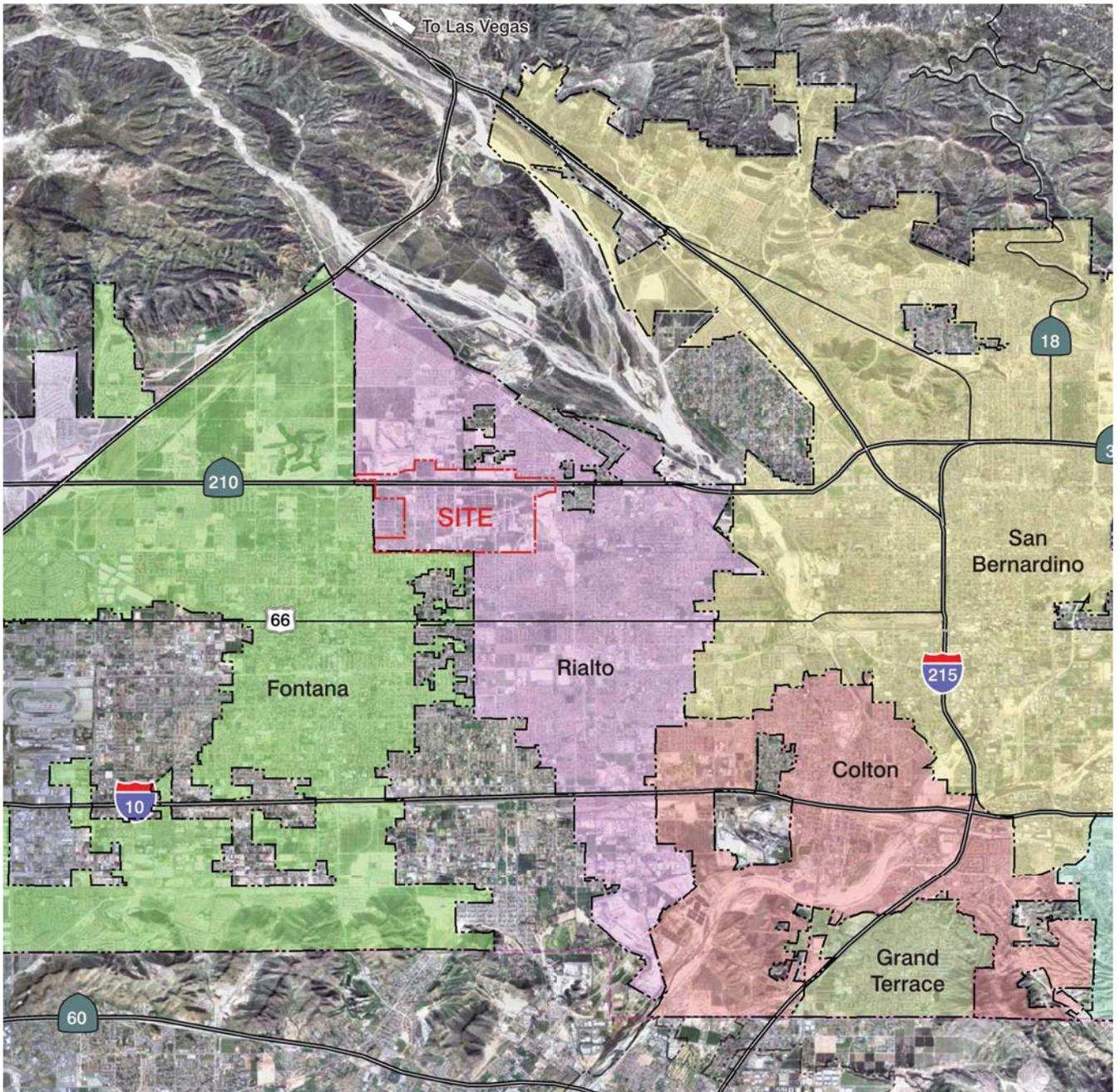
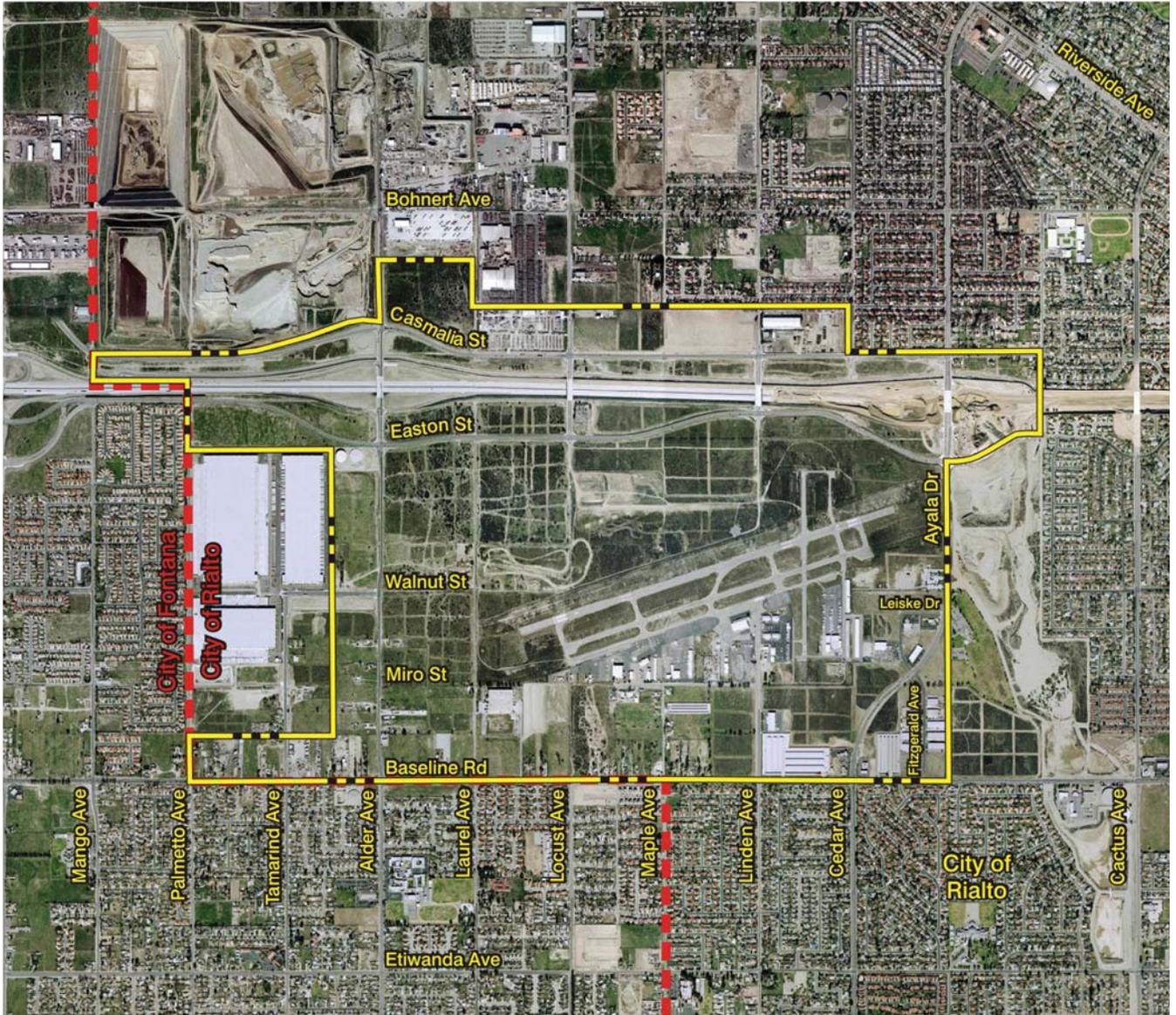


Figure 1-1 Regional Location

## Introduction

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--- Site Boundary



Figure 1-2 Local Vicinity

## Introduction

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### *Municipal Code*

The Renaissance Specific Plan works in conjunction with the City of Rialto Municipal Code. The Renaissance Specific Plan provides the detailed development standards, design guidelines, and development parameters unique to Renaissance; however, unless specifically stated in the Specific Plan, both the provisions of the City of Rialto Municipal Code and Vision and Guiding Objectives of this Specific Plan shall be used to guide decisions.

### *Rialto Airport Specific Plan*

Prior to the adoption of the Renaissance Specific Plan, Renaissance was located entirely within the existing Rialto Airport Specific Plan. The Rialto Airport Specific Plan was adopted in 1997, encompassed 3,100 acres, and was intended to provide a long-term strategy for the development of the airport and surrounding area.

However, at the December 6, 2005, City Council meeting, the City adopted resolutions declaring the intent to relocate the Rialto Airport operations to other nearby facilities. With the closure of the Rialto Airport, major assumptions and portions of the Rialto Airport Specific Plan became obsolete. Through the Renaissance Specific Plan process, the text and graphics of the Rialto Airport Specific Plan will be amended to omit the 1,439 acres comprising Renaissance (see Appendix E). The remainder of the Rialto Airport Specific Plan continues to regulate the areas outside of the Renaissance Specific Plan.

### *Zoning Map*

Through the Renaissance Specific Plan process, the zoning map will be amended to reflect the land use categories contained in the Renaissance Specific Plan, loss of the Rialto Airport, and the changes resulting from the adoption of the Renaissance Rialto Specific Plan. In addition, the Renaissance Rialto Specific Plan zoning classification will be added and reflected on the zoning map.

### *Relationship to the EIR*

The purpose of the California Environmental Quality Act (CEQA) is to inform decision makers, staff, and the public about the potential environmental impacts of development. Compliance with CEQA requires that a project be evaluated for potential impacts before being approved. Further, the state adopted a policy “that the public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available that would substantially lessen the significant environmental effects of such projects.”

Adoption of a specific plan is a project subject to CEQA. As such, an environmental impact report (EIR) has been prepared to analyze and disclose the significant environmental impacts of the Renaissance Specific Plan. The EIR is incorporated into this Specific Plan by reference and is attached separately.

# Format of the Document

The Renaissance Specific Plan is divided into the following sections:

*Section 1: Introduction*—This section provides the definition of and authority to prepare a specific plan, the format of the document, and a description of the location, opportunities, and constraints of Renaissance.

*Section 2: Renaissance*—This section describes the vision and objectives of the project, the development concept, the land use plan, and build-out statistics.

*Section 3: Development Criteria*—This section provides the detailed descriptions of the land use plan, land use categories, development standards, statistical data, circulation plan, open space plan, utility and infrastructure plans, and landscape plant palette.

*Section 4: Design Guidelines*—This section directs the type, style, and design of development in Renaissance.

*Section 5: Signage Guidelines*—This section addresses the physical development of signage in Renaissance.

*Section 6: Sustainability*—This section describes opportunities and guidelines for environmentally sustainable development within Renaissance.

*Section 7: Implementation*—This section contains the provisions for development processing, the development administration system, and the phasing program for the Renaissance Specific Plan.

*Appendices*—The Appendices contain the definitions, general plan consistency analysis, development tracking system, and amended land use plans for the Rialto Airport Specific Plan. The water, drainage, and sewer master plans and environmental impact report are contained under separate covers.

## Terminology

Numerous statements occur in this plan in the form of policies, standards, and guidelines that define action expectations to successfully implement the plan. The following terms clarify the level of commitment intended in the plan and reflect the expectation or desired outcome. The use of each term in a particular policy or action is a deliberate application of these definitions.

*Shall*—This type of policy will always be followed. Shall represents an absolute commitment to the guidance expressed in the policy. (Similar action words: require, enforce, must, ensure)

**Should**—This type of policy will be followed in most cases; exceptions or degrees of implementation are acceptable with valid reasons. When reviewing proposed projects for compliance with this Specific Plan, City Staff will evaluate the applicability of these types of policies and provide direction/recommendations through the appropriate application process. (Similar action words: may)

**Allow**—Permit someone else’s initiative and support it unless there is a very good reason not to. When reviewing proposed projects for compliance with this Specific Plan, City Staff will evaluate the applicability of these types of policies and provide direction/recommendations through the appropriate application process. (Similar action words: permit)

**Restrict**—This type of policy sets specified limits within which action and/or implementation will occur. When reviewing proposed projects for compliance with this Specific Plan, City Staff will evaluate the applicability of these types of policies and provide direction/recommendations through the appropriate application process. (Similar action words: control, limit, contain)

**Prohibit**—This type of policy requires steps to actively prevent a specified condition or decision from occurring. (Similar action words: forbid, ban)

Other terminology may appear in certain policy statements. These terms are to be interpreted according to their similarity to the appropriate term described above.

## Opportunities and Constraints

The following is a summary of the significant opportunities and constraints that were present at the time of the writing of this document.

### **Location**

Renaissance is strategically located in Rialto on regionally significant transportation corridors including SR-210, Baseline Road, and Alder and Ayala Drives. Given its location, Renaissance acts as a gateway into Rialto and should be designed to be identifiable by those traveling on the freeway.

### **Employment**

Given its location, Renaissance represents a significant opportunity to attract business and employment uses that can diversify the City’s employment base and improve its jobs to housing balance.

### **Rialto Goals**

Renaissance is a large, semi-vacant site with enormous development potential. Renaissance represents Rialto’s most significant opportunity to gain regional

## Introduction

exposure and improve its employment base. The City has long realized this fact and has made Renaissance a priority in their long-term planning. Accordingly, the City of Rialto has played the key role in laying the foundation for the acquisition and development of the site.

### *Master Plan*

Because of its size and location, Renaissance will be master planned in an efficient and creative manner to optimize its opportunities and minimize its constraints. Accordingly, instead of developing in a piece-meal fashion, Renaissance can be woven together by a system of mobility and aesthetic treatments that make it unique and identifiable. In addition, Renaissance can be designed with amenities in ideal locations so it is a destination and a desirable place to live and work.

### *SR-210*

SR-210 is the most significant gateway into Renaissance, as well as Rialto. It provides regional visibility and immediate access from Alder and Ayala Drives. Renaissance can benefit from the proximity to and visibility provided by SR-210.

SR-210 is located approximately 30 feet below grade through Renaissance, which provides a noise buffer and reduces the freeway's visual impact yet maintains visual exposure for businesses.

### *School*

Some constraints dictate the location of schools.

- ❖ A gas pipeline runs along Linden Avenue. The gas pipeline may be abandoned and reused. If the gas line remains in place, it will necessitate a separation from the school buildings as required by the California Department of Education.
- ❖ A setback of 1,500 feet from freeways to schools is recommended; however, this setback can be encroached upon with mitigating factors such as sound attenuation, berms, and double-paned windows.
- ❖ Renaissance falls within the Fontana and Rialto school districts. The Rialto school district is generally located north of Walnut Street and the Fontana school district is generally located south of Walnut Street. Residential is not planned within the Fontana School District.

### *Noise*

The site is located between major noise sources. Vehicular noise from SR-210, Baseline Road, and Ayala and Alder Avenues are critical factors to consider in the location and/or buffering of noise-sensitive uses such as residences.

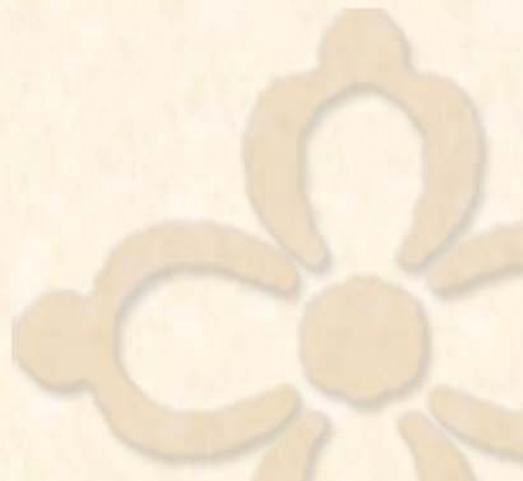
### *Existing Uses*

There are approximately 103 acres and 835,000 square feet of existing uses within Renaissance that are expected to remain. These uses include a fire station, electrical sub-station, water reservoir, self-storage facilities, and several industrial and commercial uses along Baseline Road, Casmalia Street, and Ayala Drive. Renaissance should be designed to respect and incorporate these uses while minimizing potential impacts to future and existing uses.

## Introduction

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❖ Renaissance ❖  
SECTION TWO



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## SECTION 2: RENAISSANCE

This section describes the vision and development concept for Renaissance.

### Vision and Guiding Objectives

#### *Vision*

Renaissance will be an integrated community of various housing types, closely linked to employment, retail, recreation, services, and schools that offers:

**Variety**—Renaissance will be a major node for shopping, services and employment, and provide a range of housing options all in close proximity.

**Flexible Business Options**—Renaissance will offer a unique blend of professional offices, light industrial, research and development, business park, and retail commercial uses, which take advantage of the adjacent SR-210 and complement the existing uses, to help build a robust, employment-based economy in Rialto.

**A Range of Lifestyle Choices**—Renaissance accommodates a mix of residential housing types that serve a range of lifestyles, including first-time buyers, young singles and couples, families, empty-nesters, and seniors. Accordingly, Renaissance accommodates residential choices ranging from detached residential homes, small-lot detached homes, contemporary townhouses, and condominiums.

**Community**—Renaissance will be focused on the community. Renaissance offers open spaces, paseos, walkable streets, proximity of uses, strategically located recreational facilities, and a desirable package of amenities, so residents and visitors will be encouraged to enjoy the outdoors. In this manner, Renaissance will facilitate walk-to-work/shop opportunities and engender a sense of community and identity.

#### *Guiding Objectives*

To achieve the vision, the following guiding objectives are fundamental to Renaissance.

**Quality of Life**—Renaissance will include an array of services and amenities such as employment and retail opportunities, a school, distinctive neighborhood identity, community facilities, and active and passive parks. Community interaction will be promoted to fulfill a high quality of life for employees, residents, and visitors.

**Employment Opportunities**—Renaissance will enhance the City’s existing job base through the creation of a broad range of employment and career opportunities.



Renaissance accommodates a mixture of retail, entertainment, office, R&D, medical, light industrial, light manufacturing, assembly, distribution, and manufacturing uses in a professional setting.

# Renaissance



*Renaissance provides an array of community amenities, such as neighborhood parks, which add to the identity of the place and enhance the quality of life for residents and visitors.*

***Balance***—Renaissance will provide a mix of commercial, business, educational, recreational, and residential uses that will enable local residents to live, play, work, and shop in one community.

***Community Design***—Renaissance will have clear community and neighborhood identity created by design and architectural treatments and rich landscaping patterns, streetscape, signage, and amenities.

***Recreation***—Renaissance will provide areas for active and passive recreation that are accessible by an integrated pedestrian system.

***Economic Viability***—Renaissance will accommodate a variety of land uses that will provide a range of lifestyle and employment options to meet the needs of the economy. The Specific Plan standards will allow flexibility to best meet market demand at the time of development.

***Housing Opportunities***—Renaissance will accommodate a wide variety of housing types, densities, and designs that provide living opportunities for a broad range of income levels and lifestyles and responds to both local and regional housing demands.

***New District***—Renaissance will be a new, dynamic district within Rialto that will enhance the City’s visibility, housing stock, and employment base.

## Development Plan

As conceptually shown in Figure 2-1, Renaissance is an integrated community of various housing types closely linked to employment, retail, recreation, services, and schools. Renaissance accommodates 16.2 million square feet of business and commercial uses (835,200 square feet of which is existing and expected to remain), 1,667 residential units, one school, one community park, and multiple neighborhood parks all located in close proximity.

Renaissance provides rich employment and shopping opportunities and accommodates approximately eight jobs for every housing unit within the Specific Plan. Shopping and employment are located along SR-210 and major roadways, such as Baseline Road, Alder Avenue, Renaissance Parkway, Ayala Drive, and Casmalia Street. The increased employment opportunities improve the City’s jobs-to-housing balance and the proximity of jobs-to-housing provides walk-to-work/shop opportunities. Residential units are situated within a quarter mile of shopping, employment, educational, and recreational uses to facilitate walking, bicycle riding, offer the opportunity to live and work in the community, reduce vehicular trips, and contribute to an improved level of health and wellness.

The array of housing types envisioned within the residential enclave accommodates a range of lifestyles. Renaissance accommodates residential products such as single-family detached, small-lot detached residences, duplexes, triplexes, stacked flats, and courtyard clusters. This multitude of residential products serves a wide segment of the market, from singles, young couples, first-time homebuyers, and move-up buyers. To accommodate the intended array of housing types, the allowable residential density in Renaissance is mixed, ranging from 3 to 30 units per acre.

Renaissance also offers a variety of community amenities, including a clubhouse, neighborhood parks, and a school.

### *Functional Areas*

Renaissance can generally be divided into three functional areas, Freeway Commerce, Community Commerce, and the Village as detailed below.

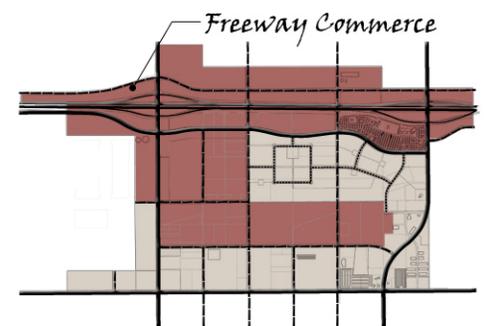
#### *Freeway Commerce*

Generally located along SR-210 and north of Miro Way, the Freeway Commerce area acts as a business-oriented corridor and commercial gateway into Rialto.

Intended to strengthen the jobs-housing balance in the City and be a regional employment and shopping area, the Freeway Commerce area accommodates uses that are compatible with a freeway and benefit from freeway visibility and/or accessibility.



*An assortment of amenities and employment opportunities, such as this conceptual illustration of the corporate offices, are provided within walking distance of the residents of Renaissance.*



# Renaissance



Examples of uses accommodated in the Freeway Commerce area.

The uses along the southern edge of SR-210 typically include retail centers and office complexes. The Town Center, which is located at the southwest intersection of Ayala and SR-210, accommodates an array of commercial uses. It is connected and integrated with the residential community by pedestrian and vehicular linkages as well as proximity, which facilitate walk-to-shop opportunities. The corporate office center is envisioned as an intense office center, reinforcing the business corridor gateway into Rialto, maximizing freeway visibility, and strengthening the surrounding commercial uses. The uses at the intersection of Renaissance Parkway and Alder Avenue are retail and entertainment related. Further south of SR-210, the land uses are focused on employment generating uses such as manufacturing, light industrial, distribution, and warehousing.

The area north of SR-210 accommodates larger buildings such as offices, furniture showrooms, automobile and boat sales, floor and tile showrooms, as well as light industrial and business park uses.

## Community Commerce

Generally located between Baseline Road and Miro Way, the Community Commerce area reinforces the community-oriented businesses along Baseline Road and provides a southern gateway into Rialto.

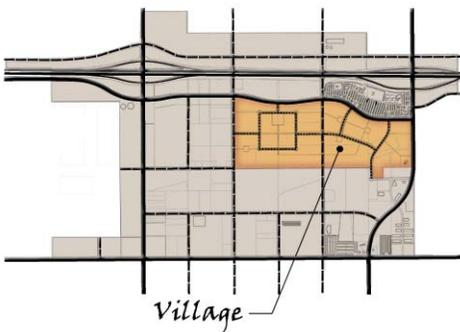
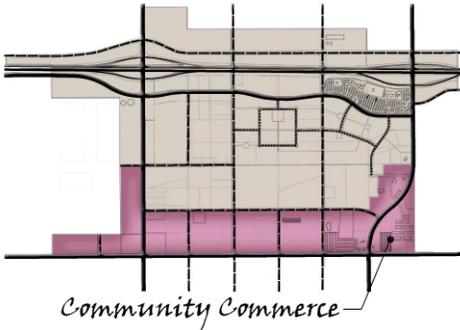
The Community Commerce area is intended to strengthen the jobs-housing balance along Baseline Road and in the City and provide local employment and services. At the major intersections of Baseline Road with Ayala and Alder Avenues, the Community Commerce area accommodates light industrial, professional office, and storage uses as well as retail uses that serve the local community such as convenience retail, and restaurants. There are several existing conforming uses located in this area, mostly near the intersection of Baseline Road and Ayala Drive. These uses are expected to remain and are accommodated within the Specific Plan.

## Village

Located south of Renaissance Parkway between Locust and Ayala Drives, the Village area is the residential enclave consisting of several unique neighborhoods, one school, a community park, and several neighborhood parks.

The Village includes a 15-acre, K-6 school in the Rialto Unified School District. As currently envisioned, the school is located on the western edge of the community; however, the school site may be relocated within the Village depending upon negotiations with the school district and in response to the California Department of Education requirements.

The Village also includes a 16-acre public park. The public park is envisioned to be an active park that may include soccer and baseball fields, basketball courts, turf play areas, and shaded picnic structures. The park is positioned so that it provides access to both residents and employees and acts as a buffer between the businesses and residences.



Within the Village, there are approximately 1,667 residences organized around parks and the private clubhouse, which will be a significant amenity of Renaissance. The clubhouse will provide amenities such as a pool, spa, fireplace and picnic area, a tennis court, children’s wading pool, and meeting rooms.

### ***Build-out Potential***

The land use plan for Renaissance is shown on Figure 2-2, *Land Use Diagram*. Table 2-1 provides a detailed breakdown of the build-out levels of Renaissance. Build-out may be less than what is shown on Table 2-1, but it may not exceed them without prior approval from the City and appropriate environmental analysis.

### ***Land Use Modifications***

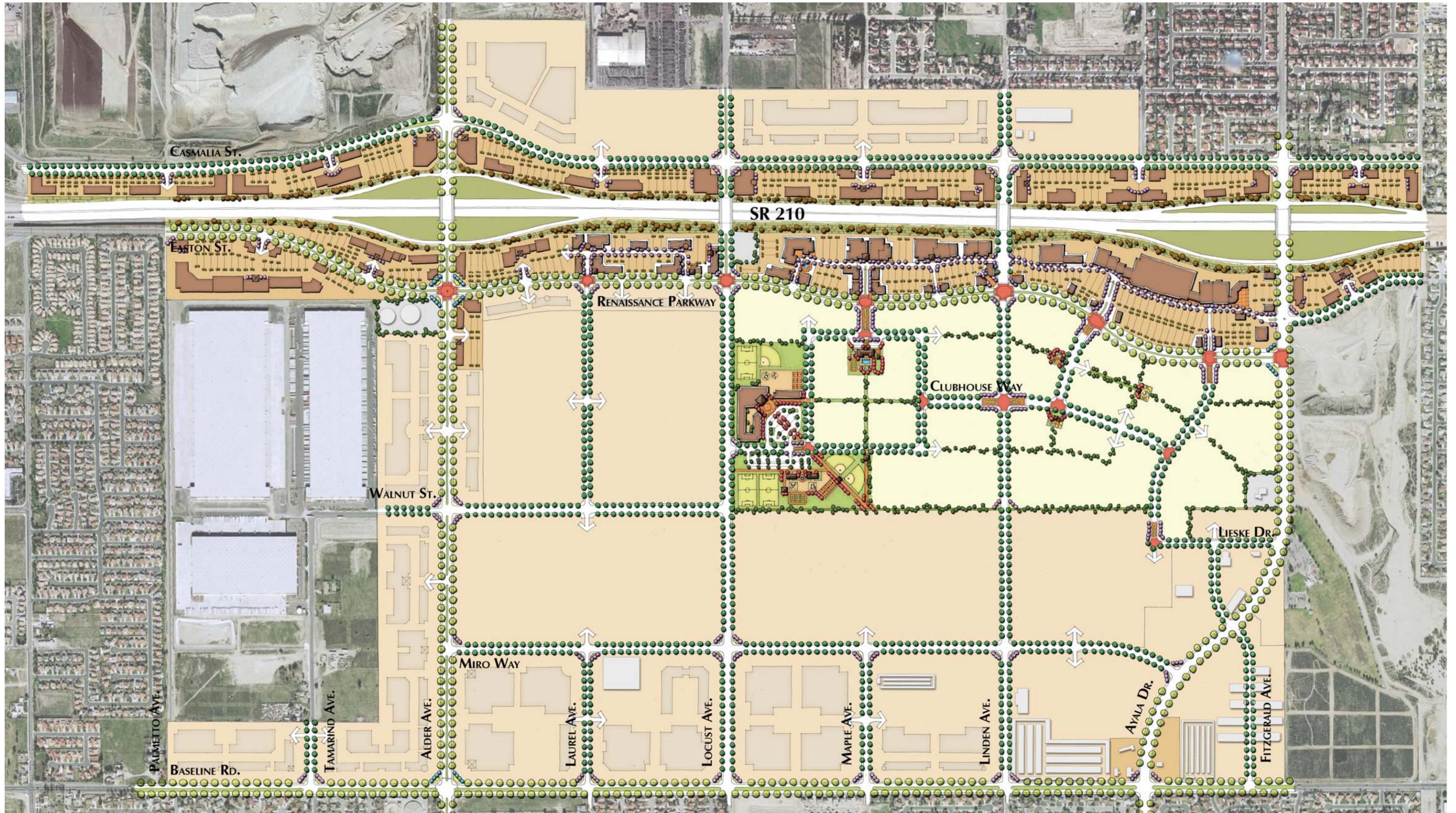
Given that Renaissance will build out over a number of years and under different owners, modifications to the land plan can be expected to respond to changes in roadway alignments, right-of-way widths, physical realities, density transfers, and new product types. In addition, it is not certain that the school district will choose to develop a school within Renaissance or in the location shown on Figure 2-2. In either event, the underlying land use shall revert to the Medium Density Residential (MDR) land use category, which can accept the transfer of residential units from other areas of the Specific Plan. The school may be relocated to any residential land use category within Renaissance. Modifications to the land use plan and specific plan are discussed in Section 7, *Implementation*. Planning Area 60a contains underground utilities within what is currently the Laurel Avenue right-of-way. These utilities may be re-routed or maintained in place as determined in the final design for development in Planning Area 60a.



*Examples of the types of residential uses envisioned in the Village.*

## Renaissance

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NOTE: This drawing is the artist's interpretation for illustrative purposes only. The placement, arrangement, design, and footprints of the structures and amenities depicted are not to be considered the final plan.

Figure 2-1 Illustrative Conceptual Plan

## Renaissance

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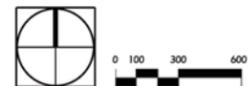
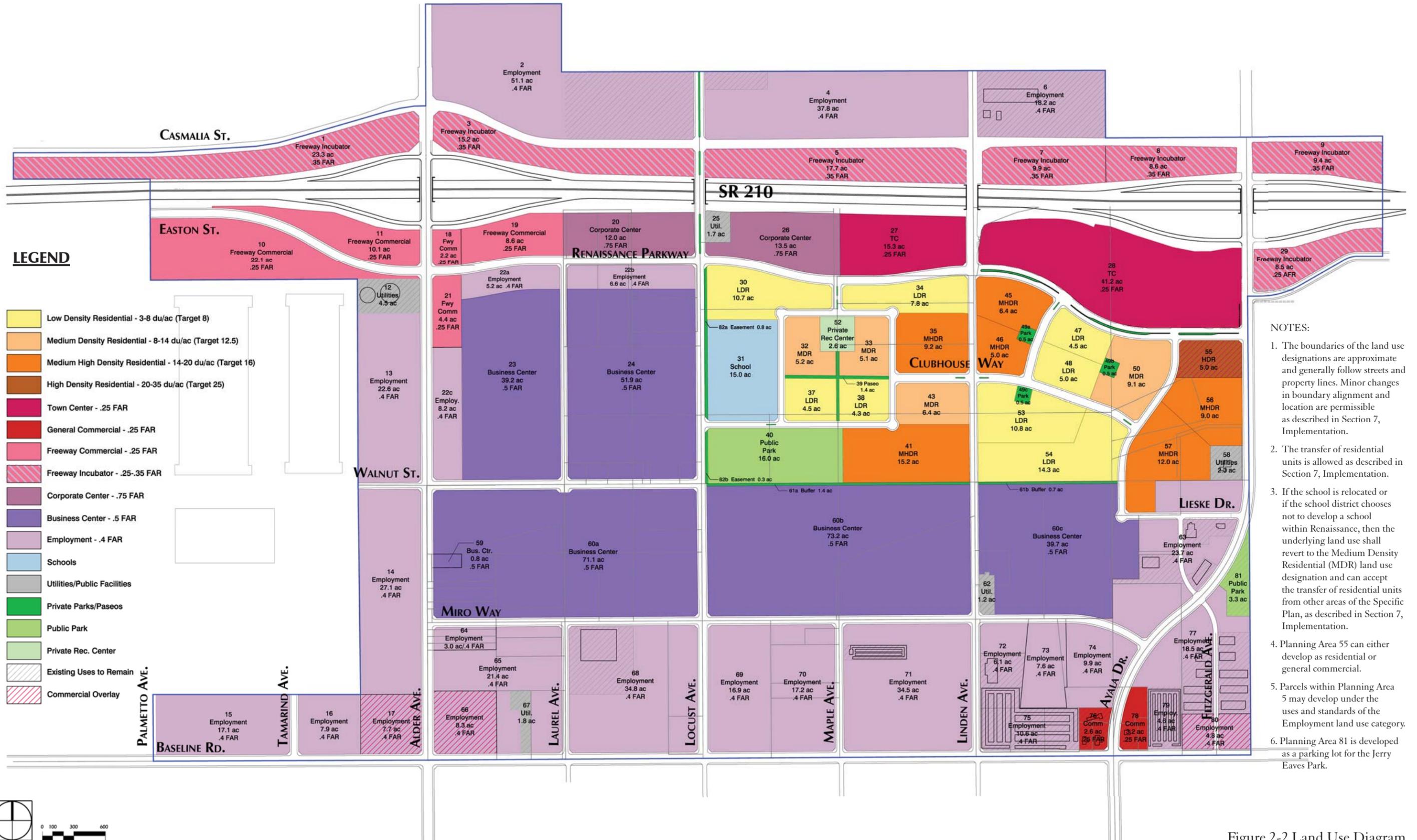


Figure 2-2 Land Use Diagram

# Renaissance

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**Table 2-1  
Statistical Data**

Land Use	Total Acres <sup>2</sup>	Future Uses <sup>1</sup>						Existing Uses Expected to Remain		
		Acres <sup>2</sup>	Target Density/ FAR <sup>3</sup>	Total Sq. Ft.	Total Units <sup>4</sup>	Jobs <sup>5</sup>	Pop. <sup>6</sup>	Acres <sup>2</sup>	Sq. Ft. <sup>7</sup>	Jobs <sup>5</sup>
<b>Residential Uses</b>										
Low Density Residential (LDR)	61.9	61.9	8 du/ac		446		1,382			
Medium Density Residential (MDR)	25.8	25.8	12.5 du/ac		290		900			
Medium High Density Residential (MHDR)	56.8	56.8	16 du/ac		818		2,536			
High Density Residential (HDR) <sup>9</sup>	5.0	5.0	25 du/ac		113		349			
<b>Business Uses</b>										
Town Center (TC)	56.5	56.5	0.25 FAR	615,285		1,231				
Corporate Center	25.5	25.5	0.75 FAR	833,085		1,667				
Business Center	275.9	275.9	0.50 FAR	6,009,102		2,404				
Freeway Commercial	47.4	47.4	0.25 FAR	516,186		1,032				
Employment	431.4	346.3	0.40 FAR	6,033,931		5,853		85.1	820,320	820
Freeway Incubator <sup>8</sup>	92.6	92.6	0.25–0.35 FAR	1,374,754		1,334				
General Commercial <sup>9</sup>	5.8	2.2	0.25 FAR	23,958		48		3.6	14,880	30

# Renaissance

**Table 2-1  
Statistical Data**

Land Use	Total Acres <sup>2</sup>	Future Uses <sup>1</sup>						Existing Uses Expected to Remain		
		Acres <sup>2</sup>	Target Density/ FAR <sup>3</sup>	Total Sq. Ft.	Total Units <sup>4</sup>	Jobs <sup>5</sup>	Pop. <sup>6</sup>	Acres <sup>2</sup>	Sq. Ft. <sup>7</sup>	Jobs <sup>5</sup>
<b>Other Uses</b>										
School <sup>10</sup>	15.0	15.0				50				
Public Parks	19.3	16.0						3.3	NA	NA
Private Recreation Center	2.6	2.6								
Private Parks	1.5	1.5								
Private Pascos	1.4	1.4								
Buffer	2.1	2.1								
Easement	1.1	1.1								
Utilities	11.5							11.5	NA	NA
ROW	299.4	299.4								
<b>Totals</b>	<b>1,438.5</b>	<b>1,335.0</b>		<b>15,406,301</b>	<b>1,667</b>	<b>13,618</b>	<b>5,167</b>	<b>103.5</b>	<b>835,200</b>	<b>850</b>

<sup>1</sup> Does not include *existing* uses that are anticipated to remain unchanged.

<sup>2</sup> Acreage is provided as net, which is everything between the major road rights-of-way (excluding local roads).

<sup>3</sup> Target density/FAR is used to determine buildout of the Specific Plan.

<sup>4</sup> As noted in Section 7, *Implementation*, residential units may be transferred within Renaissance. Total units are derived by assuming that 10% of net acres will be devoted to local roads, setbacks, and landscaping.

<sup>5</sup> Jobs Assumptions: (1) 500 square feet per employee in the Town Center, Freeway Commercial, Corporate Center, and General Commercial land use categories; (2) 1,000 square feet per employee in the Employment and Freeway Incubator land use categories (5% of the square footage of these uses is assumed to be in buildings exceeding 100,000 square feet where a 2,500 square feet per employee factor was used); (3) 2,500 square feet per employee in the Business Center land use category; (4) the school is assumed to accommodate 50 employees, and; (5) mini-storage is assumed to accommodate four employees per facility (two facilities).

<sup>6</sup> Based upon City direction, for purposes of this specific plan a factor of 3.1 persons per household is utilized for all units.

<sup>7</sup> Existing square footage has been estimated from the footprint of existing buildings.

<sup>8</sup> See Figure 2-2, *Land Use Diagram*, for the location of the various FARs within the land use category.

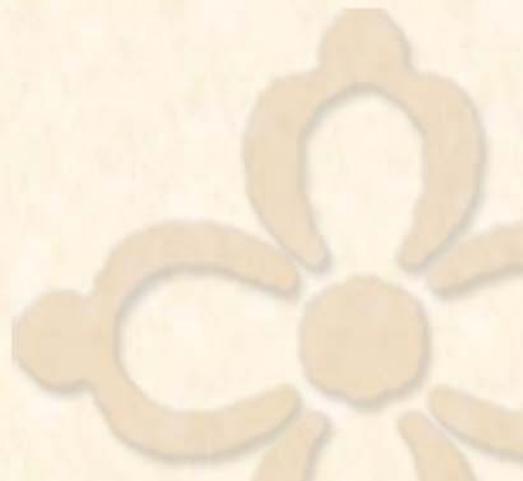
<sup>9</sup> Planning Area 55 may either develop under the High Density Residential or General Commercial land use categories. For purposes of estimating buildout, it has been assumed that this area will develop as a residential use. If it is developed as General Commercial, the units may be transferred to other residential areas of Renaissance.

<sup>10</sup> It is not certain that the school district will choose to develop a school within Renaissance or in the location shown on Figure 2-2. In either event, the underlying land use shall revert to the Medium Density Residential (MDR) land use category, which can accept the transfer of residential units from other areas of the Specific Plan. The school may be relocated to any residential land use category within Renaissance. Modifications to the land use plan and specific plan are discussed in Section 7, *Implementation*.

# Development Criteria

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## SECTION THREE



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## SECTION 3: DEVELOPMENT CRITERIA

This section includes land use categories, permitted uses, and development standards that are intended to shape the physical form of Renaissance. In addition, it includes the mobility plan, parks and open space plan, preliminary grading plan, and infrastructure plans.

### Land Use Categories and Permitted Uses

The Renaissance Specific Plan contains 17 different land use categories, including 4 residential categories, and 7 business categories. Table 3-1, *Land Use Categories*, provides a description of each land use category.

Table 3-2, *Permitted Uses*, lists the uses permitted within each land use category. For each land use category, uses are allowed by right (permitted), conditionally upon the approval of a conditional development permit, or prohibited. The inclusion of any uses not expressly listed shall be subject to the discretion of the Director of Development Services and/or Planning Commission using the Vision and Guiding Objectives of this Specific Plan as a guide.

**Table 3-1  
Land Use Categories**

Land Use Category	Description of Category
<b>Residential Uses</b>	
Low Density Residential (LDR) <sup>1</sup>	Accommodates a range of low density residential uses at a target density of 8 dwelling units per acre. The residential density within this land use category ranges between 3 and 8 units per acre.
Medium Density Residential (MDR) <sup>1</sup>	Accommodates medium density detached and attached residential uses at a target density of 12.5 dwelling units per acre. The residential density within this land use category ranges between 8 and 14 units per acre.
Medium High Density Residential (MHDR) <sup>1</sup>	Provides for high density multiple-family residential uses at a target density of 16 dwelling units per acre. The residential density within this land use category ranges between 14 and 20 units per acre.
High Density Residential (HDR) <sup>1</sup>	Provides for high density multiple-family residential uses at a target density of 25 dwelling unit per acre. The residential density within this land use category ranges between 20 and 30 units per acre. Planning Area 55, which is designated as HDR on Figure 2-2, may also be developed as General Commercial. In this event, the units may be transferred to other areas of Renaissance as detailed in Section 7, <i>Implementation</i> .
<b>Retail/Office Uses</b>	
Town Center (TC)	Accommodates an intense mixture of retail shopping, restaurants, entertainment, and regional shopping in a pedestrian-friendly environment. This land use category also accommodates uses

# Development Criteria

**Table 3-1  
Land Use Categories**

<b>Land Use Category</b>	<b>Description of Category</b>
	such as day-care centers, performing arts centers, and civic uses.
Corporate Center (CC)	Accommodates professional office, research and development, and medical uses.
Freeway Commercial (FC)	Accommodates retail uses that serve the community and the traveling public. The Freeway Commercial land use category accommodates retail, travel services, restaurants, lodging, drug stores, home improvement centers, entertainment centers, health clubs, offices, medical offices, and regional shopping centers. In addition, Planning Areas 10 and 11 accommodate car dealerships.
General Commercial (GC)	Provides for convenience retail and service uses such as gas stations, drug stores, car washes, medical offices, and restaurants that serve the local community.
<b>Business Uses</b>	
Employment (EMP) <sup>2,3</sup>	Accommodates a mixture of professional office, light industrial, research and development, business park, light manufacturing, assembly, and related storage and support services uses.
Business Center (BC)	Accommodates larger industrial, distribution, manufacturing uses.
Freeway Incubator (FI) <sup>2,3</sup>	Accommodates larger retail and business uses that serve the region, such as furniture showrooms, automobile and boat sales, lodging, travel services, professional office, floor and tile showrooms, and furniture or appliance outlets. Planning Area 5 accommodates uses in the Employment land use category as long as the FAR limits are maintained and Planning Area 7 accommodates those uses listed in the Corporate Center land use category.
<b>Other Uses</b>	
School (S)	Accommodates a public school. In the event that the school is relocated or not developed, then the underlying zoning shall revert to the Medium Density Residential (MDR) land use category, which can accept the transfer of residential units from other residential areas as long as the maximum of 1,667 units is not be exceeded. Schools are permitted in any residential land use category.
Public Parks (PP)	Accommodates a range of public open space opportunities such as tot lots, sports courts and fields, picnic areas, and joggers' exercise courses and drainage systems.
Private Recreation Center (PRC)	Accommodates the private clubhouse for Renaissance, which may include swimming pools, courts, meeting rooms, a gym, spa, lighted tennis courts, and other recreational and social amenities.
Private Parks/Paseos (PP/P)	Accommodates private recreational facilities, such as tot lots, pools, sports courts, picnic areas, and miscellaneous open spaces such as paseos and landscaped areas. These areas may also accommodate community activities such as parades, holiday displays, and concerts and drainage systems. Private parks may be reclassified as public.
Utilities (U)	Accommodates the existing water tank, electrical substation, and water filtration system.
Commercial Overlay	Allows the underlying district to develop to the uses and standards of the General Commercial land use district.

<sup>1</sup> For examples of potential residential product types found in the residential land use categories, see the Residential Home Types at the end of Section 4, *Design Guidelines*.

<sup>2</sup> These land use categories also accommodate limited dining and service uses such as coffee shops, delis, and copy centers that are less than 1,500 square feet and are intended to service the local area.

<sup>3</sup> Planning Areas 6, 8 and 9 are adjacent to existing residential neighborhoods. Planning Area 6 contains existing industrial uses. Future improvements and uses in these Planning Areas shall be designed and oriented to minimize potential impacts to adjacent residents, as described in Table 3-9.

**Table 3-2  
General Permitted Uses**

**Legend: Permitted (P), Conditional Development Permit (D), Prohibited (X)**

Use <sup>1, 2</sup>	LDR	MDR	MHDR	HDR	TC	CC	FC	GC	EMP <sup>5</sup>	BC <sup>10</sup>	FI <sup>5</sup>	S <sup>4</sup>	PP	PRC	PP/P	U
<b>Residential Uses</b>																
Congregate care, nursing homes, assisted living facilities, and convalescent homes	X	X	D	D	X	X	X	X	X	X	X	X	X	X	X	X
Day child care facility, 6 or fewer children (Private outdoor play area required)	P	P	P	P	X	X	X	X	X	X	X	X	X	X	X	X
Day child care facility, 7 to 12 children	D	D	D	D	X	X	X	X	X	X	X	X	X	X	X	X
Multifamily attached dwellings	X	P	P	P	X	X	X	X	X	X	X	P <sup>4</sup>	X	X	X	X
Single-family detached dwellings	P	P	X	X	X	X	X	X	X	X	X	P <sup>4</sup>	X	X	X	X
<b>Recreational Uses</b>																
clubhouse	P	P	P	P	X	X	X	X	X	X	X	X	P	P	P	X
Open spaces/parks	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Paseos (including bicycles and pedestrian paths)	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Swimming pool/spa	P	P	P	P	P	P	X	X	X	X	X	P	P	P	P	X
<b>Accessory Uses</b>																
Antennae, vertical/satellite dish (for telecommunications tower, see Other Uses below)	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Patio/gazebo	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	X
Commercial recreational vehicle and boat storage (no hookup) <sup>7</sup>	X	X	X	X	X	X	X	X	D	D	X	X	X	X	X	X
<b>Office Uses</b>																
Administrative and professional offices/services (e.g. medical, doctors, physical therapy, chiropractic, financial planners, banks, insurance, real estate, architects)	X	X	X	X	P	P	P	P	P	P	P	X	X	X	X	X
<b>Vehicle Related Uses</b>																
Auto/boat parts sales (new and used)	X	X	X	X	D	X	P	P	P	X	P	X	X	X	X	X
Auto/boat repair, major (e.g., bodywork, engine and drive train, painting)	X	X	X	X	X	X	X	X	D	X	X	X	X	X	X	X
Auto repair, minor (e.g., oil change, tire, tune-ups, brakes)	X	X	X	X	D	X	D	D	P	X	D	X	X	X	X	X
Car, RV, truck, boat sales (new and used)	X	X	X	X	X	X	D	X	D	X	P	X	X	X	X	X
Car washes	X	X	X	X	D	X	D	D	D	X	D	X	X	X	X	X
Service stations	X	X	X	X	D	X	D	D	X	X	D	X	X	X	X	X
Vehicle/boat leasing/rental	X	X	X	X	X	X	X	X	D	X	P	X	X	X	X	X
Stereo installation <sup>6</sup>	X	X	X	X	D	X	D	D	P	X	P	X	X	X	X	X
<b>Industrial Uses</b>																
Assembly <sup>7</sup>	X	X	X	X	X	X	X	X	P	P	P	X	X	X	X	X
Light industrial <sup>7</sup>	X	X	X	X	X	X	X	X	P	P	P	X	X	X	X	X
Light Manufacturing <sup>7</sup>	X	X	X	X	X	X	X	X	P	P	P	X	X	X	X	X
Recycling facilities <sup>3</sup>	X	X	X	X	X	X	X	X	P	P	X	X	X	X	X	X
Transportation/distribution	X	X	X	X	X	X	X	X	P	D	X	X	X	X	X	X
Warehousing <sup>7</sup>	X	X	X	X	X	X	X	X	P	D	P	X	X	X	X	X
Wholesales	X	X	X	X	X	X	X	X	P	P	P	X	X	X	X	X
Welding and related uses <sup>7</sup>	X	X	X	X	X	X	X	X	P	P	P	X	X	X	X	X
<b>Eating, Drinking Establishments</b>																
Catering establishments	X	X	X	X	P	X	X	X	P	X	P	X	X	X	X	X
Restaurants, deli, sandwich shops, donut shops, juice bars, bakeries, coffee shops, cafes (less than 12 fixed seats)	X	X	X	X	P	P	P	P	P	X	P	X	X	X	X	X
Restaurants, sit-down with no drive-through <sup>8</sup>	X	X	X	X	P	P	P	P	P	X	P	X	X	X	X	X
Restaurants, with drive-through <sup>8</sup>	X	X	X	X	D	X	D	D	P	X	D	X	X	X	X	X
<b>Entertainment</b>																
Adult entertainment	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Amusement parks (e.g. water parks, miniature golf)	X	X	X	X	X	X	X	X	X	X	D	X	X	X	X	X
Auditoriums, convention halls, concert halls, movie and concert theaters, performing art venues	X	X	X	X	P	D	P	D	X	X	X	X	X	X	X	X
Bars/lounges	X	X	X	X	D	D	D	D	X	X	D	X	X	X	X	X
Banquet halls	X	X	X	X	X	D	D	D	X	X	D	X	X	X	X	X
Billiards/bowling	X	X	X	X	P	X	P	D	X	X	X	X	X	X	X	X
Commercial sports facilities (e.g., batting cages and indoor golf)	X	X	X	X	D	D	D	X	D	X	D	X	X	X	X	X
Night clubs	X	X	X	X	D	D	D	D	X	X	D	X	X	X	X	X
Indoor recreation (e.g. paint ball, laser-tag, slot car racing, go-cart racing)	X	X	X	X	D	D	D	D	X	X	D	X	X	X	X	X
Video arcades	X	X	X	X	D	X	D	D	X	X	X	X	X	X	X	X
<b>Financial</b>																
Banks, credit unions	X	X	X	X	P	P	P	P	X	X	X	X	X	X	X	X
Finance related offices (e.g. financial planners, stock brokers, financial advisors, insurance, real estate)	X	X	X	X	P	P	P	P	P	P	P	X	X	X	X	X
Check-cashing, pay-day advance, pawn shops	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X

# Development Criteria

**Table 3-2  
General Permitted Uses**

**Legend: Permitted (P), Conditional Development Permit (D), Prohibited (X)**

Use <sup>1, 2</sup>	LDR	MDR	MHDR	HDR	TC	CC	FC	GC	EMP <sup>5</sup>	BC <sup>10</sup>	FI <sup>5</sup>	S <sup>4</sup>	PP	PRC	PP/P	U
<b>Medical/Health</b>																
Acupuncture/acupressure/massage	X	X	X	X	D	X	D	D	X	X	X	X	X	X	X	X
Blood banks	X	X	X	X	X	X	X	X	D	X	X	X	X	X	X	X
Health clinics (walk-in, urgent care, minute care)	X	X	X	X	D	D	D	X	X	X	X	X	X	X	X	X
Hospitals <sup>9</sup>	X	X	X	X	D	D	D	X	X	X	X	X	X	X	X	X
Medical Equipment and Supplies	X	X	X	X	P	X	P	X	P	X	P	X	X	X	X	X
Medical Offices (doctors, dentists, chiropractic, physical therapy)	X	X	X	X	P	P	P	P	P	P	P	X	X	X	X	X
Outpatient surgery	X	X	X	X	X	P	P	X	X	X	D	X	X	X	X	X
<b>Personal Services</b>																
Barber/beauty/nail shops/day spas	X	X	X	X	P	P	P	P	P	X	X	X	X	P	X	X
Dance schools/karate studios	X	X	X	X	P	X	P	P	P	X	P	X	X	P	X	X
Dry cleaners	X	X	X	X	P	P	P	P	P	X	X	X	X	X	X	X
Health/athletic clubs	X	X	X	X	P	P	P	P	X	X	P	X	X	X	X	X
Laundromats (self-serve)	X	X	X	X	X	X	X	P	X	X	X	X	X	X	X	X
Tattoo Parlors/ body-piercing studios	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Tailor/shoe repair	X	X	X	X	P	P	P	P	P	X	P	X	X	X	X	X
<b>Retail Commercial Uses</b>																
Convenience stores/market	X	X	X	X	D	D	D	D	X	X	D	X	X	X	X	X
Drug stores (< 2,000 sq. ft. and no drive-through)	X	X	X	X	P	P	P	P	X	X	P	X	X	X	X	X
Drug stores (> 2,000 sq. ft. and/or with drive-through)	X	X	X	X	P	X	P	D	X	X	P	X	X	X	X	X
Flower/gift shops	X	X	X	X	P	P	P	P	X	X	X	X	X	X	X	X
General merchandise (e.g. clothing, department stores, electronics, art, books, photography, discount stores, music, and supermarkets)	X	X	X	X	P	X	P	P	X	X	X	X	X	X	X	X
Home improvements stores (CDP required for outdoor displays of lumber, garden, and nursery items)	X	X	X	X	P	X	P	X	X	X	P	X	X	X	X	X
Liquor stores	X	X	X	X	D	X	D	D	X	X	X	X	X	X	X	X
Nurseries/garden supplies	X	X	X	X	P	X	P	X	P	P	P	X	X	X	X	X
Office equipment/supplies	X	X	X	X	P	P	P	X	P	X	P	X	X	X	X	X
Secondhand stores/thrift shops/pawn shops	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
<b>Service Commercial Uses</b>																
Cleaning/janitorial	X	X	X	X	X	X	X	X	P	X	P	X	X	X	X	X
Copy centers/postal service centers/blueprinting	X	X	X	X	P	P	P	P	P	X	P	X	X	X	X	X
Equipment rental/sales/service yard (only north of 210 Freeway)	X	X	X	X	X	X	X	X	D	X	D	X	X	X	X	X
Laboratories (e.g., film, medical, dental, R&D, etc.)	X	X	X	X	P	P	P	X	P	X	P	X	X	X	X	X
Lodging <sup>9</sup>	X	X	X	X	D	D	D	X	X	X	D	X	X	X	X	X
Misc. repairs/service, indoor only (service of clocks, jewelry, vacuums, electronic equipment)	X	X	X	X	X	X	P	X	P	P	P	X	X	X	X	X
Publishing/printing plants	X	X	X	X	X	X	X	X	P	P	P	X	X	X	X	X
Veterinary services/grooming (no boarding)	X	X	X	X	P	X	P	X	P	X	P	X	X	X	X	X
<b>Other Uses</b>																
Bakeries (industrial)	X	X	X	X	X	X	X	X	P	X	P	X	X	X	X	X
Cable companies	X	X	X	X	P	X	P	P	P	P	X	X	X	X	X	X
Churches	X	X	X	X	D	X	D	X	D	D	D	X	X	X	X	X
Clubs, lodges, and meeting halls	X	X	X	X	D	X	D	X	D	D	D	X	X	P	X	X
Drainage/detention	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Funeral parlors/mortuaries	X	X	X	X	X	X	X	X	P	X	X	X	X	X	X	X
Libraries <sup>9</sup>	X	X	X	X	D	D	D	X	X	X	X	P	X	X	X	X
Mini-storage <sup>7</sup>	X	X	X	X	X	X	X	X	D	D	X	X	X	X	X	X
Museums <sup>9</sup>	X	X	X	X	D	D	D	X	X	X	X	D	X	X	X	X
Radio/television broadcasting	X	X	X	X	X	X	D	X	D	D	D	X	X	X	X	X
Schools <sup>4</sup>	P	P	P	P	X	X	D	X	X	X	X	P	X	X	X	X
Commercial Day Care	X	X	X	X	D	X	X	X	D	X	D	X	X	X	X	X
Private/public utility facilities	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
R&D facilities	X	X	X	X	X	P	X	X	P	P	X	X	X	X	X	X
Social service uses/centers	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Swap meets	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X	X
Vocational/trade schools	X	X	X	X	D	D	D	X	D	D	D	X	X	X	X	X
Wireless telecommunication facilities	X	X	X	X	D	D	D	D	D	D	D	D	D	D	D	D

<sup>1</sup> Those uses not specifically listed above are subject to a determination by the Director of Development Services as either permitted, permitted subject to a conditional development permit, or prohibited consistent with the land use category and the Specific Plan. Decisions of the Director are appealable to the Planning Commission.

<sup>2</sup> Exceptions: In addition to the uses permitted by the underlying land use category, 1) Planning Areas 10 and 11 accommodate car dealerships; 2) Planning Area 7 accommodates those uses listed in the Corporate Center land use category; 3) Planning Area 5 accommodates uses listed in the Employment land use category; 4) Planning Area 10 accommodates Vehicle Boat Leasing/Rentals with approval of a Conditional Development Permit (D);

<sup>3</sup> In the Employment land use category, light processing recycling facilities are allowed north of SR-210 per Section 18.108 of the Municipal Code.

<sup>4</sup> In the event that the school is relocated or not developed, then the underlying zoning shall revert to the Medium Density Residential (MDR) land use category, which can accept the transfer of residential units from other residential areas as long as the maximum of 1,667 units is not exceeded.

<sup>5</sup> Planning Areas 6, 8 and 9 are adjacent to existing residential neighborhoods. Planning Area 6 contains existing industrial uses. Future improvements and uses in these Planning Areas shall be designed and oriented to minimize potential impacts to adjacent residents as described in Table 3-9.

<sup>6</sup> Allowed in the TC and FC designations only if installation is not sole use of business and installation area does not exceed 10% of total floor area.

<sup>7</sup> Not permitted to front or face onto the freeway or major arterial.

<sup>8</sup> Permitted along major arterials only.

<sup>9</sup> Within the TC designation, this use is only permitted in Planning Area 27.

<sup>10</sup> For proposals within Planning Areas 60b or 60c larger than 500,000 square feet, see the EDC Review Procedure in Section 7, *Implementation*.

## Development Standards

Development standards are provided to regulate development in Renaissance. Standards are grouped into residential, business, and open space/other uses within this Specific Plan. Within each of these three categories, the following subcategories provide the detailed standards for each land use category.

- ❖ *Development standards*, which provide the tailored standards for each land use category and include such provisions as lot size, lot dimension, and building height and setback requirements. Development Standards are provided in Table 3-3 for detached residential, Table 3-4 for attached residential, Table 3-5 for business, and Table 3-8 for open space/other uses.
- ❖ *General development standards*, which include regulations that apply to most, if not all, land use categories. These are provided within the text of residential, business, and open space/other uses.
- ❖ *Development and design variations*, which tailor the base standards to respond to the characteristics of the planning area and adjacent roadway. The development and design variations are found in Table 3-9.
- ❖ *Incentives*, which allow variations in selected business-related development standards in exchange for desirable design features and amenities.

## Residential Development Standards

### *Detached Residential Standards*

Development standards for detached residential products, which are allowed in the LDR and MDR land use categories, are provided in Table 3-3.

### *Attached Residential Standards*

Development standards for attached residential products are provided in Table 3-4.

# Development Criteria

**Table 3-3  
Development Standards – Detached Residential Uses**

Standard	LDR (Standard Detached)	LDR (Clustered Detached)	MDR
<b>Density</b>			
Target density <sup>11</sup>	8 DU/ac	8 DU/ac	12.5 DU/ac
Density range	3-8 DU/ac	3-8 DU/ac	8-14 DU/ac
<b>Building Site Specifications</b>			
Minimum lot size per unit <sup>1</sup>	3,600 square feet	Not applicable	Not applicable
Minimum net project size	3 acres	3 acres	3 acres
Minimum lot width <sup>2,3</sup>	35 feet	None	None
Minimum lot depth	70 feet	None	None
Corner lot streetside lot width	40 feet	None	None
Min. landscape coverage <sup>13</sup>	Not applicable	20%	Not applicable
Maximum lot coverage	60%	60%	70%
<b>Front Setbacks <sup>4</sup></b>			
To habitable structure	10 feet	10 feet	10 feet
To front-entry garage	≤5 or ≥18 feet	≤5 or ≥18 feet	≤5 or ≥18 feet
To side-entry garage	10 feet	10 feet	5 feet
To porch	5 feet	5 feet	5 feet
<b>Side Setbacks from Street <sup>4</sup></b>			
To structure	10 feet	10 feet	10 feet
Projections into setback <sup>5</sup>	4 feet	4 feet	3 feet
<b>Interior Side Setbacks</b>			
To habitable structure <sup>6,12</sup>	0 or 5 feet	0 or 5 feet	0 or 3 feet
To front-entry garage in rear 1/3 of lot	0 feet	0 feet	0 feet
To private drive-entry garage	0 feet	0 feet	0 feet
Projections into setback <sup>5</sup>	1.5 feet	1.5 feet	1.5 feet
<b>Rear Setbacks <sup>4</sup></b>			
To habitable structure (interior lot)	10 feet	10 feet	10 feet
To habitable structure (home-to-private drive) <sup>7</sup>	0 feet	0 feet	0 feet
To private drive-entry garage	0 feet, but either (1) a 28-foot garage-door-to-garage-door separation is required in private drives that serve 8 or fewer units and are less than 150 feet in length, or (2) a 30-foot garage-door-to-garage-door separation is required in private drives that serve 8 or more units or that exceed 150 feet in length.		
To front-entry garage in rear 1/3 of lot	0 feet	0 feet	0 feet
Projections into setback <sup>5,8</sup>	3 feet	3 feet	3 feet
<b>Height</b>			
Maximum height <sup>9</sup>	40 feet (3 stories)	40 feet (3 stories)	40 feet (3 stories)

**Table 3-3  
Development Standards – Detached Residential Uses**

<b>Standard</b>	<b>LDR (Standard Detached)</b>	<b>LDR (Clustered Detached)</b>	<b>MDR</b>
<b>Other</b>			
Tandem parking	Not permitted		
Minimum private open space	Not applicable	250 sq. ft./unit	250 sq. ft./unit
Minimum livable area	1,300 sq. ft.	1,300 sq. ft.	1,000 sq. ft.
Building separation <sup>10</sup>	10 feet	10 feet	6 feet

Notes:

All setbacks shall be measured from the property line or, if applicable, the back of landscape easement unless otherwise noted.

DU/ac = Dwelling units per net acre

<sup>1</sup> Minimum lot sizes do not apply to developments that are subdivided by means of a condominium map.

<sup>2</sup> Any lot with a width of 35 feet or less shall provide garage access from a private drive unless on a knuckle or cul-de-sac. Conventional front-loaded products shall provide a minimum 40 wide lot width.

<sup>3</sup> For knuckle and cul-de-sac lots, the minimum lot width shall be measured at the setback line.

<sup>4</sup> See Table 3-9 for variations to the standards noted on this table.

<sup>5</sup> Projections are architectural features that extend beyond the building face. Projections include features such as eaves, chimneys, bay windows, stairways, and other architectural detailing. UBC requirements take precedence over this requirement.

<sup>6</sup> In the rear one-third of lots that employ a 0-foot setback on one side and a 10-foot setback on the other, the structure can encroach 5 feet into the interior side setback as long as a 15-foot rear building separation is maintained.

<sup>7</sup> Habitable structures (nongarage) may cantilever beyond the garage door to the rear property line.

<sup>8</sup> Applies to habitable structures only. Garage projections are not permitted.

<sup>9</sup> Maximum building height is defined as the height from the top of the finished grade to the top of the roof peak.

<sup>10</sup> Building separation is measured from the exterior wall of a structure to the exterior wall of another structure. The following may encroach into the required separation: 1) architectural projections, bay windows, and eaves-2 ft, 2) covered porches and stoops-3 ft, 3) balconies and stairways-3 ft.

<sup>11</sup> Target density is the density used to determine the buildout of Specific Plan.

<sup>12</sup> A zero lot-line detached product is permissible if the balance of the required setback is provided in the opposite side yard and if the minimum building separation is maintained.

<sup>13</sup> See the landscape provisions in the General Residential Development Standards section and Landscape Plant Palette later in this section for greater detail and definition.

# Development Criteria

**Table 3-4  
Development Standards – Attached Residential Uses**

<b>Standard</b>	<b>MDR</b>	<b>MHDR</b>	<b>HDR</b>
<b>Density</b>			
Target density	12.5 DU/ac	16 DU/ac	25 DU/ac
Density range	8-14 DU/ac	14–20 DU/ac	20–30 DU/ac
<b>Building Site Specifications</b>			
Minimum net project size	3 acres	3 acres	3 acres
Maximum site coverage (building footprint)	65%	70%	80%
Minimum landscape coverage <sup>6</sup>	30%	20%	20%
<b>Setbacks From Exterior Public Street <sup>1</sup></b>			
To habitable structure	10 feet	10 feet	10 feet
To porch	5 feet	5 feet	5 feet
Projections into setback <sup>2</sup>	3 feet	3 feet	3 feet
<b>Building Separation <sup>3</sup></b>			
Front-to-front separation	15 feet average	20 feet average	20 feet average
Side, rear, oblique separation <sup>7</sup>	6 feet	6 feet	10 feet
Private drive	Either: (1) a 28-foot garage-door-to-garage-door separation in private drives that serve 8 or fewer units and are less than 150 feet in length, or (2) a 30-foot garage-door-to-garage-door separation in private drives that serve 8 or more units or that exceed 150 feet in length.		
<b>Height</b>			
Maximum height <sup>4</sup>	40 feet (3 stories)	45 feet (3 stories and loft)	55 feet (4 stories and loft)
<b>Other</b>			
Minimum common open space <sup>5</sup>	Per Table 3-11, Parks and Recreation Facilities Summary		
Minimum livable area	Studio – 550 square feet 1 Bedroom – 700 square feet 2 Bedroom – 900 square feet 3 Bedroom – 1,100 square feet		
Min. private open space per unit <sup>5</sup>	75 square feet per unit Minimum dimension of 6 feet in any direction.		

**Notes:**

All setbacks shall be measured from the property line or, if applicable, the back of landscape easement unless otherwise noted.

DU/ac = Dwelling units per net acre

<sup>1</sup> See Table 3-9 for variations to the standards noted on this table.

<sup>2</sup> Projections are uninhabitable architectural features that extend beyond the building face. Projections include features such as eaves, bay windows, stairways, porches, and other architectural detailing.

<sup>3</sup> Building separation is measured from the exterior wall of a structure to the exterior wall of another structure.

<sup>4</sup> Maximum building height is defined as the height from finished grade to the top of the roof peak, whichever is greater.

<sup>5</sup> See the open space provisions in the General Residential Development Standards section for greater detail and definition.

<sup>6</sup> See the landscape provisions in the General Residential Development Standards section and Landscape Plant Palette at the end of this Section for greater detail and definition.

<sup>7</sup> For buildings with less than a 10 foot separation, widows on adjacent buildings should be offset, eliminated, clearstory, or opaque to ensure privacy.

### ***General Residential Development Standards***

The following General Development Standards apply to all residential uses within Renaissance.

#### ***Product Variation***

Renaissance will be attractive and visually interesting. Accordingly, residential neighborhoods will include a variety of product types and design styles.

- ❖ Developments over 50 units should have a minimum of three different elevations and material and color palettes. Developments of less than 50 units should have a minimum of two different elevations and material and color palettes. No two homes with identical elevations and color or materials palettes shall be located adjacent to one another.
- ❖ Developments over 50 units shall include a minimum of three floor plan designs. Developments of less than 50 units shall include a minimum of three floor plan designs. No two homes with identical floor plans shall be located adjacent to one another unless different color palettes are utilized.



*Employ a variety of building materials and colors to add variety to a neighborhood.*

#### ***Garage Variation***

To avoid the monotony of projects that employ the same garage placement (e.g. all front-entry garages), projects in Renaissance are required to include a variety of garage placements and orientations. Standard garage placement is a front-loaded garage set back 18 feet from the front property line. Roll-up garage doors with automatic openers are required for all garages. The following are potential alternative garage placements:

- ❖ Private drive-loaded garages
- ❖ Side-entry garages
- ❖ Split garages
- ❖ Courtyards or driveways providing multiple-unit access
- ❖ Recessed garages (5 feet to 10 feet recessed)
- ❖ Straight-in garages in rear two-thirds of the lot

#### ***Street Access***

Per Section 18.02.100, Street Access, of the Municipal Code.

#### ***Definitions***

Per Appendix A of this Specific Plan.

#### ***Property Maintenance–Nuisances***

Per Chapter 18.72, Property Maintenance – Nuisances, of the Municipal Code.

# Development Criteria

## Location of Accessory Buildings

Accessory buildings shall comply with the setback criteria for the main structure.

## Location of Patio Covers and Patio Enclosures

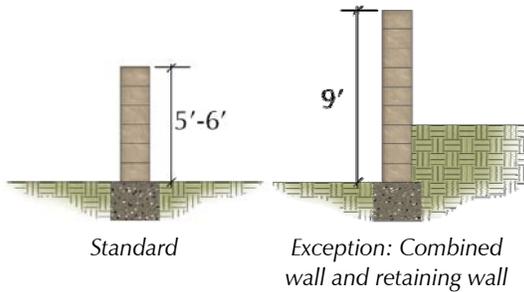
Patio covers and patio enclosures, defined as nonhabitable space in the adopted Uniform Building Code, may be attached to the rear and/or side of a residential structure provided that the minimum setbacks are maintained.

## Cornices and Eave Projections

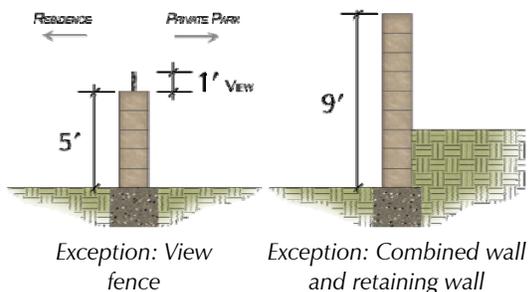
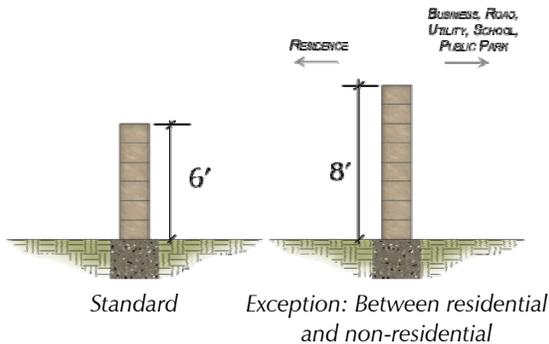
Cornices and eaves may extend into a required yard per the Uniform Building Code.

## Fences and Walls

### Interior Rear and Side Walls



### Perimeter/Exterior Rear and Side Walls



❖ The height of walls and fences shall be measured from the top of the highest adjacent grade unless otherwise noted or if adjacent to a public right-of-way, in which case the measurement shall be taken from the side of the public right-of-way.

❖ Wood, chain link, barbed and razor wire are not permitted. Chain link may be used on a temporary basis at construction sites. Vinyl coated chain link may be used as a fencing material for outdoor park facilities such as tennis courts

❖ Interior rear and side yards. Solid masonry walls a minimum of 5 feet in height and a maximum of 6 feet in height shall be required at all interior rear and side property lines with the following exception:

- When a retaining wall is necessary, combined retaining walls and walls shall be permitted as long as they do not exceed a combined total height of 9 feet as measured from the lowest adjoining grade.

❖ Exterior/perimeter rear and side yards. Solid masonry walls a maximum of 6 feet in height shall be required at exterior/perimeter property lines with the following exceptions:

- A solid masonry wall a maximum of 8 feet in height is permissible when adjacent to any business related use or land use designation, utility, public park, school, or roadway.
- When adjacent to private parks and the private recreation center, a combination of solid masonry wall (minimum 5 feet in height) and view-fencing (maximum 1 foot in height) may be utilized.
- No wall located within an area defined by a corner cutoff line between two points each measured 30 feet horizontally from the point of intersection of the property lines at the street corner may exceed a height of 3 feet.

- For retaining walls located on the perimeter, side, or rear property lines:
  - When a retaining wall is necessary, combined retaining walls and walls shall be permitted as long as they do not exceed a combined total height of 9 feet as measured from the lowest adjoining grade.
  - If visible from the public right-of-way, the maximum height of any solid retaining wall shall be 3 feet as measured from the lowest adjacent grade.
  - If retaining walls are not visible from public areas or if they are visible from public areas and unique designs are incorporated to disguise or break up the mass of the retaining wall (e.g. offsets, landscape walls, unique materials, or public art) the maximum height may be 6 feet as measured from the lowest adjacent grade.
  - A second retaining wall may be erected above the lower retaining wall with a minimum 3 foot landscaped separation. The height provisions governing the lower retaining wall, above, shall apply to the second retaining wall.
- ❖ Front yard. The maximum height of walls and fences located between the front property line and the nearest building wall (either garage or habitable structure) shall be 3.5 feet. Thereafter, the provisions for walls in rear and side yards noted above shall apply.
  - When a retaining wall is required in the front yard:
    - The maximum retaining wall height shall be 2 feet and may be directly topped with a maximum 18 inch solid wall or open fence for a total height of 3.5 feet, or
    - The maximum retaining wall height may be 3 feet and may be directly topped with a maximum 18 inch open fence for a total height of 4.5 feet.

### *Parking and Loading Standards*

#### **Minimum Number of Parking Spaces**

- ❖ For detached units in the LDR and MDR land use categories: Two enclosed garage spaces and .5 guest space per unit. Guest parking may be provided in off-street lots or on-street. On-street parking shall be credited toward the guest parking requirement.

# Development Criteria

- ❖ For attached units in the MDR, MHDR, and HDR land use categories, the following shall apply:
  - Studio Unit: 1 space per unit. One space per unit shall be within an enclosed garage
  - One Bedroom Unit: 1.75 space per unit. One space per unit shall be within an enclosed garage
  - Two and Three Bedroom Unit: 2 spaces per unit. Two spaces per unit shall be within an enclosed garage
  - Each additional bedroom: 0.25 space per unit distributed throughout the development
  - Guest Parking: 1 guest parking space for every 3 units. Guest parking may be provided in off-street lots or on-street. On-street parking shall be credited toward the guest parking requirement.
  - Unenclosed Stall Dimensions: the minimum interior dimensions of an unenclosed parking stall (e.g. parking lot, on-street) shall be 9 feet × 20 feet.
  - Enclosed Stall Dimensions: the minimum interior dimensions of an enclosed parking stall (e.g. garage or carport) shall be 10 feet x 20 feet.
  
- ❖ Parks and recreation areas:
  - Clubhouse: 1 space per 5,000 square feet of the park area. On-street parking within 500 feet of the Clubhouse shall be credited toward the guest parking requirement.
  - Public/Private Parks: 1 space per 5,000 square feet of park area. On-street parking within 500 feet of the park shall be credited toward the guest parking requirement.

## Parking Design and Use Provisions

- ❖ General provisions. Per Section 18.58.020, General Provisions, of the Municipal Code.
- ❖ Driveways for single-family detached residential units. The driveway to a garage shall have a minimum width of 10 feet and a maximum grade of 12 percent.
- ❖ Drive aisles to on-site parking areas. Either (1) loop system, where each one-way drive aisle has a minimum width of 20 feet; or (2) drive aisles with a minimum width of 26 feet to permit two-way traffic. For two-way, private drive aisles that are less than 175 feet long and are not necessary for fire access, as approved by the Fire and Engineering Departments, the minimum driveway width may be 22 feet.

- ❖ Along all driveway and vehicular access ways that provide access for fire or emergency vehicles, as determined by the Fire Department, there should be a minimum vertical clearance of 14 feet and horizontal clearance of 20 feet subject to approval by the Fire Department.
- ❖ Handicapped parking. Per Section 18.58.030 (B) of the Municipal Code.
- ❖ Direct vehicular access to individual residential units shall not be taken from arterials.
- ❖ Recreational vehicles (RVs). The parking or storing of recreational vehicles, dismantled campers, camper shells, boats, trailers, or similar recreational items on lawns, landscaped areas, or other unpaved surfaces within the front yard or on streets is prohibited. In attached developments without yards, parking of these recreational items must be in a common storage area or off-site.
- ❖ Senior-citizen housing. Per Section 18.58.060 (C) of the Municipal Code.

### *Transportation Control Measures*

Per Chapter 18.59, Transportation Control Measures, of the Municipal Code.

### *Open Space Provisions*

Open spaces are those areas that are used for private or common use, as defined below. Open space does not include public or private streets, alleys, driveways, parking spaces, parkways, or recreational storage areas.

#### **Private Open Space**

Private open space is defined as those areas that are designed and intended to be used exclusively by the individual homeowner. Private open space includes patios, balconies, fenced private yards, and other private areas. Private open space may include ground-floor patios or courtyards, second-/or third-floor balconies or decks, and rooftop decks. Private open space may be covered, but must be open on at least one side. Refer to Table 3-4 for private open space requirements.

#### **Common Open Space**

Common open space is defined as those areas designated for the use and enjoyment of all residents and developed for recreational or leisure-time activities. These common areas may include game courts, swimming pools, garden grounds, landscaped areas, sauna baths, public trails, tennis courts, basketball courts, volleyball courts, putting greens, play lots, and clubhouse facilities.

It is the express intent of this Specific Plan to focus common open space requirements into the locations depicted on Figures 2-2 and 3-16 to create community gathering places and a sense of identity. Accordingly, private recreation



*Examples of the treatment of common open space envisioned in Renaissance.*

## Development Criteria

facilities and public parks count towards the required common open space requirement noted on Table 3-4.

### *Nonconforming Uses*

Per Chapter 18.60, Nonconforming Uses, of the Municipal Code.

### *Building Bulk*

Comply with the provisions of the Design Guidelines to minimize building bulk.

### *Signs*

Per Section 5, *Signs*, of this Specific Plan.

### *Sustainable Practices*

For guidance on sustainable and green design and building practices, see Section 6, *Sustainability*, of this Specific Plan.

### *Pedestrian Circulation*

A pedestrian circulation system shall be incorporated into the residential development for the purpose of providing direct access to and from all individual dwelling units, trash storage areas, parking areas, recreational areas, and other outdoor living areas. The circulation system shall be developed with a combination of the following standards:

- ❖ A sidewalk system shall be developed adjacent to all streets as shown on the street sections contained in this Specific Plan. Meandering sidewalks may be utilized along those streets with landscaped easements behind the sidewalks.
- ❖ The minimum width of pedestrian walks along the interior, private streets shall be 4 feet. Walkway systems shall utilize materials such as concrete, brick, flagstone or other materials.

### *Utilities*

- ❖ Public utilities shall be placed underground per the requirements of Chapter 15.32 Underground Utilities of the Municipal Code and the utility companies
- ❖ Each dwelling unit shall be provided individual services and utility meters.
- ❖ Existing utility easements may be retained in place, realigned, or rerouted as determined through the tract map approval process.
- ❖ Roof-mounted equipment, such as air conditioning units, in residential zones shall be screened from view from the adjoining public right-of-way.

- ❖ Exposed gutters, downspouts, vents, louvers, and other similar elements shall be painted to match the surface to which they are attached, unless the elements are incorporated as part of the design element of the site.

### *Trash Collection and Recycling Areas*

- ❖ Common trash collection and recycling areas, if required, shall be provided within 250 feet of the furthest unit they are to serve. Trash and recycling receptacles shall be completely screened from public rights-of-way and parking areas through site orientation, screening materials (walls/landscaping), or a combination of both, and shall be situated so as to eliminate noise and visual intrusion and eliminate fire hazards.
- ❖ Refuse and recycling enclosures for common collection bins shall have a minimum interior dimension of 10 feet by 8 feet and be enclosed by 6 foot tall walls with self-latching gates or as required by trash provider/City ordinance.
- ❖ Individual trash and recycling pickup service may be permitted, provided provisions are specified in the conditions, covenants, and restrictions.

### *Private Storage Areas*

For each dwelling unit without an attached garage, a minimum of 75 cubic feet of private storage space shall be provided outside the dwelling. The design, location, and size of the storage space shall be integrated into the development.

### *Antennas*

- ❖ A central television antenna or a connection to a cable television system shall be provided and shall be connected to each dwelling unit by underground/screened wiring.
- ❖ A satellite antenna may be installed on a lot in any zoning district provided that: (1) it is not located in a required front yard; (2) when roof-mounted, the antenna or satellite antenna is located on the rear one-half of the roof, and; (3) the antenna does not exceed 10 feet if installed on the ground or, if roof-mounted, does not exceed the maximum building height for the district in which the satellite antenna is located.
- ❖ Direct Broadcast Satellite (DBS) antennas 1 meter or less in diameter should be installed in locations that are not readily seen from neighboring properties or from the street, such as the rear of the property. Antennas that are greater than 1 meter in diameter are prohibited.
- ❖ Wireless telecommunication facilities are prohibited in any residentially designated area.

# Development Criteria

## *Garage Sales*

Per Chapter 5.69, Garage, Yard or Patio Sales, of the Municipal Code.

## *Landscape Provisions*

- ❖ As noted on Tables 3-3 and 3-4, a minimum percentage of landscaping is required in all residential developments. Landscape coverage shall be calculated from the property lines of detached products and perimeter property lines for clustered or attached projects. Items that may be counted toward the landscape coverage provisions include: ground-floor patios, yards, or courtyards, landscaped parkways, slope areas, gardens, water features, parks, paseos, pedestrian pathways, planter areas, turfed areas, pools, landscape easements, and sports courts. Landscape coverage shall not include streets, driveways, parking spaces, or street-adjacent sidewalks depicted on the street sections.
- ❖ All setback areas fronting on or visible from a public street and recreation/common open space areas shall be landscaped and permanently maintained in an attractive manner. Such landscaping shall primarily consist of turf, lawn, ferns, trees, shrubs, or other living plants.
- ❖ Permanent, 100 percent automatic irrigation facilities shall be provided in all landscaped areas.
- ❖ For guidance on sustainable and green landscape practices, see Section 6, *Sustainability*, of this Specific Plan.

## *Lighting*

A detailed lighting plan, including specifications and design standards, shall be submitted as part of the precise plan of design application. Such lighting shall be directed on the driveways and walkways and away from adjacent property. Walkway lighting shall be low-level fixtures, spaced to provide adequate walkway illumination, and shall not intrude into the residential dwelling units.

## *Detention/Drainage*

Detention and drainage areas shall be permitted in all land use categories as necessary and on a case-by-case basis. When possible, these areas should be designed to blend in with the surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space. On-site detention facilities may be permanent or temporary based upon the capacity, status, and connection to downstream drainage facilities.

## *Home Occupations*

Home occupations include a vocation such as the professional offices of a lawyer, engineer, music teacher, or art teacher, that are carried on solely by the occupant of

the premises. Home occupations are allowed in any residence per Chapter 5.68, Home Occupations, of the Municipal Code provided all of the following provisions are met.

- ❖ There is no alteration in the residential character of the premises
- ❖ All operations are carried on within the dwelling
- ❖ No more than 15 percent of the dwelling is occupied or used
- ❖ No merchandise or articles are displayed for advertising purposes
- ❖ No assistants are employed at the premises
- ❖ The premises are not be used as a point of sale or walk-in trade
- ❖ All operations in connection with the home occupation are conducted so as to prevent the emanation of any dust, gas, smoke, noise, fumes, odors, vibrations, or electrical disturbances

### *Private Drives (alleys)*

- ❖ Private drives (alleys), shall include landscaping to soften their appearance and lighting to provide security as follows:
  - A minimum of 16” wide and 24” deep landscape planter shall be provided between garage doors. The planter shall be able to support a vertical hedge or vines capable of reaching and being maintained at 5’ height.
  - Vining plants that grow up and over top of garage door are encouraged.
  - Provide a vertical accent tree or large vertical shrub at the corner of the building facing the private drives to soften and frame the building.
  - Plant materials shall comply with the landscaping requirements and plant palette of this Specific Plan.
  - Adequate lighting shall be provided in the private drives and may include free standing lighting and/or lighting affixed to the garage itself (i.e. carriage lights).



*Examples of the treatment of private drives with landscaping.*

### **Business-Related Standards**

Development standards for developments in the Town Center, General Commercial, Freeway Commercial, Freeway Incubator, Corporate Center, Business Center, and Employment land use categories are provided on Table 3-5.

**Table 3-5  
Development Standards –Business and Commercial Uses**

<b>Standard</b>	<b>Town Center</b>	<b>General Commercial</b>	<b>Freeway Commercial <sup>7</sup></b>	<b>Freeway Incubator <sup>6</sup></b>	<b>Corporate Center</b>	<b>Employment</b>	<b>Business Center <sup>12</sup></b>
<b>Maximum Intensity</b>							
0-5 acres: Max. FAR <sup>1,11</sup>	0.25	0.25	0.25	0.25–0.35 <sup>8</sup>	0.35–0.75 <sup>8</sup>	0.35 FAR	0.45 FAR
5-10 acres: Max. FAR <sup>1,11</sup>	0.25	0.25	0.25	0.25–0.35 <sup>8</sup>	0.35–0.75 <sup>8</sup>	0.40 FAR	0.50 FAR
> 10 acres: Max. FAR <sup>1,11</sup>	0.25	0.25	0.25	0.25–0.35 <sup>8</sup>	0.35–0.75 <sup>8</sup>	0.45 FAR	0.55 FAR
<b>Building Site Specifications</b>							
Min. lot size	15,000 s.f.	15,000 s.f.	15,000 s.f.	22,500 s.f.	22,500 s.f.	22,500 s.f.	1 acre.
Min. lot width <sup>9</sup>	100 ft	100 ft	100 ft	150 ft	150 ft	150 ft	200 ft
Min. lot depth <sup>9</sup>	100 ft	100 ft	100 ft	150 ft	150 ft	150 ft	200 ft
Min. landscape coverage <sup>10</sup>	10%	10%	10%	10%	10%	10%	10%
<b>Front and Side Street Setbacks <sup>2</sup></b>							
Bldgs < 100,000 sq. ft.	0 ft <sup>3</sup>	10 ft avg. 5 ft min.	20 ft avg. 15 ft min.	20 ft avg. 15 ft min.	0 ft	20 ft avg. 15 ft min.	20 ft avg. 15 ft min.
Bldgs > 100,000 sq. ft.	0 ft <sup>3</sup>	10 ft avg. 5 ft min.	20 ft avg. 15 ft min.	30 ft avg. 25 ft min.	30 ft avg. 25 ft min.	30 ft avg. 25 ft min.	30 ft avg. 25 ft min.
<b>Interior Side Setbacks <sup>2</sup></b>							
If adjacent to business uses	0 ft or separated by 20 ft plus 10 ft for each additional floor						
If adjacent to residential uses <sup>4</sup>	NA	20 ft single story and 5 ft for each additional floor	NA	40 ft single story and 10 ft for each additional floor	NA	40 ft single story and 10 ft for each additional floor	40 ft single story and 10 ft for each additional floor

## Development Criteria

**Table 3-5  
Development Standards –Business and Commercial Uses**

Standard	Town Center	General Commercial	Freeway Commercial <sup>7</sup>	Freeway Incubator <sup>6</sup>	Corporate Center	Employment	Business Center <sup>12</sup>
<b>Rear Setbacks <sup>2</sup></b>							
If adjacent to business uses/streets	10 ft avg. 5 ft min.	15 ft avg. 10 ft min.	15 ft avg. 10 ft min.	15 ft avg. 10 ft min.	0 ft or min. 15 ft	25 ft avg. 20 ft min	25 ft avg. 20 ft min
If adjacent to residential uses <sup>4</sup>	NA	20 ft single story and 5 ft for each additional floor	NA	40 ft single story and 10 ft for each additional floor	NA	40 ft single story and 10 ft for each additional floor	40 ft single story and 10 ft for each additional floor
<b>Height <sup>2</sup></b>							
Max. height <sup>5</sup>	60 ft	50 ft	60 ft	75 ft	75 ft	75 ft	75 ft
Max. stories	4	2	4	6	6	6	6
<b>Other</b>							
Building separation	0 ft or 10 ft	10 ft	0 ft or 10 ft	0 ft or 10 ft	0 ft or 10 ft	0 ft or 10 ft	0 ft or 10 ft

Note: All setbacks shall be measured from the property line or, if applicable, the back of landscape easement unless otherwise noted.

<sup>1</sup> Maximum nonresidential intensity may be increased with qualifying features as noted on Nonresidential Incentives section later in this Specific Plan. For purposes of determining maximum buildout, a target FAR of 0.4 was assumed in the Employment category and 0.5 in the Business Center category.

<sup>2</sup> See Table 3-9 for variations to the standards noted on this table.

<sup>3</sup> Zero feet setback if the building entrances and windows are oriented toward the street, otherwise 20-foot setback.

<sup>4</sup> If loading and unloading areas directly face residences, there shall be a minimum setback of 120 feet from the property line to the loading bay doors and a minimum building-to-building separation of 150 feet. Stated setbacks apply to situations where the loading doors do not directly face residences (e.g. side-on configuration).

<sup>5</sup> Maximum building height is defined as the height from finished grade to the top of the roof pitch or top of parapet, whichever is greater. An architectural projection such as a chimney or nonhabitable tower may exceed the maximum building height by 10 feet.

<sup>6</sup> Parcels within Planning Area 5 may be developed to the uses and standards of the Employment land use category and Planning Area 7 may accommodate those uses listed in the Corporate Center land use category.

<sup>7</sup> Planning Area 21 shall not develop in a single-line format, that is, as a typical strip commercial use. Development in Planning Area 21 shall be required to be developed as noted in Table 3-9, *Development and Design Variations*.

<sup>8</sup> See Figure 2-2, *Land Use Diagram*, for the locations of the various densities.

<sup>9</sup> In some cases, when the minimum lot width and depth are multiplied together, they do not match the minimum lot size. This was done on purpose to allow flexibility in lot configurations; however, all lots must meet the minimum lot size, width, and depth standards.

<sup>10</sup> See the landscape provisions in the General Business Development Standards section for greater detail and definition.

<sup>11</sup> Maximum FAR's apply to those parcels that do not involve consolidations with other lots. See Table 3-10 in the Nonresidential Incentives section regarding the potential for the City to award FAR bonuses to projects that involve consolidation with other lots to increase their base lot size.

<sup>12</sup> For proposals within Planning Areas 60b or 60c larger than 500,000 square feet, see the EDC Review Procedure in Section 7, Implementation

### General Business Development Standards

The following General Development Standards apply in the Town Center, General Commercial, Freeway Commercial, Freeway Incubator, Corporate Center, Employment, and Business Center land use categories within the Renaissance Specific Plan.

#### Refuse and Recycling Receptacles

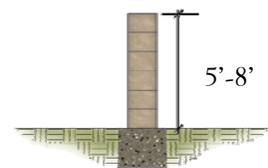
- ❖ Refuse and recycling receptacles shall be completely screened from public rights-of-way and parking areas through site orientation, enclosures, and/or landscaping.
- ❖ All development shall be provided with 1 refuse bin and 1 recycling bin enclosure per building or as required by trash provider/City ordinance.
- ❖ Refuse and recycling enclosures shall have a minimum interior dimension of 10 feet by 8 feet and be enclosed by 6 foot tall walls with self-latching gates or as required by trash provider/City ordinance.
- ❖ Refuse and recycling receptacles shall be within easy access to trash removal and shall be separated from any residential structure by a minimum of 20 feet.

#### Walls and Fences

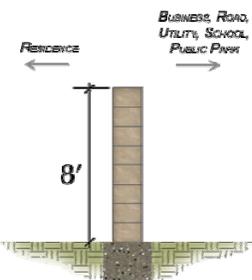
Walls, fences, or other screening devices in shall be allowed provided they meet the following requirements:

- ❖ The height of walls and fences shall be measured from the top of the highest adjacent grade unless otherwise noted or if adjacent to a public right-of-way, in which case the measurement shall be taken from the side of the public right-of-way.
- ❖ Solid masonry walls shall be required at all rear and side property lines as follows:
  - Walls shall be a minimum of 5 feet in height and a maximum of 8 feet in height when located on an interior property line adjacent to a business use or land use designation.
  - Walls shall be a minimum of 8 feet in height when adjacent to any residential use or land use designation or if located on the exterior property line.
  - When a retaining wall is necessary, combined retaining walls and walls shall be permitted as long as they do not exceed a combined total height of 9 feet as measured from the lowest adjoining grade.
  - No wall located within an area defined by a corner cutoff line between two points each measured 30 feet horizontally from the

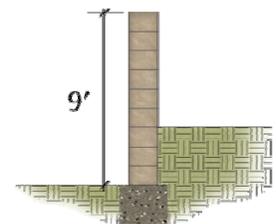
Rear and Side Walls



Standard interior wall



Exception: Between residential and non-residential



Exception: Combined wall and retaining wall

## Development Criteria

point of intersection of the property lines at the street corner may exceed a height of 3 feet.

- For retaining walls located on the perimeter, side, or rear property lines:
  - When a retaining wall is necessary, combined retaining walls and walls shall be permitted as long as they do not exceed a combined total height of 9 feet as measured from the lowest adjoining grade.
  - If visible from the public right-of-way, the maximum height of any solid retaining wall shall be 3 feet as measured from the lowest adjacent grade.
  - If retaining walls are not visible from public areas or if they are visible from public areas and unique designs are incorporated to disguise or break up the mass of the retaining wall (e.g. offsets, landscape walls, unique materials, or public art) the maximum height may be 6 feet as measured from the lowest adjacent grade.
  - A second retaining wall may be erected above the lower retaining wall with a minimum 3 foot landscaped separation. The height provisions governing the lower retaining wall, above, shall apply to the second retaining wall.
- ❖ All fences and walls shall be designed to be compatible with the overall architectural style, of a durable quality, and of a color and material complementary to the overall physical design.
- ❖ The following types of fence materials are prohibited:
  - Barbed wire
  - Wood
  - Wire
  - Corrugated metal
  - Electrically charged fences
  - Plain exposed plastic concrete/PCC
  - Chain-link with or without grapestake fencing or similar materials
- ❖ All walls, fencing, or screening materials shall be maintained in a physical state similar to that at the time of installation. Repair and/or replacement of damaged, defective, or severely weathered materials shall be completed immediately upon occurrence or within a minimum of 20 days of notification by the City.
- ❖ Display areas visible from the public right-of-way shall utilize decorative material, including but not limited to block, wrought iron, or other similar material.

- ❖ Landscaping shall be used to screen walls or fences that are visible from the public right-of-way and that exceed 50 feet in length.
- ❖ No storage shall exceed the height of the wall when adjacent to a residential use or land use designation unless it complies with the building setbacks.

### *Screening*

- ❖ All utility connections shall be coordinated with the development of the site and should not be exposed, except where deemed appropriate or necessary by the City.
- ❖ All electrical, telephone, cable TV, and similar service wires and cables shall be installed underground or within buildings.
- ❖ Utility equipment, such as surface-mounted transformers, pedestal-mounted terminal boxes and meter cabinets, and sprinkler manifolds may be placed aboveground, provided they are screened from view in the building, a fully enclosed structure, or by landscaping. All vent pipes and similar devices that are attached to the building shall be painted to match the building. All roof-access ladders shall be screened from view.
- ❖ All roof-mounted equipment, including but not limited to mechanical equipment, satellite dishes, tanks, and ducts, shall be screened on all sides from street-level public view and neighboring residences by a parapet wall, decorative enclosure, or other architectural element. Equipment screening shall be designed and painted to match the building and shall be equal to the maximum height of the equipment.
- ❖ All storage, including cartons, containers, materials, or trash shall be shielded from view within a building or area enclosed by a solid fence.
- ❖ No storage shall exceed the height of the wall when adjacent to a residential use or land use designation unless it complies with the building setbacks.
- ❖ Ground-mounted equipment, including heating and air conditioning units and trash receptacles, shall be completely screened from view from surrounding properties through the use of screen walls, landscaping, or other methods.
- ❖ Loading docks that are adjacent to public streets, residential areas, and open space shall be screened from adjacent uses through the use of landscaping, earthen berms, or decorative walls or fencing.
- ❖ In locations where residences abut nonresidential uses or streets, screening or buffering is necessary to reduce impacts. See Table 3-9 and the street sections contained in the Mobility Plan section for screening provisions.

## Development Criteria

- ❖ Exposed gutters, downspouts, vents, louvers, and other similar elements shall be painted to match the surface to which they are attached, unless the elements are incorporated as part of the design element of the site.

### *Property Maintenance*

Property shall be maintained in an acceptable visual condition. This maintenance includes the following.

- ❖ Irrigation, seeding, pruning, and necessary replanting of landscaping areas
- ❖ Removal of trash, discarded materials, and equipment
- ❖ Removal of boats, trailers or other vehicles either unrelated to the on-site business or in a deteriorated or incomplete condition
- ❖ Removal of containers or shipping materials after their function is accomplished
- ❖ Painting or appropriate preservation of building façades and walls

### *Lighting*



*Example of the use of lighting envisioned in Renaissance.*

- ❖ Lighting standards shall be energy efficient and in scale with the height and use of the structures on-site.
- ❖ Light standards shall not exceed 35 feet in height above finish grade, shall be consistent with the architectural theme of the facility served, and shall be located within landscape planter areas. Within 100 feet of the property line of a residentially designated parcel, light standards shall not exceed 20 feet in height.
- ❖ All lighting, including security lighting, shall be directed away from adjoining properties and the public right-of-way.
- ❖ Parking areas shall have lighting that provides adequate illumination for safety and security as approved by the Building and Safety and/or the Police Department. At a minimum, parking lot lighting fixtures should maintain a minimum of 1 foot-candle of illumination at ground level at any location within the parking facility, excepting at convenience markets, where the standard should be a minimum of 1.5 foot-candles.
- ❖ The level of lighting shall not exceed 0.5 foot-candle at any residential property line or 1 foot-candle at any nonresidential property line.
- ❖ All lighting fixtures, other than pedestrian walkway lighting less than 6 feet in height, shall be shielded (equipped glare reflector shields designed to redirect spill lighting downwards and away from surrounding areas) such

that no light is emitted above the horizontal plane of the bottom of the light fixture.

- ❖ Office and commercial structures should incorporate exterior lighting to illuminate significant exterior features and landscaping.

### *Noise Control*

- ❖ The driver of any diesel vehicle: (1) shall not idle the vehicle's primary diesel engine for greater than 5 minutes at any location; and (2) shall not idle a diesel fueled auxiliary power system for more than 5 minutes to power a heater, air conditioner, or any ancillary equipment on the vehicle if it has a sleeper berth and the truck is located within 100 feet of a restricted area (homes and schools)
- ❖ Noise control must comply with Chapter 9.50 of the Rialto Municipal Code
- ❖ Signage shall be posted in the parking lots of each parcel that is adjacent to residentially designated areas with the following text, "Non-essential idling for more than 5 consecutive minutes is prohibited (13 CCR § 2485). Report violators: 1-800-CUT-SMOG"

### *Operational Restrictions*

No commercial or industrial activity located immediately adjacent to a residentially zoned area (e.g. not separated by a street or park) shall be permitted to operate between the hours of 11:00 p.m. and 6:00 a.m. and loading activities shall not be permitted between the hours of 10:00 p.m. and 6:00 a.m.

### *Transportation Control Measures*

Per Chapter 18.59, Transportation Control Measures, of the Municipal Code.

### *Nonconforming Uses*

Per Chapter 18.60, Nonconforming Uses, of the Municipal Code.

### *Signs*

Per Section 5, *Signs*, of this Specific Plan.

### *Landscape Provisions*

- ❖ As noted on Table 3-5, a minimum percentage of landscaping is required. Landscape coverage shall be calculated from the property lines of individual buildings and perimeter property lines of multi-tenant developments. Items that may be counted toward the landscape coverage provisions include: ground-floor patios, yards, or courtyards, landscaped parkways, slope areas, water features, parks, paseos, pedestrian pathways, planter areas, landscape easements, and turfed areas,. Landscape coverage shall not include streets,

## Development Criteria

driveways, parking spaces, or street-adjacent sidewalks depicted on the street sections.

- ❖ All setback areas fronting on or visible from a public street and recreation/common open space areas shall be landscaped and permanently maintained in an attractive manner. Such landscaping shall primarily consist of turf, lawn, ferns, trees, shrubs, or other living plants.
- ❖ All landscaping shall comply with Chapter 12.50, *Water Efficient Landscape Requirements*, of the Rialto Municipal Code.
- ❖ A minimum of 10 percent of the required gross off-street parking area shall be landscaped. The parking area shall include access drives, aisles, stalls, maneuvering areas and required landscape setbacks when adjacent to the setback area.
- ❖ Landscaping, earthen berms, or a wall approximately three (3) feet in height shall be provided between parking areas, driveways, drive-through lanes and the streets to provide screening and shield headlights.
- ❖ At least one 15 gallon tree shall be provided for every 6<sup>th</sup> stall as counted linearly to provide shade for parking and driveway areas. These trees may be grouped or clustered if approved by the Director of Development Services.
- ❖ Planters or end islands shall be provided to physically and visually separate the ends of parking bays from drive aisles. Planters shall be a minimum of five feet in width and contain landscaping to screen parking spaces and trees to provide shade.
- ❖ Permanent, 100 percent automatic irrigation facilities shall be provided in all landscaped areas.

### *Parking and Loading*

#### **Parking Standards**

Table 3-6, on the following page, shows the minimum parking ratio and spaces required for each use or equivalent use in Renaissance.

**Table 3-6  
Parking Standards**

Use	Standard
Neighborhood shopping center (multitenant or multioccupant shopping center of less than 2 net acres in size)	One space per 200 square feet on the ground floor plus one space per 300 square feet on all floors above the ground floor.
Community shopping center (2 to 20 net acres in size)	One space per 250 square feet on the ground floor, plus one space per 400 square feet on all floors above the ground floor.
Regional shopping center (more than 20 net acres in size)	One space per 300 square feet on the ground floor, plus one space for 400 square feet on all floors above the ground floor
Retail (grocery store, convenience market, etc.)	One space per 250 square feet
Restaurant (including fast food drive-through)	One space per 100 square feet
Office/banks	One space per 250 square feet
Health club	One space per 200 square feet
Movie theater/entertainment	One space per every four seats
Light industrial/manufacturing <sup>1</sup>	One space per 500 square feet for up to 40,000 square feet of building space; plus either: a) one space per 750 square feet for building area beyond 40,000 square feet, or 2) one space for each employee on the maximum shift as determined by the Director of Development Services; plus one space per 250 square feet of gross floor area of office space.
Distribution/warehouse <sup>1</sup>	One space per 1,000 square feet for up to 40,000 square feet of building space, plus one space per 4,000 square feet for building area beyond 40,000 square feet plus one space per 250 square feet of gross floor area of office space.
Service yards, contractor storage yards, and the like	One space per 2,000 square feet of lot area

Notes:

Unless noted, parking standards are based on gross floor area.

All uses not specifically listed or substantially equivalent to a listed use are subject to the Rialto Municipal Code, Section 18.58.050 Office, Commercial and Industrial Parking Requirements.

<sup>1</sup> Where multiple tenants and or uses occupy the same building, the parking shall be calculated based upon the floor area used by each tenant or use.

# Development Criteria

## Parking Requirements

- ❖ General provisions. Per Section 18.58.020, General Provisions, of the Municipal Code.
- ❖ Disabled parking per the requirements of the Rialto Municipal Code, Section 18.58.050, Office, Commercial and Industrial Parking Requirements.
- ❖ All commercial sites shall be evaluated for parking on a site-by-site basis. In addition, larger developments that are part of a mixed-use retail center shall have the option of preparing a shared parking analysis to determine the proper amount of parking throughout a 24-hour period.
- ❖ All large commercial uses of 15,000 square feet or greater, except medical, require one loading space unless otherwise noted. In addition, appropriate cueing of six-car stacking back from the ATM or pick-up window for drive-through banks, pharmacies, and eating facilities shall be provided.
- ❖ Standard parking stall dimensions shall be minimum dimensions of each stall shall be not less than nine feet wide and eighteen feet long and shall have a vertical clearance of not less than eight and one-half feet.
- ❖ Each office, commercial or industrial parking space or facility shall have adequate drive aisles and usable turning and maneuvering areas per Section 18.58.030 of the Municipal Code.
- ❖ If located immediately adjacent to a residentially designated area (e.g. not separated by a street or park), 5 feet of irrigated landscaping is required between the parking and/or drive isle and the residential property line. In addition, a minimum 20 foot building separation is required between residential and non-residential uses. In order to ensure that this landscaped area provides maximum benefit and function, it may be added to the abutting residential properties. If this is not possible (e.g. the residences are developed first), then the landscaped area shall be provided on the non-residential side.
- ❖ All parking areas, loading facilities and drive aisles shall be surfaced and maintained with asphaltic concrete, cement or other permanent impervious or pervious surfacing material that is acceptable by the Development Review Committee and City Engineer.

## Driveway Requirements

- ❖ The number and location of driveways shall be subject to Development Review Committee approval of the parking facility design. The dimensions of curb cuts shall comply with approved city standards.

- ❖ For non-residential projects, the location and width of the driveways shall be reviewed and approved by the Traffic Engineer.
- ❖ Properties that cannot meet the minimum spacing requirements shall enter into an irrevocable reciprocal driveway easement(s) with adjacent properties. The legal instrument shall be approved by the City and recorded in the Office of the County Recorder prior to final building permit approval. The recorded agreement shall remain in effect in perpetuity, except as modified or rescinded pursuant to the expressed written approval of the City. Interim driveways are acceptable to allow access until such time as the reciprocal driveways can be executed.

### *Detention/Drainage*

Detention and drainage areas shall be permitted in all land use categories as necessary and on a case-by-case basis. When possible, these areas should be designed to blend in with the surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space. On-site detention facilities may be permanent or temporary based upon the capacity, status, and connection to downstream drainage facilities.

### *Outdoor Dining*

- ❖ Outdoor dining is encouraged to enliven the street scene.
- ❖ Outdoor seating areas shall have a 6-foot minimum dimension from the storefront and the minimum 5-foot sidewalk clearance shall be maintained.
- ❖ Outdoor dining and patio areas may be enclosed by the tenant by open rail compatible to the architecture of the building, hedges, or other suitable separation.



*Outdoor dining is encouraged in Renaissance.*

### *Temporary Outdoor Activities*

Temporary outdoor activities are events (with or without charge) that are held outside on public or private property on a short-term basis. Temporary outdoor activities include concerts, musical festivals, stage or theatrical shows, fairs, carnivals, exhibits, displays, sports events, and private parties held on vacant property not associated with a building or established facility.

A Temporary Use Permit, which can be obtained from the Development Services Department, is required for all outdoor activities. It is expressly understood that a Temporary Use Permit is not required for authorized ongoing events, construction, or significant alterations of the land. The following temporary activities are permitted in Renaissance with approval of a Temporary Use Permit.

## Development Criteria

**Table 3-7  
Temporary Uses**

<b>Temporary Activity or Use</b>	<b>Zones Permitted</b>	<b>Max Events/Calendar Year</b>	<b>Max Days per Event</b>	<b>Max. Days per Calendar Year</b>	<b>Time/Days between Events</b>
Parking lot, outdoor, and sidewalk sales	All commercial or industrial zones	4	4	16	60
Tent revivals	All commercial or Industrial zones	2	10	20	60
Circuses, carnivals, rodeos, pony riding, or special events	All commercial, industrial, and public park zones	2	7	14	90
Promotional events, such as radio and TV promotions and contests	All commercial, public park, and industrial zones	4	4	16	60
Concerts, exhibits, arts and crafts shows, and festivals	All commercial or industrial zones	4	3	12	60
Temporary sports events	All commercial or Public park zones	7	2	14	30
Christmas tree lots	All commercial or industrial zones	1	60	November 1st December 31st	10 months
Pumpkin sales lots	All commercial or industrial zones	1	30	October 1st October 31st	11 months
Temporary land uses in association with an existing business	All commercial or industrial zones	Duration and frequency as determined by the Director of Development Services			

### *Antennas*

- ❖ These provisions do not apply to a telecommunications facility (e.g. cell tower).
- ❖ A satellite antenna may be installed on a lot in any zoning district provided that: (1) it is not located in a required front yard; (2) when roof-mounted, the antenna or satellite antenna is located on the rear one-half of the roof, and; (3) the antenna does not exceed 10 feet if installed on the ground or, if roof-mounted, the maximum building height for the district in which the satellite antenna is located.
- ❖ Direct Broadcast Satellite (DBS) antennas 1 meter or less in diameter should be installed in locations that are not readily seen from neighboring properties or from the street, such as the rear of the property. Antennas that are greater than 1 meter in diameter are prohibited.

### *Wireless Telecommunication Facilities*

- ❖ A wireless telecommunications facility is an antenna structure and any accessory equipment that is used in connection with the provision of commercial mobile services. This section does not apply to:
  - Any antenna designed and used solely to receive UHF, VHF, AM, and FM broadcast signals from licensed radio and television stations
  - Any antenna that is designed and used solely in connection with authorized operations of an amateur radio station licensed by the FCC (i.e., a "HAM" radio transmission).
  - Any antennae measuring one (1) meter or less in diameter and used solely for connection with satellite dish television services.
- ❖ Wireless telecommunication facilities shall be permitted through a Conditional Development Permit and shall:
  - Comply with the height requirements of the underlying land use designation.
  - Be either 1) stealth or 2) wall/roof mounted and completely screened from public view in a manner that is consistent with the buildings existing architectural style, color, or materials.
  - "Stealth" means any wireless telecommunications facility that is disguised to appear as another object that exists in the surrounding environment or which is architecturally integrated into a building or other structure. They may include, but are not limited to, architecturally screened roof mounted antenna, façade-mounted antenna as design features, clock towers, flagpoles, church crosses, and "tree" poles (monopalms, monopines).

# Development Criteria

## *Drive-Through Facilities*

A drive-through facility shall be operated only as part of a restaurant, bank, or pharmacy for a drug store or grocery store and in such locations provided for such uses within the Renaissance Specific Plan. All drive-through facilities shall be subject to the following performance standards and shall be approved through a Conditional Development Permits, as outlined in Chapter 18.66 of the Municipal Code.

### **Separation from Sensitive Land Uses**

- ❖ A drive-through facility shall be separated from any single-family residential development or single-family residential district by a public street, a private street, or a distance of no less than 200 feet. A drive-through facility within a General Commercial or Town Center District shall be separated from any stand-alone multi-family building by no less than 150 feet. This standard may be modified, particularly for nonrestaurant drive-through uses, if mitigation measures satisfactory to the City are presented in the overall design program.
- ❖ The drive-through facility shall be architecturally treated with service and delivery areas visually screened from residential development with a wall, landscaping, other screening features, or by other natural or constructed barriers, such as other commercial development.

### **Minimum Site or Part of Center or Development Complex**

A restaurant with drive-through facilities shall have a minimum 0.5-acre land area (legal lot or tax parcel) or shall be part of a commercial center or larger development complex of at least 1 acre.

### **Setbacks**

All structures, parking areas, drive-through stacking and exit lanes, intercom systems, trash enclosures, etc., shall be set back a minimum of 10 feet from any property line, or if part of a commercial center or larger development complex, 10 feet or more from any perimeter property line of the center or complex.

### **Circulation**

- ❖ The design, length, and width of drive-through lanes shall be determined by the City's traffic engineer. Drive-through lanes shall be screened through building orientation, landscaping, low screen walls, hedges, or trelliswork.
- ❖ A minimum of 10 percent of the required gross off-street parking area shall be landscaped. The parking area shall include access drives, aisles, stalls, maneuvering areas and required landscape setbacks when adjacent to the setback area.

- ❖ Landscaping, earthen berms, or a wall approximately three (3) feet in height shall be provided between parking areas, driveways, drive-through lanes and the streets to provide screening and shield headlights.
- ❖ Planters or end islands shall be provided to physically and visually separate the ends of parking bays from drive aisles. Planters shall be a minimum of five feet in width and contain landscaping to screen parking spaces and trees to provide shade.

### **Pedestrian Safety**

- ❖ The project applicant may be required to prepare and submit to the City a traffic study. The study shall address (1) the function of the internal street(s) or driveway(s) that serve the subject parcel for bypass, parking access, and drive-through queuing; (2) the placement, design, and adequacy of the vehicle queuing aisle; (3) the on-site circulation, parking lot design, and pedestrian/bicycle safety for the parking supporting the use; and (4) additional information as requested by the City Engineer. The traffic circulation study shall identify potential adverse impacts and include measures for mitigating such impacts.
- ❖ Appropriate queuing of six cars for drive-through banks and pharmacies shall be provided in either a single lane or two lanes.
- ❖ Site design shall minimize pedestrian/vehicle conflicts by including sidewalks that promote safe and convenient pedestrian movement.

### **Restrooms**

Access to bathroom facilities located within the restaurant development shall be from within the structure, with no direct access from the parking area.

### **Noise**

- ❖ Noise levels from the drive-through facilities shall not exceed the City noise standards.
- ❖ The project applicant shall provide the plans and specifications for any potential noise sources, such as intercom system, trash compactor, etc. Plans shall include measures to mitigate any potential adverse impact from such noise sources.
- ❖ Speaker boxes of any point to point intercom system shall be oriented away from residential development and other sensitive receptors located in the general area of the drive-through facility.
- ❖ Outdoor maintenance and cleaning activities shall be limited, if determined necessary by the City, to achieve compatibility with surrounding land uses.

## Development Criteria

- ❖ The on-site manager shall not permit any loud music, noise, amplified sounds, fighting, quarreling, loitering, or other nuisance that disturbs the peace of the premises or the neighborhood. Outdoor music as part of an outdoor dining area shall be allowed subject to City-approved noise thresholds, between the hours of 9:00 a.m. and 9:00 p.m., with speakers oriented away from residential uses.
- ❖ Generally speaking, drive-through and delivery operations shall be limited to the hours of 6:00 a.m. to 12:00 a.m. within Planning Area 55 if it is developed as commercial or within 150 feet of residential uses, unless mitigation measures are provided to the City's satisfaction to address potential noise impacts on adjacent uses.

### Light and Glare

- ❖ All lighting fixtures, other than pedestrian walkway lighting less than 6 feet in height, shall be shielded (equipped glare reflector shields designed to redirect spill lighting downwards and away from surrounding areas) such that no light is emitted above the horizontal plane of the bottom of the light fixture.
- ❖ All lighting in the parking lots and drive-through areas shall comply with the provisions of the Renaissance Specific Plan. No motion-sensor lighting shall be allowed within any areas exposed to residential uses.
- ❖ All lighting shall be subject to a 30-day review period, during which time illumination levels shall be evaluated and adjusted where determined necessary by the City.
- ❖ Within 100 feet of the property line of a residentially designated parcel, light standards shall not exceed 20 feet in height and shall not exceed 0.5 foot-candle at any residential property line.

### Maintenance

- ❖ The site shall be maintained in a litter-free condition and no undesirable odors shall be generated on the site. The on-site manager shall make all reasonable efforts to see that the trash or litter originating from the use is not deposited on adjacent properties. Trash enclosures and bins shall be enclosed on all sides to suppress odors and prevent spillage of materials. Employees shall be required daily to pick up trash or litter originating from the site within 150 feet of the perimeter of the property. Graffiti shall be removed within 48 hours.
- ❖ The on-site manager of the use shall take whatever steps are deemed necessary to assure the orderly conduct of employees, patrons, and visitors on the premises.

- ✦ A copy of the above maintenance standards and any conditions of approval shall be posted alongside the necessary business licenses and be visible at all times to employees.

### Additional Conditions

The above performance standards constitute the minimum deemed necessary under general circumstances and in most cases to prevent adverse effects from drive-through facilities. Other standards may be required as conditions of approval defined by City Planning staff to ensure that such uses are in accord with the intent of the Specific Plan and in concert with the integrity of the proposed project.

## Other Uses

Development standards for the Schools, Public, Private Parks, Public Parks, Private Recreation Center, and Utilities land use categories are provided on Table 3-8.

**Table 3-8  
Development Standards – Open Space and Recreation Uses**

Standard	Schools	Private Parks	Public Parks	Private Recreation Center	Utilities
<b>Building Setbacks<sup>1</sup></b>					
From street	20 feet	10 feet	10 feet	10 feet	10 feet
From any abutting residential lot	100 feet	15 feet	25 feet	10 feet	10 feet
<b>Height</b>					
Maximum building height <sup>2</sup>	50 feet	25 feet	25 feet	45 feet	NA
Maximum stories	2	1	1	3	NA

Notes:

All setbacks shall be measured from the property line or, if applicable, the back of landscape easement unless otherwise noted.

<sup>1</sup> Does not apply to shade structures of 150 square feet or less and informational signs.

<sup>2</sup> Uninhabitable architectural projections, such as towers, turrets, and chimneys may exceed the maximum building height by 15%.

# Development Criteria

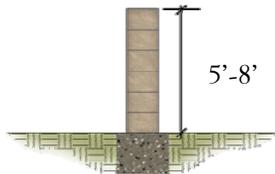
## General Development Standards

The following General Development Standards apply to all developments in the Schools, Private Parks/Paseos, Public Parks, Private Recreation Center, and Utilities land use categories.

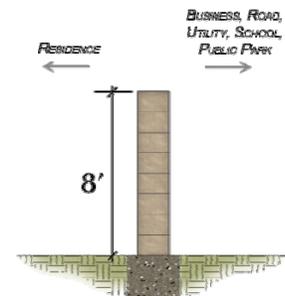
### Refuse Receptacles

- ❖ Refuse and recycling receptacles shall be completely screened from public rights-of-way and parking areas through site orientation, enclosures, and/or landscaping.
- ❖ All development shall be provided with 1 refuse bin and 1 recycling bin enclosure per building or as required by trash provider/City ordinance.
- ❖ Refuse and recycling enclosures shall have a minimum interior dimension of 10 feet by 8 feet and be enclosed by 6 foot tall walls with self-latching gates or as required by trash provider/City ordinance.
- ❖ Refuse and recycling receptacles shall be within easy access to trash removal and shall be separated from any residential structure by a minimum of 20 feet.

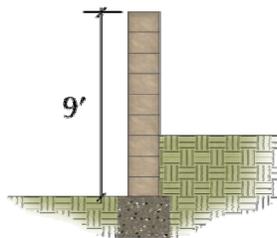
Rear and Side Walls



Standard interior wall



Exception: Between residential and non-residential



Exception: Combined wall and retaining wall

### Walls and Fences

Walls, fences, or other screening devices in areas designated for schools, parks, and utilities shall be allowed provided they meet the following requirements:

- ❖ The height of walls and fences shall be measured from the top of the highest adjacent grade unless otherwise noted or if adjacent to a public right-of-way, in which case the measurement shall be taken from the side of the public right-of-way.
- ❖ Wood, chain link, barbed and razor wire are not permitted. Chain link may be used on a temporary basis at construction sites. Vinyl coated chain link may be used as a fencing material for outdoor park facilities such as tennis courts
- ❖ Solid masonry walls a minimum of 6 feet in height shall be required with the following exceptions:
  - A solid masonry wall a maximum of 8 feet in height is permissible when adjacent to any residential use or land use designation.
  - Around private parks and the private recreation center, a combination of solid masonry wall (minimum 5 feet in height) and view-fencing (maximum 1 foot in height) may be utilized.
  - No wall located within an area defined by a corner cutoff line between two points each measured 30 feet horizontally from the

point of intersection of the property lines at the street corner may exceed a height of 3 feet.

❖ For retaining walls:

- When a retaining wall is necessary, combined retaining walls and walls shall be permitted as long as they do not exceed a combined total height of 9 feet as measured from the lowest adjoining grade.
- If visible from the public right-of-way, the maximum height of any solid retaining wall shall be 3 feet as measured from the lowest adjacent grade.
- If retaining walls are not visible from public areas or if they are visible from public areas and unique designs are incorporated to disguise or break up the mass of the retaining wall (e.g. offsets, landscape walls, unique materials, or public art) the maximum height may be 6 feet as measured from the lowest adjacent grade.
- A second retaining wall may be erected above the lower retaining wall with a minimum 3 foot landscaped separation. The height provisions governing the lower retaining wall, above, shall apply to the second retaining wall.

❖ All fences and walls shall be designed to be compatible with the overall architectural style, of a durable quality, and of a color and material complementary to the overall physical design.

❖ The following types of fence materials are prohibited:

- Barbed wire
- Wire
- Wood
- Corrugated metal
- Electrically charged fences
- Plain exposed plastic concrete/PCC
- Chain-link with or without grapestake fencing or similar materials

❖ All walls, fencing, or screening materials shall be maintained in a physical state similar to that at the time of installation. Repair and/or replacement of damaged, defective, or severely weathered materials shall be completed immediately upon occurrence or within a minimum of 20 days of notification by the City.

❖ Landscaping shall be used to screen walls or fences that are visible from the public right-of-way and that exceed 50 feet in length.

# Development Criteria

## *Screening*

- ❖ All utility connections shall be coordinated with the development of the site and should not be exposed, except where deemed appropriate or necessary by the City.
- ❖ Utility equipment, such as surface-mounted transformers, pedestal-mounted terminal boxes and meter cabinets, and sprinkler manifolds, may be placed above ground provided they are screened from view in the building or a fully enclosed structure. All vent pipes and similar devices that are attached to the building shall be painted to match the building. All roof-access ladders shall be screened from view.
- ❖ All roof-mounted equipment, including but not limited to mechanical equipment, satellite dishes, tanks, and ducts, shall be screened on all sides from street-level public view and neighboring residences by a parapet wall, decorative enclosure, or other architectural element. Equipment screening shall be designed and painted to match the building and shall be equal to the maximum height of the equipment.
- ❖ All storage, including cartons, containers, materials, or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than 6 feet in height.
- ❖ Ground-mounted equipment, including heating and air conditioning units and trash receptacles, shall be completely screened from view from surrounding properties through the use of screen walls, landscaping, or other methods.
- ❖ Loading docks that are adjacent to public streets, residential areas, and open space shall be screened from adjacent uses through the use of landscaping, earthen berms, or decorative walls or fencing.
- ❖ Exposed gutters, downspouts, vents, louvers, and other similar elements shall be painted to match the surface to which they are attached, unless the elements are incorporated as part of the design element of the site.

## *Lighting*

- ❖ Lighting standards shall be energy efficient and in scale with the height and use of the structures on-site.
- ❖ Light standards shall not exceed 15 feet in height above finish grade, the design shall be consistent with the architectural theme of the facility served, and shall be located within landscape planter areas.
- ❖ All lighting, including security lighting, shall be directed away from adjoining properties and the public right-of-way.

- ❖ Parking areas shall have lighting that provides adequate illumination for safety and security. Parking lot lighting fixtures shall maintain a minimum of 1 foot-candle of illumination at ground level at any location within the parking facility.
- ❖ The level of lighting shall not exceed 0.5 foot-candle at any residential property line or 1 foot-candle at any nonresidential property line.
- ❖ All lighting fixtures, other than pedestrian walkway lighting less than 6 feet in height, shall be shielded (equipped glare reflector shields designed to redirect spill lighting downwards and away from surrounding areas) such that no light is emitted above the horizontal plane of the bottom of the light fixture.
- ❖ Exterior lighting may be used to illuminate significant exterior features and landscaping.

### *Operational Restrictions*

No recreational activity located adjacent to a residentially zoned area shall be permitted to operate between the hours of 11:00 p.m. and 6:00 a.m.

### *Signs*

Per Section 5, *Signs*, of this Specific Plan.

### *Parking and Loading Standards*

Per Chapter 18.58, *Off-Street Parking*, of the Municipal Code.

### *Detention/Drainage*

Detention and drainage areas shall be permitted in all land use categories as necessary and on a case-by-case basis. When possible, these areas should be designed to blend in with the surrounding development, be landscaped, and be designed to accommodate uses that can be flooded, such as active/passive recreation and natural open space. On-site detention facilities may be permanent or temporary based upon the capacity, status, and connection to downstream drainage facilities.

### *Temporary Outdoor Activities*

As noted in the Table 3-7, *Temporary Uses*, of the General Business Development Standards above.

## Development Criteria

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## Development and Design Variations

In order to create the desired street scene, variations to some development standards are necessary. The following variations override standards listed on Tables 3-3, 3-4, 3-5, and 3-7 in the specific locations noted below in Table 3-9.

**Table 3-9  
Development and Design Variations**

Feature	Issue	Unique Uses/Standards
SR-210	Desire to create a gateway into City and Renaissance and maximize regional visibility. Signage and buildings should be located immediately adjacent to the freeway right-of-way and allowed greater height and mass to increase visibility. Distinct architecture, coloring, landscaping, and lighting should be used to create identity.	<ul style="list-style-type: none"> <li>Habitable structures fronting on SR-210 may exceed the underlying height of the base district by 20%.</li> <li>Nonhabitable architectural features, such as towers, may exceed the maximum height of the underlying land use district by 30%.</li> <li>The Town Center is permitted the freeway-adjacent pylon signs as shown on Figure 5-1. These pylon signs may be a maximum 750 square feet per face and a maximum height of 50 feet.</li> </ul>
Renaissance Parkway	In order to enhance this signature roadway, a wide landscape and setback area is created for residences, as detailed in the street sections. Residences may be oriented with front, rear, or side edges facing Renaissance Parkway and an adequate setback is necessary.	<ul style="list-style-type: none"> <li>From the back of curb, residences that front Renaissance Parkway shall have an average setback of 40 feet. Front porches may encroach 5 feet into this setback.</li> <li>Rear or side oriented buildings shall have an average setback of 35 feet from the back of curb.</li> <li>Direct vehicular access to individual units shall not be taken from Renaissance Parkway.</li> </ul>
Ayala Drive	To minimize noise and pollution impacts, residences should be setback and buffered from these roadways.	<ul style="list-style-type: none"> <li>From the back of curb, residences along Ayala Drive shall have an average setback of 40 feet and a minimum setback of 35 feet.</li> <li>Direct vehicular access to individual units shall not be taken from Ayala Drive.</li> </ul>
Locust and Linden Avenues	These are significant streets that abut residential areas and residences should be shielded from the impacts of traffic and adjacent business uses to the extent feasible.	<ul style="list-style-type: none"> <li>From the back of curb, residences that front Locust and Linden Avenues shall have an average setback of 40 feet. Front porches may encroach 5 feet into this setback.</li> <li>Rear or side oriented buildings shall have an average setback of 35 feet from the back of curb.</li> <li>Direct vehicular access to individual units shall not be taken from Locust and Linden Avenues.</li> <li>Between Renaissance Parkway and Walnut Avenue, these roadways shall not be used for truck through-trips, that is local deliveries are permitted but these segments of the roads shall not be truck routes and shall be signed accordingly.</li> <li>If development in Planning Area 24 has access to Laurel Avenue, truck access shall not be permitted on Locust Avenue. However, if development does not have access to Laurel Avenue, truck access on Locust Avenue may be permitted only if: 1) truck access is</li> </ul>

# Development Criteria

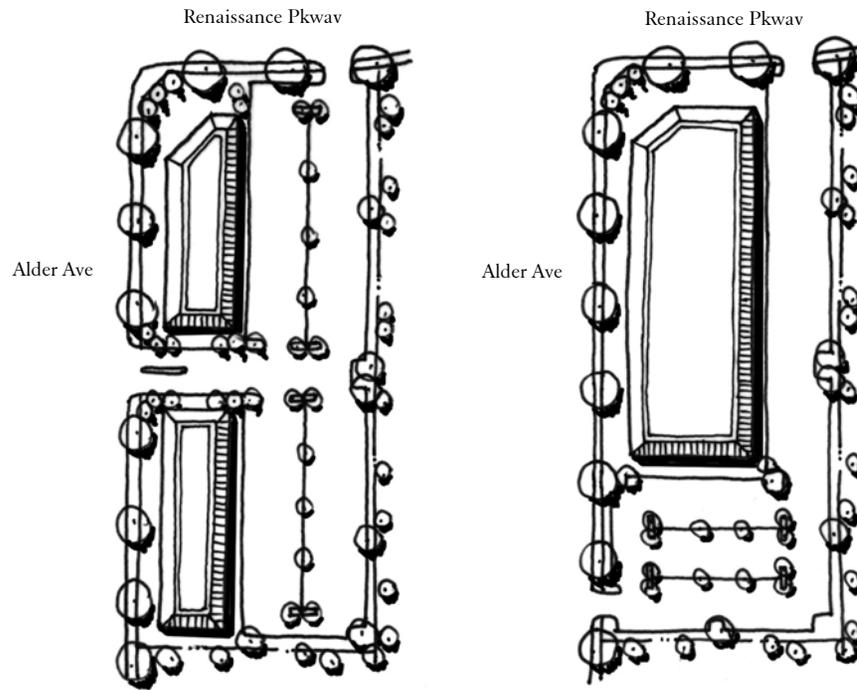
**Table 3-9  
Development and Design Variations**

Feature	Issue	Unique Uses/Standards
		limited to 2 driveways, one in the northern third and one in the southern third of the Locust Avenue frontage; 2) trucks using the northern driveway enter and exit to/from the north (Renaissance Parkway) and trucks using the southern driveway enter and exit to/from the south (Walnut Street); and 3) signs is provided by the developer stating these restrictions.
Gated entries	Entries are the initial theme-setting features of the Village and should be designed to establish character of neighborhoods, accommodate landscaping, and minimize noise impacts to adjacent residences.	<ul style="list-style-type: none"> <li>• Rear-or side-oriented buildings shall have an average setback of 15 feet from the back of sidewalk. Garages shall have a minimum 18 foot setback from property line.</li> <li>• For rear facing residences, an 8-foot-high solid wall may be used within 100 feet of the entry gates to minimize visual and noise impacts.</li> </ul>
Access to major arterials	There are many small parcels with fragmented ownerships along the roadways and there is a need to limit the number of driveways to reduce vehicular conflicts. Accordingly, smaller parcels should be consolidated and/or enter into reciprocal driveway access agreements to share accessways.	<ul style="list-style-type: none"> <li>• Direct vehicular access to individual residential units shall not be taken from arterials.</li> <li>• For non-residential projects, the location and width of the driveways shall be reviewed and approved by the Traffic Engineer.</li> <li>• Properties that cannot meet the minimum spacing requirements shall enter into an irrevocable reciprocal driveway easement(s) with adjacent properties. The legal instrument shall be approved by the City and recorded in the Office of the County Recorder prior to final building permit approval. The recorded agreement shall remain in effect in perpetuity, except as modified or rescinded pursuant to the expressed written approval of the City. Interim driveways are acceptable to allow access until such time as the reciprocal driveways can be executed and shall be reviewed and approved by the Traffic Engineer.</li> </ul>
Planning Areas 82a and 82b	These Planning Areas accommodate an Edison easement	<ul style="list-style-type: none"> <li>• This area is assumed to be a private paseo at this stage; however, it may be public if subsequently approved by the city as such.</li> </ul>
Planning Area 60a	Underground utilities are located within what is currently the Laurel Avenue right-of-way	<ul style="list-style-type: none"> <li>• These utilities may be maintained in place, realigned, or rerouted as determined in the final design for development in Planning Area 60a.</li> </ul>
Business uses adjacent to residential uses (Planning Area 6, 8, 9, 60b, 60c, and 63)	Businesses and residences (existing and proposed) will be located on adjacent parcels and considerations must be incorporated that minimize negative impacts on residences.	<ul style="list-style-type: none"> <li>• If loading and unloading areas directly face residences, there shall be a minimum setback of 120 feet from the property line to the loading bay doors and a minimum building-to-building separation of 150 feet. Stated setbacks apply to situations where the garage doors do not directly face residences (e.g. side-on configuration).</li> <li>• Loading docks that are adjacent to residential areas shall be screened from adjacent uses through the use of landscaping, earthen berms, or decorative walls or fencing.</li> <li>• No commercial or industrial activity located immediately adjacent to a residentially zoned area (e.g. not separated by a street or park) shall be permitted to operate between the hours of 11:00 p.m. and 6:00 a.m. and loading activities shall not be permitted between the hours of 10:00 p.m. and 6:00 a.m.</li> <li>• When adjacent to residential use or land use designation, the side and rear setbacks of structures in the Employment and Freeway Incubator land use designations shall be 40</li> </ul>

**Table 3-9  
Development and Design Variations**

Feature	Issue	Unique Uses/Standards
		feet for single story and 10 feet for each additional floor. <ul style="list-style-type: none"> <li>• In the Employment and Freeway Incubator land use designations, walls shall be a minimum of 8 feet in height when adjacent to any residential use or land use designation.</li> <li>• No storage shall exceed the height of the wall when adjacent to a residential use or land use designation unless it complies with the building setbacks.</li> <li>• Within 100 feet of the property line of a residentially designated parcel, light standards shall not exceed 20 feet in height and shall not exceed 0.5 foot-candle at any residential property line.</li> </ul>
Planning Area 21	Due to lot configuration, there is a potential for the site to develop in a single, in-line format like a typical strip commercial center and which is not desired at this location.	<ul style="list-style-type: none"> <li>• A single, in-line structure located along the eastern property line is not permitted.</li> <li>• Since there many design options, the City shall review options through the Precise Plan of Design process to ensure compliance with these provisions. Possible options include a central parking courtyard; corner plaza; corner structure with unique treatment such as a tower, or corner building cut-off as shown in the thumbnails below.</li> </ul>

# Development Criteria



*Suggested site layouts for Planning Area 21*

## Nonresidential Incentives

Within Renaissance, there are a variety of nonresidential properties and ownership conditions. Some nonresidential properties will be improved as part of a master development while others will occur individually over time. In order to encourage the level of detail and aesthetic quality envisioned, the Renaissance Specific Plan includes this optional package of incentives that are intended to stimulate private investment and result in desired development.

The Incentive Program is a menu of incentives that the City is willing to provide in exchange for desired features. While the underlying land use designations still apply, the property owner may request, and the City may choose to apply, aspects of this program to stimulate desirable development.

The Incentive Program is only available to development within the nonresidential land use categories (Town Center, General Commercial, Freeway Commercial, Freeway Incubator, Corporate Center, Business Center, and Employment) and shall be awarded at the discretion of the City based upon the merits of the amenity/feature. The determination of the appropriate incentive for the proposed feature shall be awarded through a conditional development permit (major and minor), precise plan design, and/or tentative tract map application process on a case-by-case basis using the provisions of this section as a guide.

The City may only award 2 incentives per project. In addition, the same incentive may not be awarded twice for the same project, except as noted below for lot consolidation. For example, an increase in floor area ratio may not be awarded to the same project for both shared parking and reciprocal driveway access. If the incentives listed are not desirable/useful to the applicant, the applicant may propose and the City may consider through a conditional development permit, different incentives including reduced setbacks, increased height limits, or reduced landscape requirements.

Incentives will be considered for the following features:

- ❖ **Shared parking.** Projects that consolidate and combine individual parking lots into shared parking facilities are eligible to either reduce the overall parking requirement by up to 10 percent or increase the allowable floor area ratio by 2 percent. Reciprocal parking, access and circulation easements are executed by owners of contiguous lots subject to approval by the Development Review Committee and Engineering Division.
- ❖ **Lot consolidation.** Larger lots are desired in the Employment and Business Center land use categories. As an incentive to achieve larger lots, the City shall consider awarding a floor area ratio bonus, as described in Table 3-10, for each lot combined when the consolidation exceeds 5 acres. Lot consolidations that result in a parcel that exceeds 5 acres would be entitled

## Development Criteria

to receive a 1 percent FAR bonus for each acre of incremental growth up to 25 acres.

**Table 3-10**  
**Density Bonus Incentives for Lot Consolidations**

Acres	Employment			Business Center		
	Min. FAR	Bonus FAR	Max FAR	Min. FAR	Bonus FAR	Max FAR
0-5	0.35	0	0.35	0.45	0	0.45
6	0.35	0.01	0.36	0.45	0.01	0.46
7	0.35	0.02	0.37	0.45	0.02	0.47
8	0.35	0.03	0.38	0.45	0.03	0.48
9	0.35	0.04	0.39	0.45	0.04	0.49
10	0.35	0.05	0.4	0.45	0.05	0.5
11	0.35	0.06	0.41	0.45	0.06	0.51
12	0.35	0.07	0.42	0.45	0.07	0.52
13	0.35	0.08	0.43	0.45	0.08	0.53
14	0.35	0.09	0.44	0.45	0.09	0.54
15	0.35	0.1	0.45	0.45	0.1	0.55
16	0.35	0.11	0.46	0.45	0.11	0.56
17	0.35	0.12	0.47	0.45	0.12	0.57
18	0.35	0.13	0.48	0.45	0.13	0.58
19	0.35	0.14	0.49	0.45	0.14	0.59
20	0.35	0.15	0.5	0.45	0.15	0.6
21	0.35	0.16	0.51	0.45	0.16	0.61
22	0.35	0.17	0.52	0.45	0.17	0.62
23	0.35	0.18	0.53	0.45	0.18	0.63
24	0.35	0.19	0.54	0.45	0.19	0.64
25	0.35	0.2	0.55	0.45	0.2	0.65

- ❖ **Reciprocal driveway access.** Projects that consolidate and combine driveway access to minimize conflicts with traffic are eligible to either reduce the overall parking requirement by up to 10 percent or increase the allowable floor area ratio by 2 percent. To receive this incentive, the applicant shall enter into an irrevocable reciprocal driveway easement(s) with adjacent properties. The location and width of the accessways shall be reviewed and approved by the Traffic Engineer. The legal instrument shall be approved by the Planning Department and the City Attorney as to form and content and, when approved, shall be recorded in the Office of the County Recorder prior to final building permit approval. The recorded agreement shall remain in effect in perpetuity, except as modified or rescinded pursuant to the expressed written approval of the City.

- ❖ ***Pedestrian building orientation.*** Projects that orient the parking in the rear of the lot and orient the main entrance of the building toward the public sidewalk are eligible to either receive an increase of up to 2 percent in floor area ratio or reduce the overall parking requirement by up to 5 percent. The height limits and setbacks may be adjusted up to 10 percent to accommodate the increased floor area.
- ❖ ***Public plaza.*** Projects adjacent to intersections that include a public plaza are eligible to receive an increase in floor area ratio. Projects are eligible to receive an increase of 2 square feet in floor area for every square foot of public plaza that exceeds a minimum area of 625 square feet (no dimension less than 25 feet) and is adjacent to and accessible from the public sidewalk. The height limits and setbacks may be adjusted up to 10 percent to accommodate the increased floor area.
- ❖ ***Public art.*** Projects that provide permanent, outdoor art that is viewable by the public from the public sidewalk are eligible to either receive an increase of up to 2 percent in floor area ratio or reduce the overall parking requirement by up to 2 percent. The height limits and setbacks may be adjusted up to 10 percent to accommodate the increased floor area.
- ❖ ***Sustainable features.*** Projects that achieve a LEED certification (or equivalent, measurable, and verifiable rating system) are eligible to either receive an increase of up to 2 percent in floor area ratio or reduce the overall parking requirement by up to 2 percent. The height limits and setbacks may be adjusted up to 10 percent to accommodate the increased floor area. See the Interpretation provisions of Section 7, *Implementation*, for a discussion of a process to resolve potential barriers to achievement of LEED certification.
- ❖ ***Landmark intersection features.*** Projects adjacent to intersections of arterial or collector roadways that provide a unique landmark that helps to create identity and place are eligible to receive an increase in floor area ratio. Projects that provide features such as a unique corner building treatment (e.g. tower, corner cut-off), entries or monuments (as identified on Figure 4-1), fountain, or other similar features are eligible to either receive an increase of up to 2 percent in floor area ratio or reduce the overall parking requirement by up to 2 percent. The height limits and setbacks may be adjusted up to 10 percent to accommodate the increased floor area.
- ❖ ***City participation.*** The City/Redevelopment Agency will consider participating in developments to acquire land, reduce development costs, and mitigate entitlement risk. The City/Redevelopment Agency may also consider contributing toward the cost of public improvements imposed as conditions of approval in the event that lots are consolidated such that they exceed the minimum sizes listed on Table 3-5 or if blighted or underutilized properties are demolished to facilitate new development that

## Development Criteria

complies with the provisions of this Specific Plan. Redevelopment Agency participation requires approval of a development agreement.

### Mobility Plan

Renaissance is designed for more than the automobile; it provides mobility options to allow convenient access to the region and promote a healthy lifestyle. Renaissance is designed with an efficient internal and external circulation system that includes a network of roadways, multiple-use trails, and paseos that provide a range of options for vehicular, pedestrian, bicycle, and transit mobility.

#### *Vehicular Circulation*

As shown in Figure 3-1, *Vehicular Circulation Plan*, the Renaissance Specific Plan includes a vehicular circulation plan that fits into and complements the existing surrounding roadway system. Similar to the City's surrounding grid-pattern circulation system, Renaissance's roadway system consists of a hierarchy of streets, including arterials, secondary arterials, collectors, local and private streets, and private drives, as described below.

#### *State Route 210*

SR-210 is a Caltrans facility that serves as a new east–west connection between San Bernardino and Los Angeles counties. SR-210 stretches 28.2 miles through the cities of La Verne, Claremont, Upland, Rancho Cucamonga, Fontana, Rialto, and San Bernardino. Through Rialto, SR-210 is a depressed, eight-lane freeway (three lanes plus a carpool lane in both directions) with intersections at Ayala and Alder Avenues, Riverside Drive, and Pepper.

#### *Major Arterials (Figure 3-2 through 3-4)*

Major Arterial roadways are intended to carry large volumes of relatively high-speed traffic between the region, different parts of the City, and Renaissance. The Major Arterials in Renaissance include:

- ❖ Renaissance Parkway
- ❖ Alder Avenue
- ❖ Baseline Road

#### *Secondary Arterials (Figures 3-5 and 3-8)*

Secondary Arterials are smaller than Major Arterials but are extremely important in creating a backbone circulation system. They serve as the primary roadways within Renaissance, carrying the majority of traffic into and throughout the site. The Secondary Arterials in Renaissance include:

- ❖ Ayala Drive
- ❖ Casmalia Street
- ❖ Locust Avenue

- ❖ Miro Way
- ❖ Linden Avenue

### ***Residential Collectors (Figures 3-9 and 3-10)***

Residential Collectors are used by local traffic within the Village. These two-lane roads are intended to allow slow-speed traffic and on-street parking. Residential Collectors are designed to complement the character of surrounding uses. The circulation plan and street layout within the Village may be modified during the tract map process with approval of the Development Review Committee and Traffic Engineer.

### ***Commercial Collectors (Figure 3-11)***

Commercial Collectors are used by local traffic within commercial areas. These two-lane roads are intended to provide access to businesses and provide on-street parking. Commercial Collectors include:

- ❖ Laurel Avenue
- ❖ Maple Avenue
- ❖ Tamarind Avenue
- ❖ Palmetto Avenue
- ❖ Walnut Street

### ***Local Streets (Figure 3-12)***

While not shown on the Circulation Plan, Local Streets will be developed within each residential and business neighborhood. The circulation plan and street layout within the Village may be modified during the tract map process with approval of the Development Review Committee and Traffic Engineer.

### ***Private Drives***

Private drives may be found in some residential neighborhoods depending upon the type of product. Private drives help to create a more attractive street scene in the front by moving the car to the rear of the building, which breaks the monotony of front-entry garage products with driveways along the street. Private drives are not depicted on the Vehicular Circulation Plan, as they will be the function of individual projects and approved during the tract map process.

### ***Gated Entries***

Gated entries are permitted in Renaissance. Potential locations are shown on Figure 3-1. The locations, number, and design of gated entries may be modified based upon the final circulation patterns in the Village, precise site design, plotting, and construction documents with ministerial approval from the Development Review Committee. A conceptual section and site plan of a Major Gateway, which may be used to mark the primary entry into the Village, are shown on Figure 3-13.

## Development Criteria

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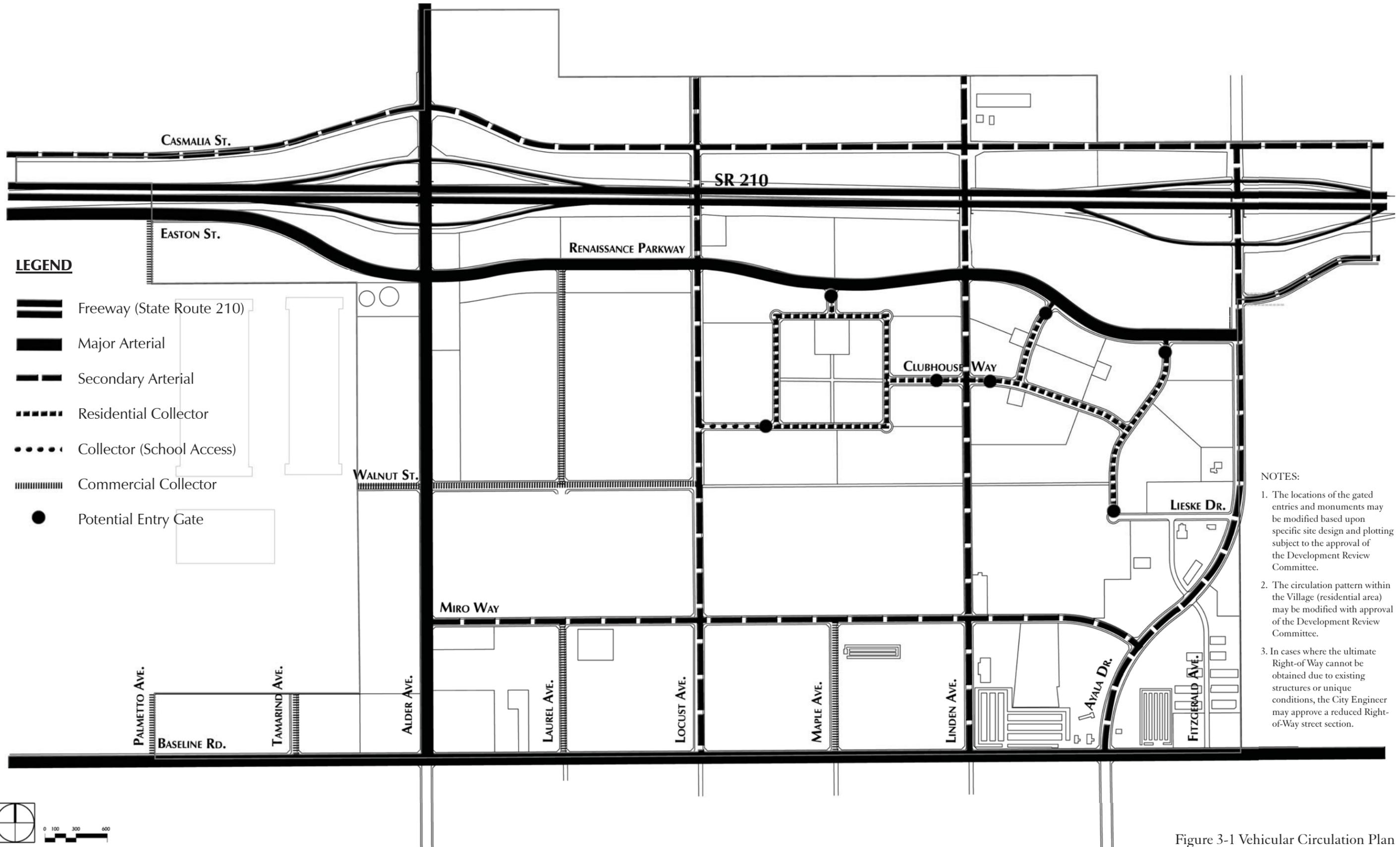
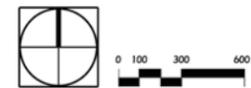
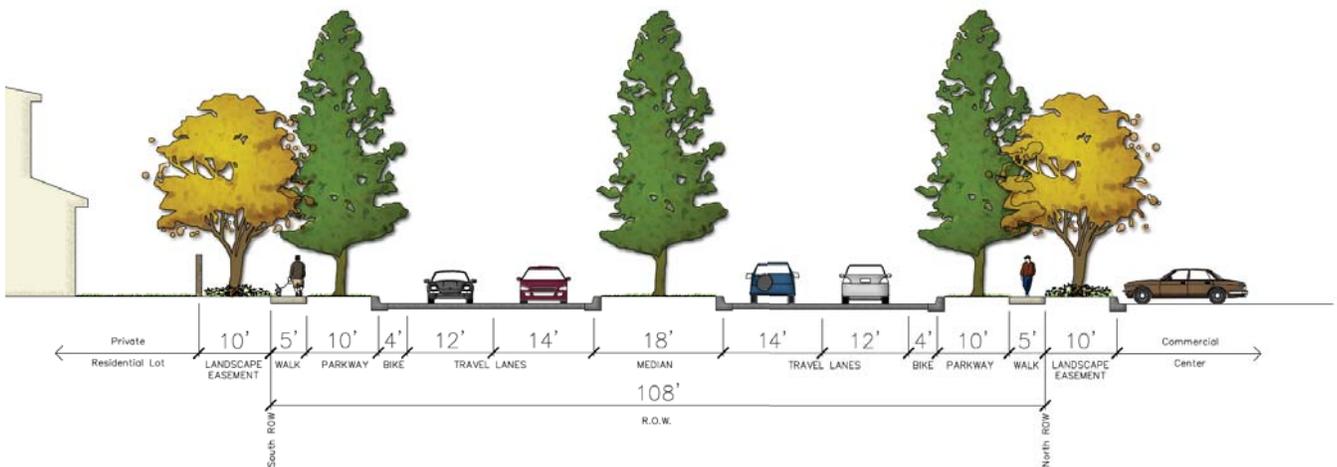
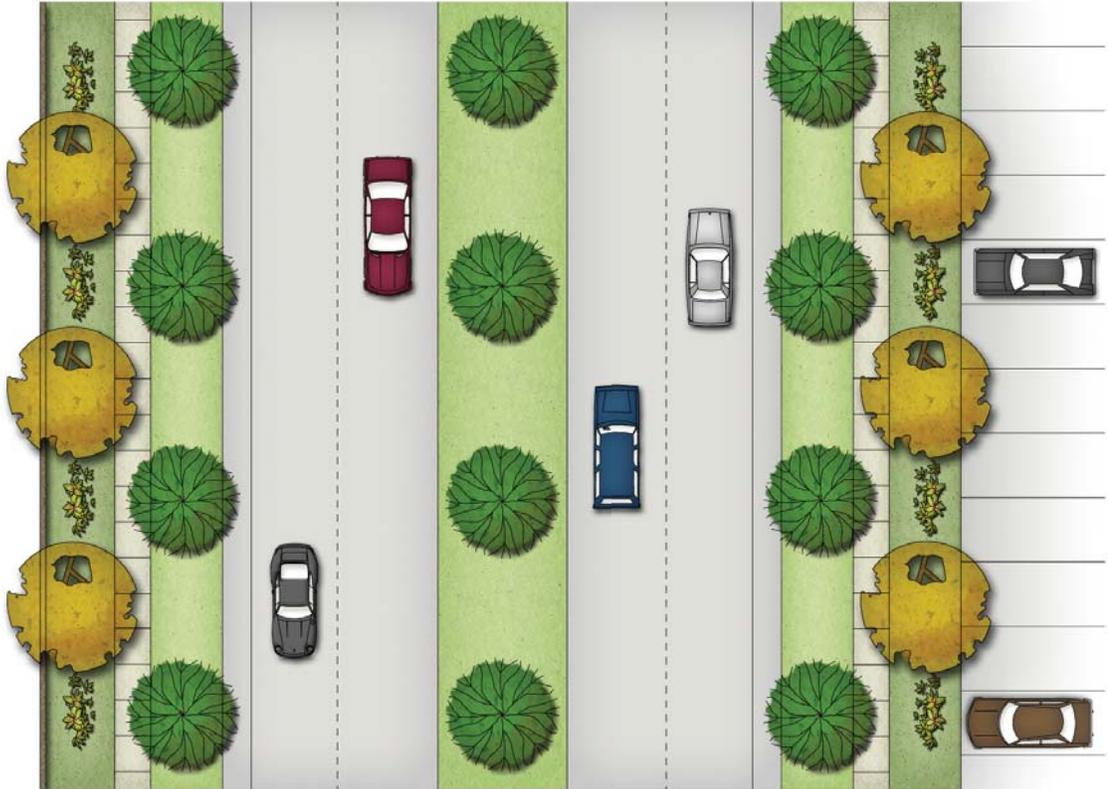


Figure 3-1 Vehicular Circulation Plan



## Development Criteria

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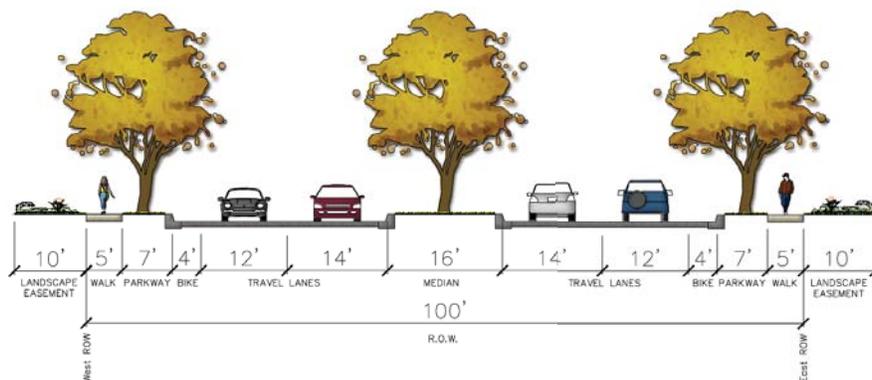
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-2 Renaissance Parkway

## Development Criteria

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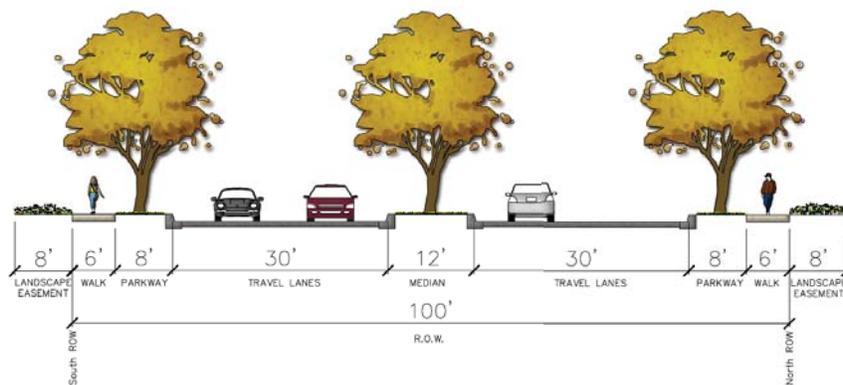
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-3 Alder Avenue

## Development Criteria

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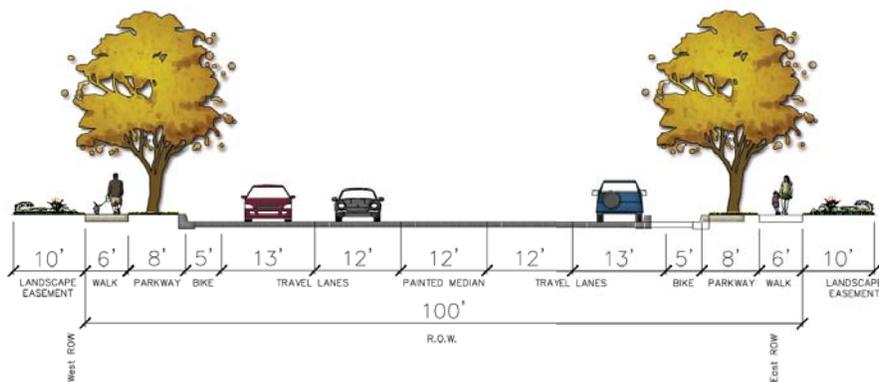
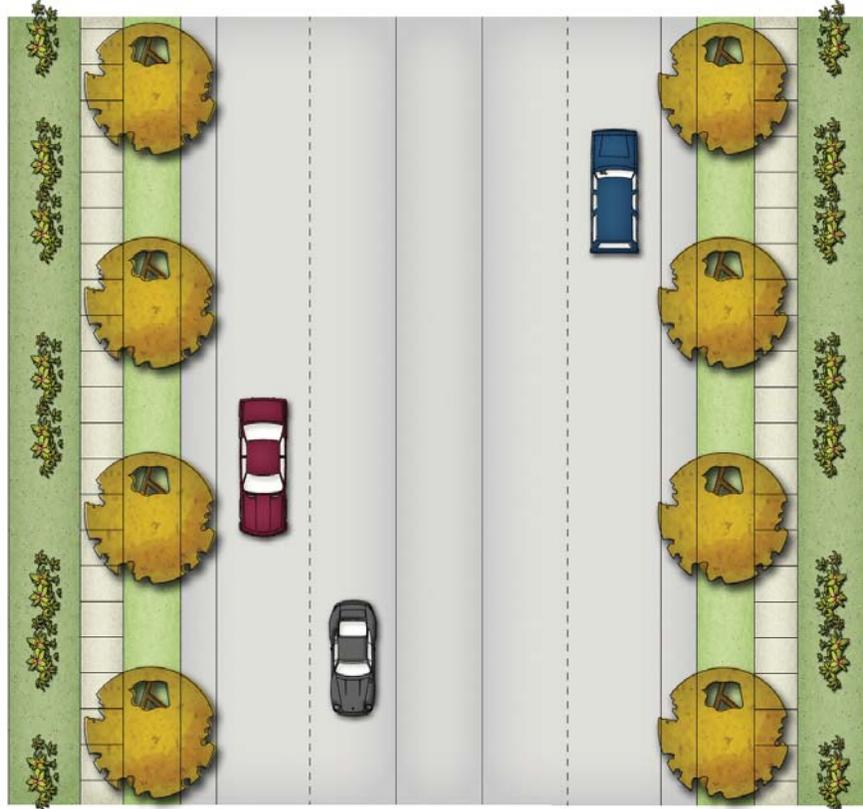
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-4 Baseline Road

## Development Criteria

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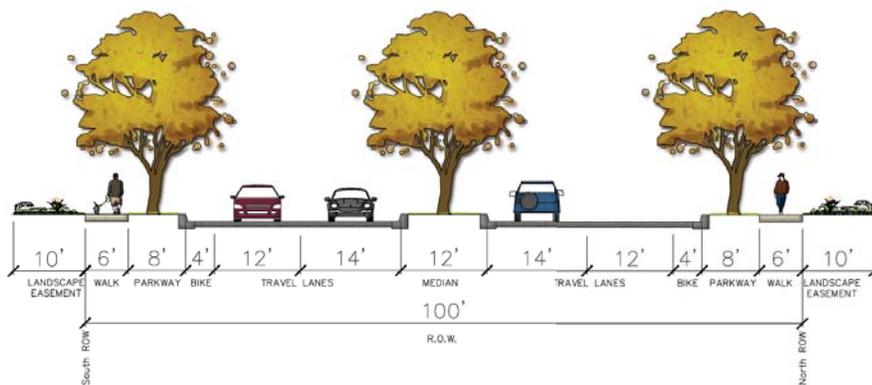
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-5 Secondary Arterial

## Development Criteria

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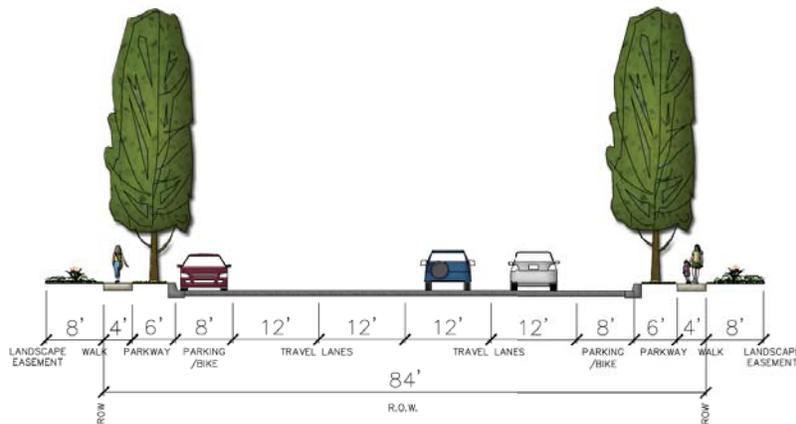
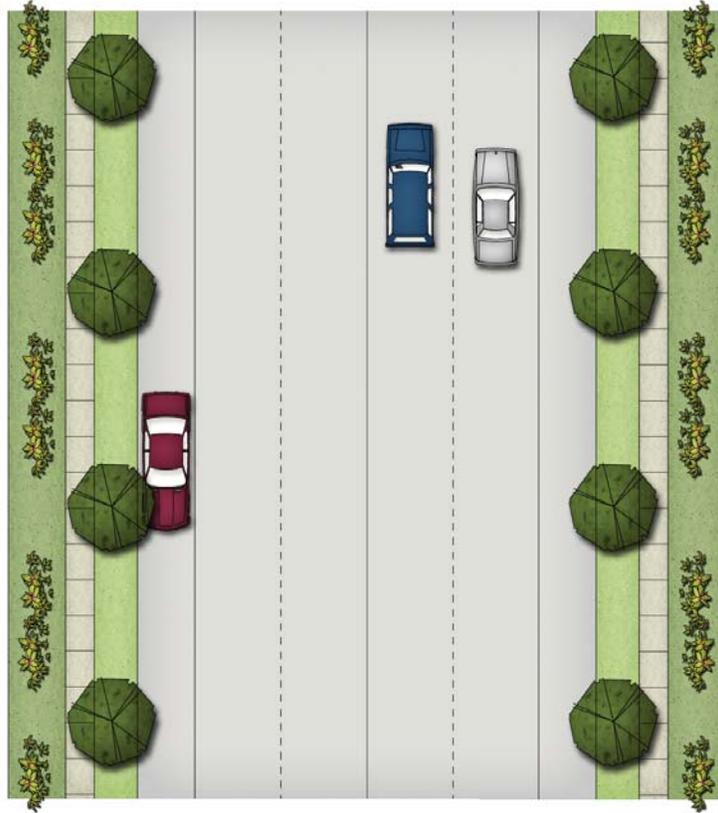
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-6 Secondary Arterial  
(Casmalia St.)

## Development Criteria

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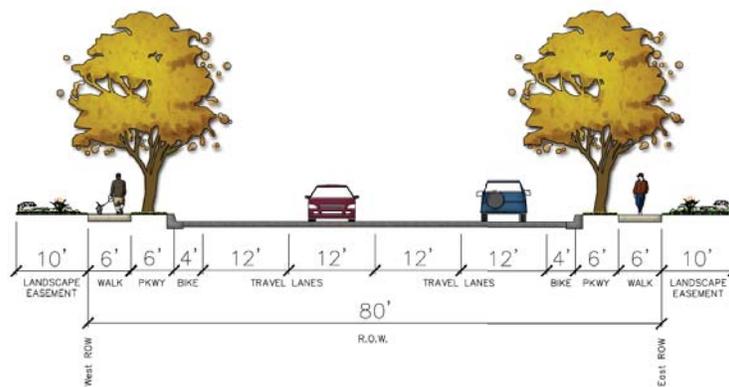
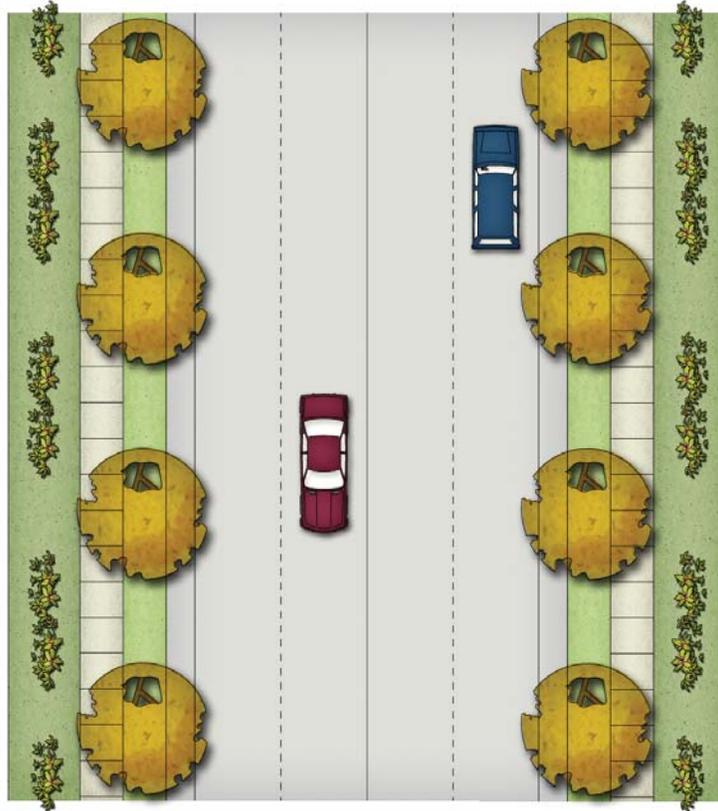
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-7 Secondary Arterial  
(Miro St. & Locust Ave.)

## Development Criteria

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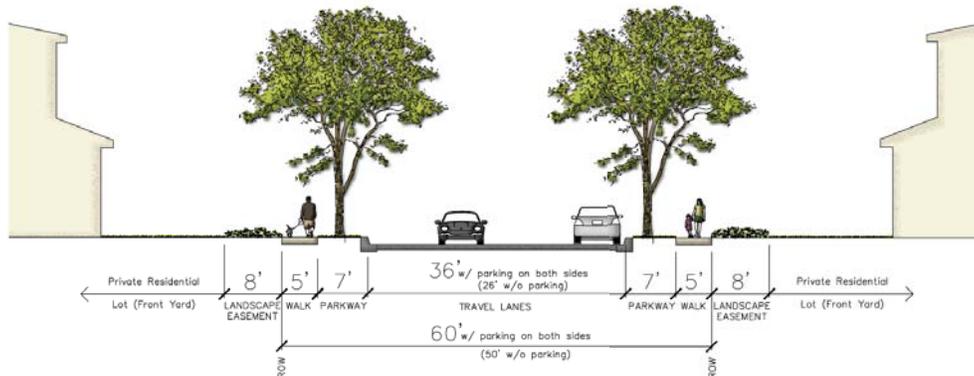
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-8 Secondary Arterial  
(Linden Ave.)

## Development Criteria

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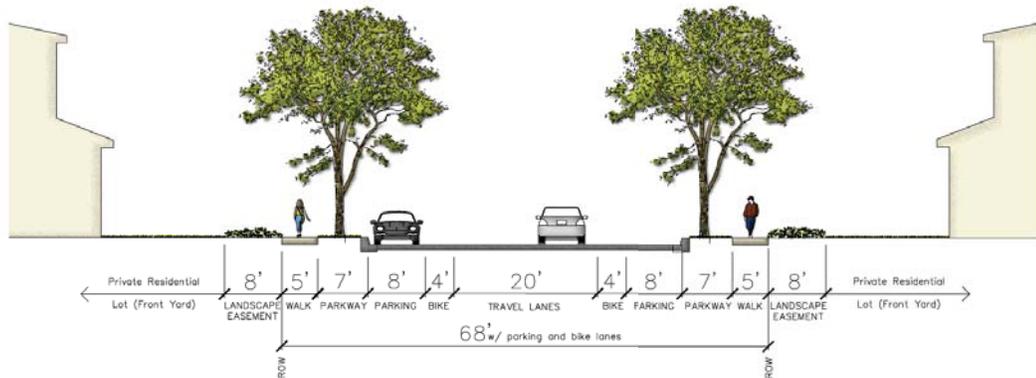
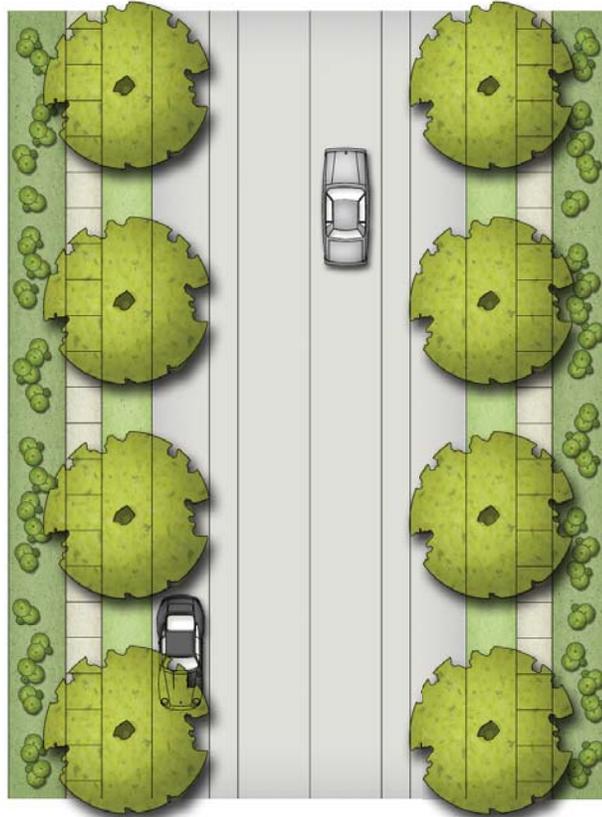
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-9 Residential Collector (Typical)

## Development Criteria

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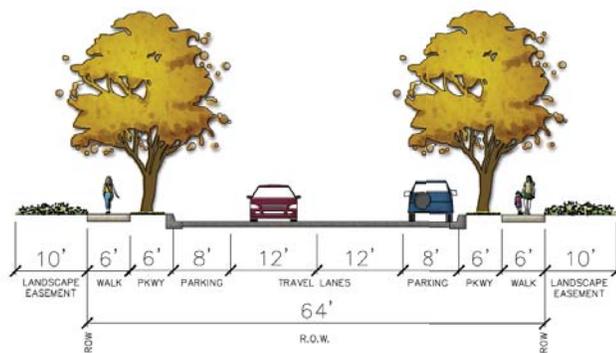
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-10 Collector (School Access)

## Development Criteria

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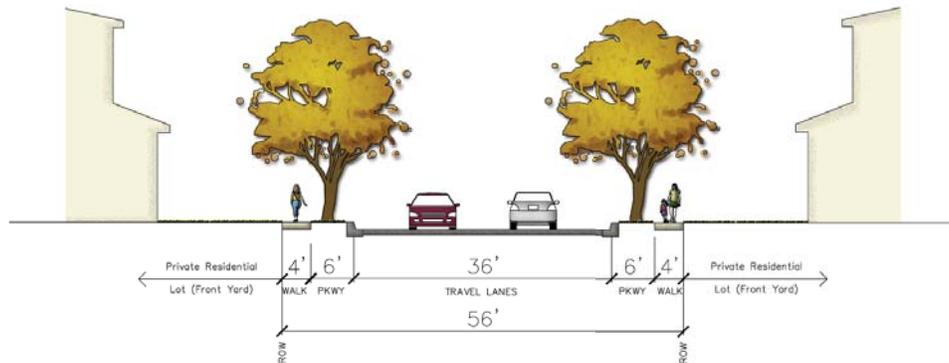
NOTES:

1. In cases where the ultimate Right-of-Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.
3. Setbacks measured from the back of landscape easement.
4. Meandering sidewalks may be utilized on this street. A minimum separation of 5 feet must be maintained between the sidewalks edge and back of curb and back of landscape easement.

Figure 3-11 Commercial Collector

## Development Criteria

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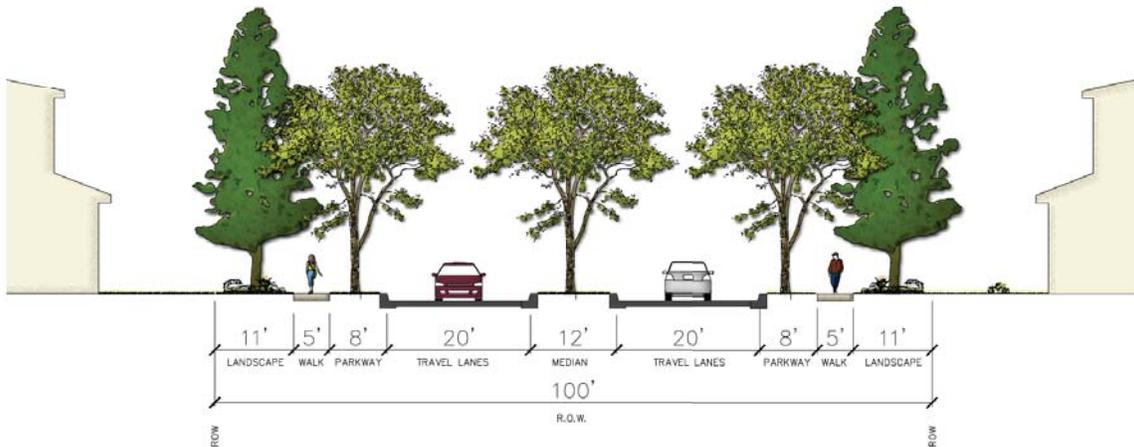
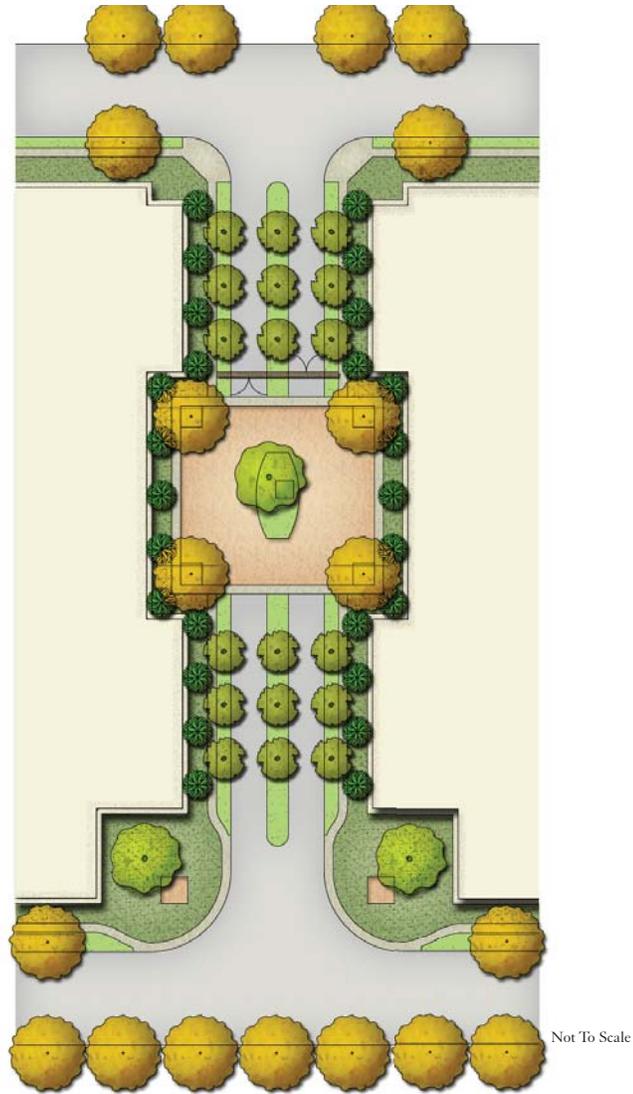
NOTES:

1. In cases where the ultimate Right-of Way cannot be obtained due to existing structures or unique conditions, the City Engineer may approve a reduced Right-of-Way street section.
2. Standard street sections do not apply at intersections. Modifications are necessary to maintain the City's level of service goals and accommodate features such as turn lanes and deceleration lanes.

Figure 3-12 Local Street (Typical)

## Development Criteria

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NOTE: Location to be determined.

Figure 3-13 Major Gated Entry

## Development Criteria

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### ***Truck Routes***

The truck routes in and around Renaissance are shown on Figure 3-14, *Truck Routes*. The truck routes in Renaissance are intended to direct on-site truck traffic westward to Alder Avenue and away from residential areas.

Truck traffic is accommodated along Baseline Road, Alder Avenue, Ayala Drive, Laurel Avenue, Walnut Street, and Miro Way. All roadways south of Miro Way and north of Renaissance Parkway accommodate truck traffic. Due to the interface of business and residential uses, the following standards apply to truck routes:

- ❖ On-site truck traffic will be directed to Alder Avenue through the use of directional signage at business driveways.
- ❖ Truck trips are restricted to local deliveries (no through-trips) on: 1) those portions of Locust and Linden Avenues and Renaissance Parkway that are adjacent to residentially designated areas, and 2) to Baseline Road, Casmalia Street, and Renaissance Parkway west of Alder Avenue.
- ❖ Truck access to Locust Avenue from Planning Area 24 is restricted, as described in Table 3-9.
- ❖ Developments along Renaissance Parkway, Locust Avenue, Linden Avenue, and Ayala Drive that abut residentially designated properties shall comply with the design provisions noted in Table 3-9.
- ❖ In cases where two truck routes intersect, a minimum 35 foot curb corner radius shall be used.

### ***Pedestrian and Bicycle Circulation***

An objective for Renaissance is to create a lifestyle that promotes walking and bicycling and enhances the community's health and wellness. Accordingly, Renaissance includes an extensive pedestrian system, as shown in Figure 3-15, *Bicycle and Pedestrian Circulation Plan*, which is composed of both public and private on- and off-street facilities.

All pedestrian facilities shall be designed to meet ADA and City requirements.

The pedestrian and bicycle circulation system joins important community features, such as the Town Center, clubhouse, school, and parks. This promotes safe nonvehicular movement throughout Renaissance and adjacent uses. If the paseos on the western and southern edges of the Village are private, access-ways shall provide controlled pedestrian and bicycle access. As depicted on Figure 3-15, Renaissance includes pedestrian-only access-ways at the terminus of the private paseos and shared vehicular and pedestrian access-ways at major entries.

## Development Criteria

Bicycle circulation in Renaissance may occur as follows.

- ❖ Class I: off-road multipurpose trails within the paseos.
- ❖ Class II: striped on-street bike lanes, as shown on the street sections
- ❖ Class III: nonstriped, on-street bike routes.

### *Transit*

Omnitrans is the transit provider in Rialto and Renaissance. Omnitrans is a joint-powers authority governed by a 20-member Board of Directors representing the County of San Bernardino and the 15 cities Omnitrans serves. Currently, one Omnitrans bus line provides service to Renaissance. Route 10 operates along Baseline Road and provides connections between downtown San Bernardino and the Metrolink station in Fontana. Approximately 2 miles east of Renaissance at Riverside Avenue, riders can connect with Route 22, which provides connections between north and south Rialto. From Route 22, riders can board the Metrolink at the Rialto station.

While it is not within the control of the City or developer to dictate the location of future Omnitrans transit routes and stops, it is believed that, at buildout of Renaissance, expanded transit service would be merited within the Specific Plan. Ideally, transit service would be provided in the Town Center/Village area, Corporate Center, as well as to areas designated as Employment and Business Center. As bus routes change and development occurs in Renaissance, the accommodation of transit lines shall be coordinated with Omnitrans. In addition, any bus stops located within Renaissance shall be designed to complement the surrounding architecture and landscaping, the Renaissance design guidelines, and comply with the Omnitrans Official Bus Stop Guidelines.

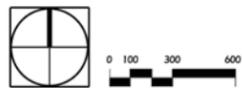
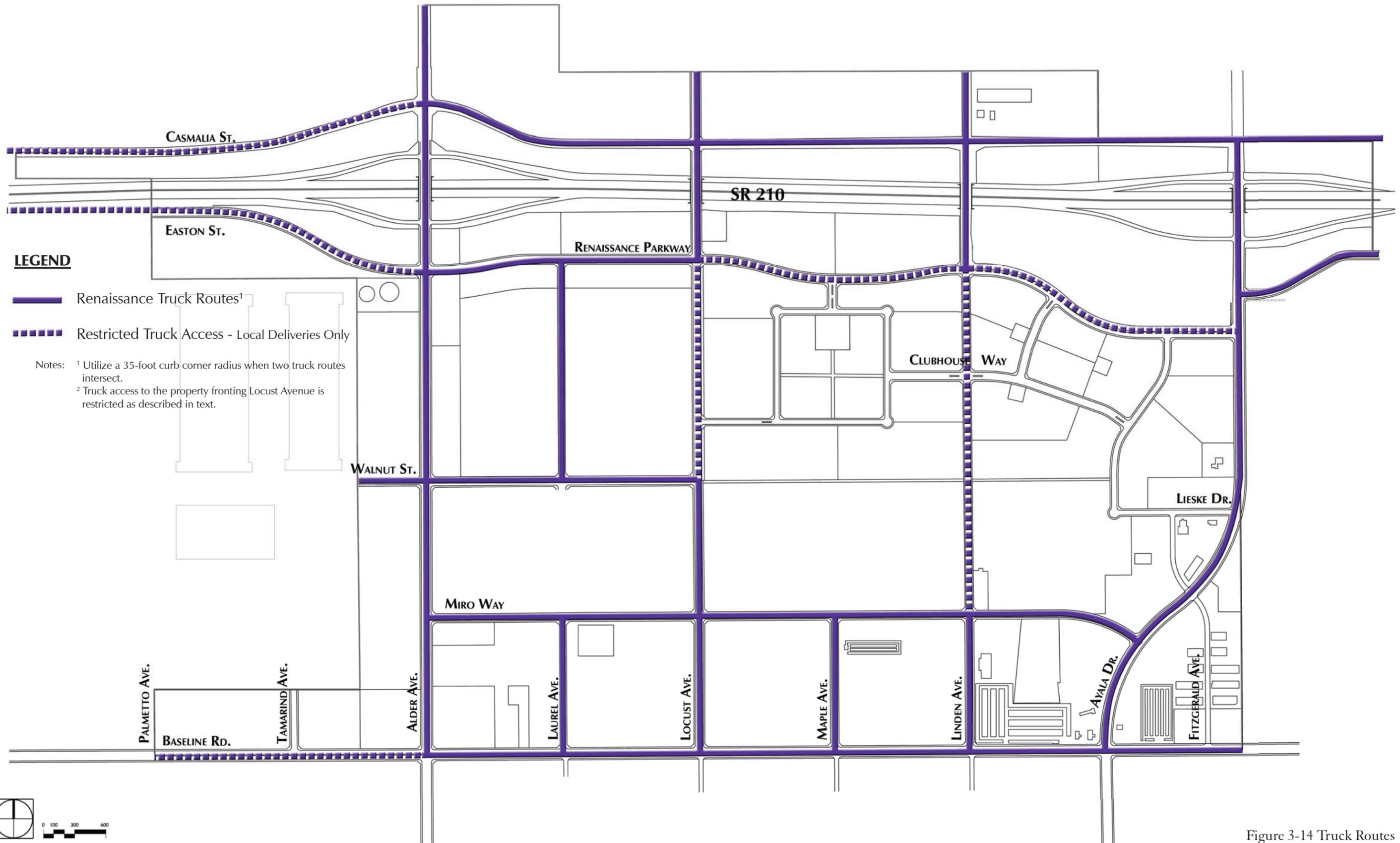
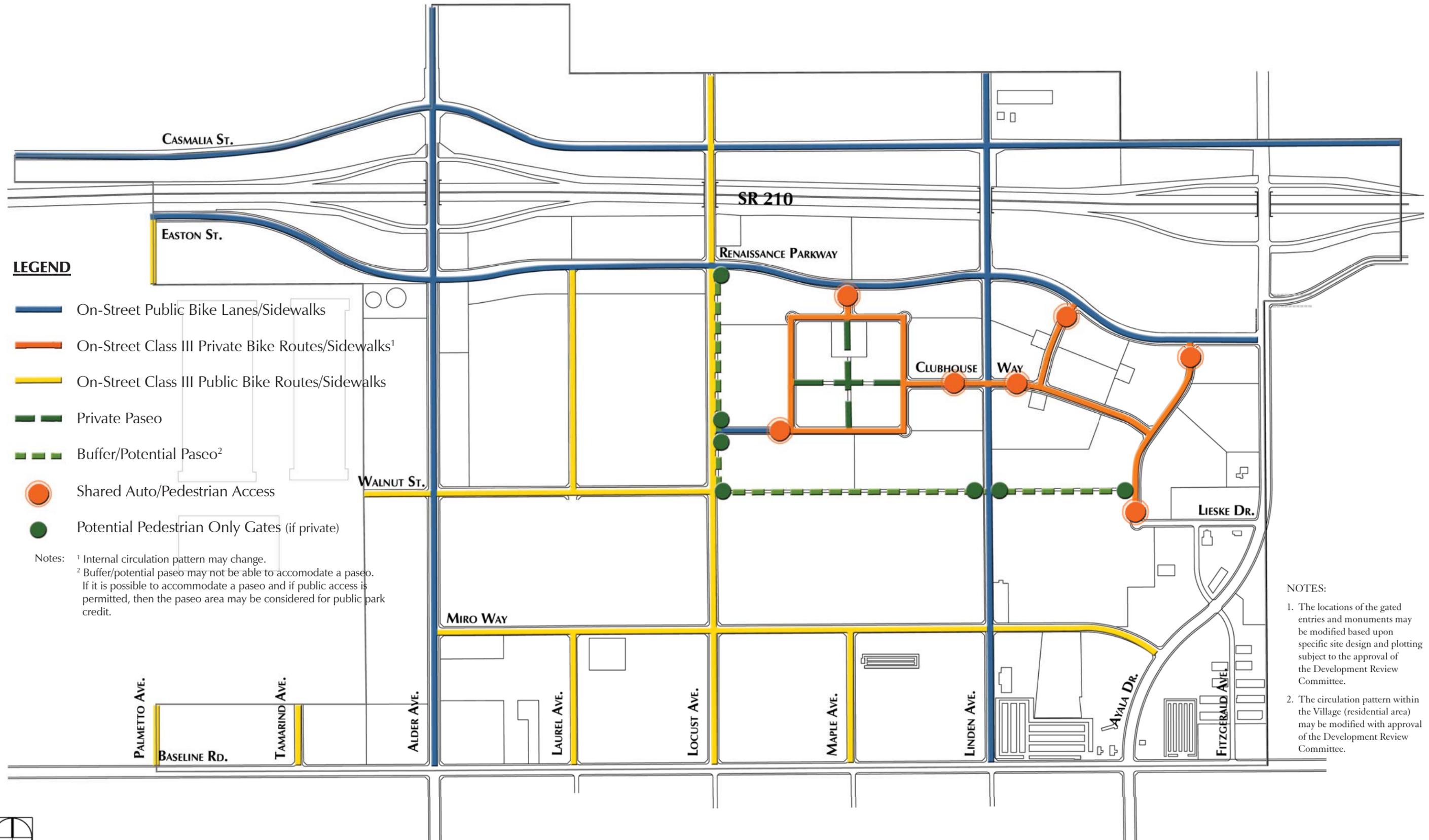


Figure 3-14 Truck Routes

## Development Criteria

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**LEGEND**

- On-Street Public Bike Lanes/Sidewalks
- On-Street Class III Private Bike Routes/Sidewalks<sup>1</sup>
- On-Street Class III Public Bike Routes/Sidewalks
- - - Private Paseo
- - - Buffer/Potential Paseo<sup>2</sup>
- Shared Auto/Pedestrian Access
- Potential Pedestrian Only Gates (if private)

Notes: <sup>1</sup> Internal circulation pattern may change.  
<sup>2</sup> Buffer/potential paseo may not be able to accommodate a paseo. If it is possible to accommodate a paseo and if public access is permitted, then the paseo area may be considered for public park credit.

NOTES:  
 1. The locations of the gated entries and monuments may be modified based upon specific site design and plotting subject to the approval of the Development Review Committee.  
 2. The circulation pattern within the Village (residential area) may be modified with approval of the Development Review Committee.

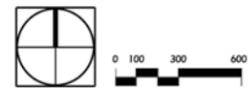


Figure 3-15 Bicycle and Pedestrian Circulation Plan

## Development Criteria

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## Parks and Open Spaces

As shown on Figure 3-16, *Parks and Open Space Plan*, Renaissance includes open spaces that serve multiple functions: as recreational opportunities, as community separators, as buffers, as visual landmarks, as theme setters, and as an interconnecting system of trails. The parks are easily accessible to residents and are located to ensure that all homes are within one-quarter mile of a park.

Renaissance accommodates 1,667 units and a population of approximately 5,167 residents. Based on the City’s standard of 3 acres of parkland per 1,000 residents, full buildout of the Specific Plan would result in the need to provide 15.5 acres of parkland or an equivalent fee in lieu of dedicated parkland. Renaissance provides 16 acres of public parks and 24.7 acres of open space, as summarized in Table 3-11 and further described on the following pages. This total does not include the portion of Jerry Eaves Park, Planning Area 81, which is designated as a public park but developed as a parking lot within the Specific Plan boundary.



*Parks and open space create community gathering areas and help define the character of Renaissance.*

**Table 3-11  
Open Space Summary**

<b>Public Parks <sup>1</sup></b>	<b>Acres</b>
Planning Area 40	16.0
<b>Private Parks and Paseos <sup>1</sup></b>	<b>Acres</b>
Planning Area 49a	0.5
Planning Area 49b	0.5
Planning Area 49c	0.5
Private recreation center (Planning Area 52)	2.6
Paseos (Planning Area 39)	1.4
Planning Area 61a and 61b <sup>2</sup>	2.1
Planning Areas 82a and 82b <sup>3</sup>	1.1
<b>Subtotal Private Parks and Paseos</b>	<b>8.7</b>
<b>Potential Recreation Facilities</b>	<b>Acres</b>
<b>Total</b>	<b>24.7</b>

Notes:

<sup>1</sup> Parks may be relocated and resized; however, an appropriate in-lieu fee, dedication, or improvement must be approved by the City through the Precise Plan of Development or Tract Map processes. Unless specifically credited toward park credits by the City, only public parks may be counted as public park credit.

<sup>2</sup> These Planning Areas may accommodate a paseo. If so, detailed plans shall be approved by the City. These Planning Areas are currently assumed to be private, but if they are approved as public, any potential to count toward park credits will be determined at that time.

<sup>3</sup> These Planning Areas are located within an SCE easement and, if approved by the utility company, may accommodate, landscaping, a paseo, and recreational features such as a tot lot. If so, detailed plans shall be approved by the City. These Planning Areas are currently assumed to be private, but if they are approved as public, any potential to count toward park credits will be determined at that time.

## Development Criteria

It is the expressed direction of this Specific Plan that if the amount of parkland described above is provided, then the common open space requirement for attached residential development, as addressed in Table 3-4, shall be satisfied. In addition, the parks described above may be resized and relocated as long as the total amount of parkland is not decreased without the approval of the Development Review Committee and, if necessary, the payment of an appropriate in-lieu fee. In addition, unless specifically credited toward park credits by the City, only public parks may be counted as public park credit.

### *Renaissance Recreational Facilities*



*One type of active use envisioned for the Public Park.*



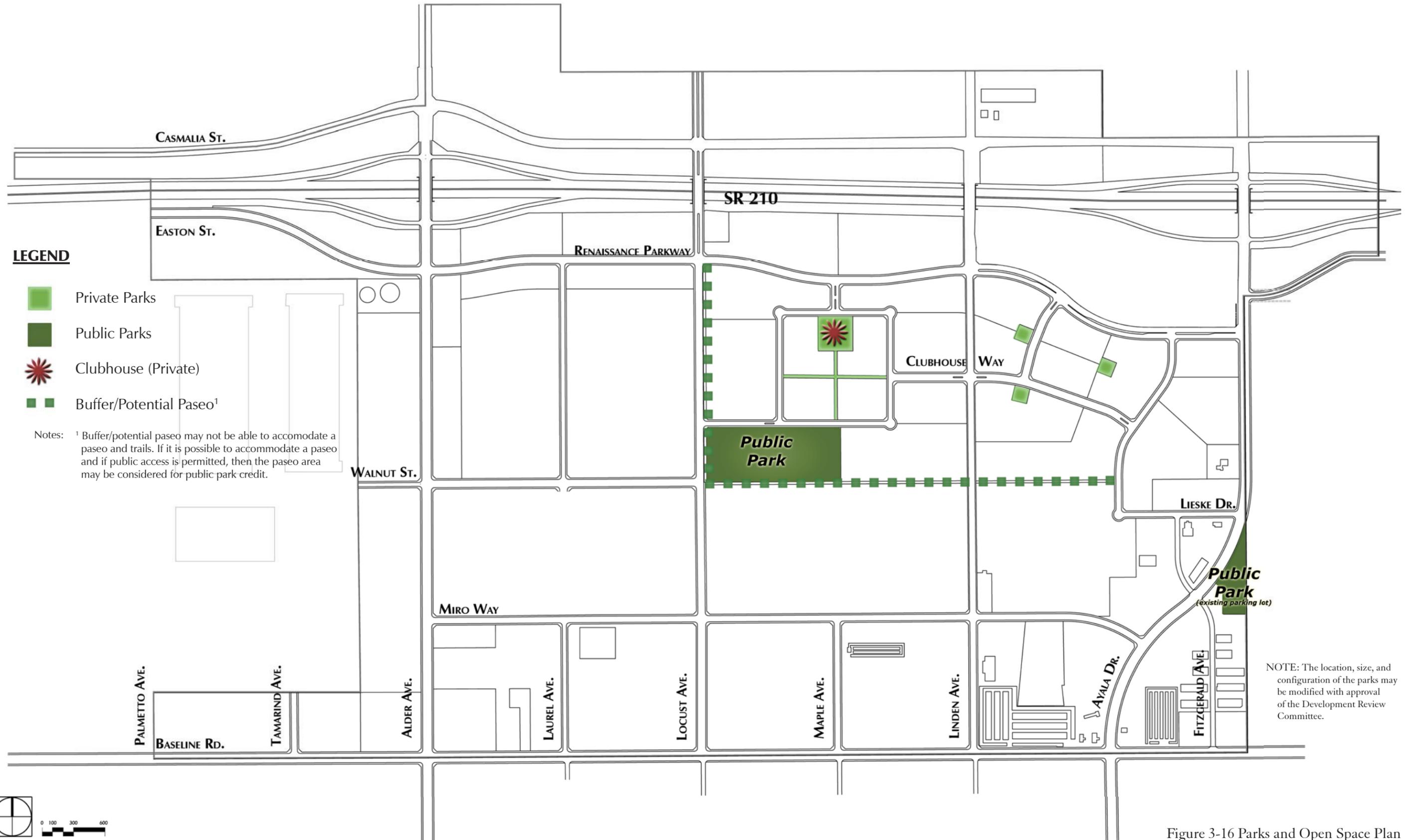
*The private clubhouse will act as an activity center for the residents of Renaissance.*

**Public Park.** Renaissance includes a 16-acre public park on Locust Avenue that may accommodate facilities such as illuminated baseball and soccer fields, basketball courts, an amphitheater, play equipment, open green areas, shaded picnic areas, and may include a community building with information, snacks, community sports activities, and restrooms. The design and final amenities of this park will be determined by the Recreation Department.

Facilities for active recreational uses that are likely to draw cheering crowds, elicit loud play, have amplified game announcements (i.e., stadiums, soccer fields, tennis courts, basketball courts, etc.), or are illuminated for night-time play should be located within the park's interior and away from surrounding residential uses. Lights and speakers shall be directed away from residences to minimize light and glare impacts. Lights shall be hooded, positioned, and directed toward the field of play to minimize light spillover. Any speakers should be located near the ground and distributed evenly to minimize the need for high volume. Speakers should be limited to daylight hours and lights should be turned off by 10:00 p.m.

**Clubhouse.** This 2.6-acre active recreation facility will form the hub of activity for Renaissance. The private clubhouse may include recreational amenities such as a clubhouse, tennis courts, oversized spa, gym, fireplace room, restrooms, meeting rooms, and a junior-Olympic-sized pool. The clubhouse will be designed to create a visual impact at the main entry to Renaissance and parking will be located to minimize exposure from the entrance.

**Private Parks.** The three private, half-acre parks are located within the Village; however, their exact size and location may vary. These parks will be both active and passive and may include amenities such as pools, spas, barbecues, small pool houses for equipment and restrooms/showers, open green area large enough for open play, shade structures with tables and chairs, and gathering areas. To receive park credit, private parks shall be built by the developer per City specifications.



NOTE: The location, size, and configuration of the parks may be modified with approval of the Development Review Committee.

Figure 3-16 Parks and Open Space Plan

## Development Criteria

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***Village Interior Paseos.*** These paseos are located within the interior of The Village and connect residences, parks/open spaces, the clubhouse, and provide separation between neighborhoods. The paseos contain multipurpose (walking/cycling) trails, landscaping, pedestrian-level lighting, and entry and community theme features. In order to be eligible for park credits, paseos should also contain amenities such as play equipment, sports courts (e.g. half court basketball), community gardens, distance markers, benches, and exercise and dog comfort stations.



*Example of the treatment of open space envisioned in Renaissance.*

Paseos in the Village are a minimum of 25 feet in width and are enhanced by 10 foot front setbacks on either side for a total open space area of approximately 45 feet. The width may vary depending upon the layout of the residences. Conceptual depictions of the Paseos can be found in Figures 3-17 and 3-18.

***Buffers/Potential Paseos.*** This Specific Plan also includes the option to develop paseos along the western and southern edges of the Village (Planning Areas 61a, 61b, 82a, and 82b) if they are proven feasible through the final design process. Paseo in these locations would allow a functional use of the space, provide a direct, off-street connection to the school, reduce the need for automobiles, and contribute to the overall health of the residents. The following requirements apply to these features:

- ❖ Along the western edge (Planning Areas 82a and 82b, the paseo may be developed within an Edison power line easement if permitted by the power company.
- ❖ Along the southern edge (Planning Areas 61a and 61b), the paseo may be developed within the buffer between the residences and the adjacent industrial uses and would be a minimum of 30 feet in width. A conceptual depiction of the paseo on the southern edge can be found in Figure 3-19.
- ❖ The buffers/paseos are intended to serve multiple purposes including drainage and utility corridors, graded slope, landscaped buffer, and provide recreational options.
- ❖ Any paseo in these locations shall be approved by the City during the tract map process and shall be accessible to the residents, visible, well-lit, and well maintained.
- ❖ These paseos are currently assumed to be private, but if they are approved as public and to be eligible for park credits, they should contain amenities such as a multipurpose trail, play equipment, sports courts (e.g. half court basketball), community gardens, distance markers, benches, and exercise and dog comfort stations.
- ❖ All landscaping shall be compatible with any utilities and storm drains developed within the paseo.

# Grading and Infrastructure Plans

## *Introduction*

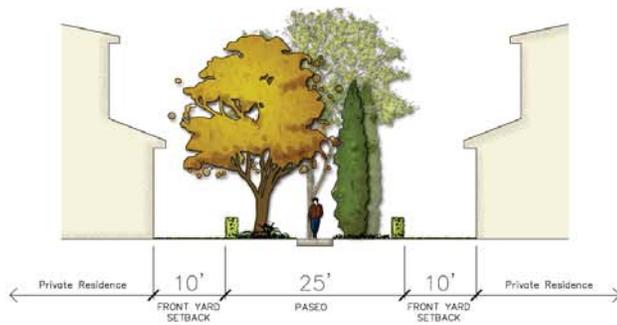
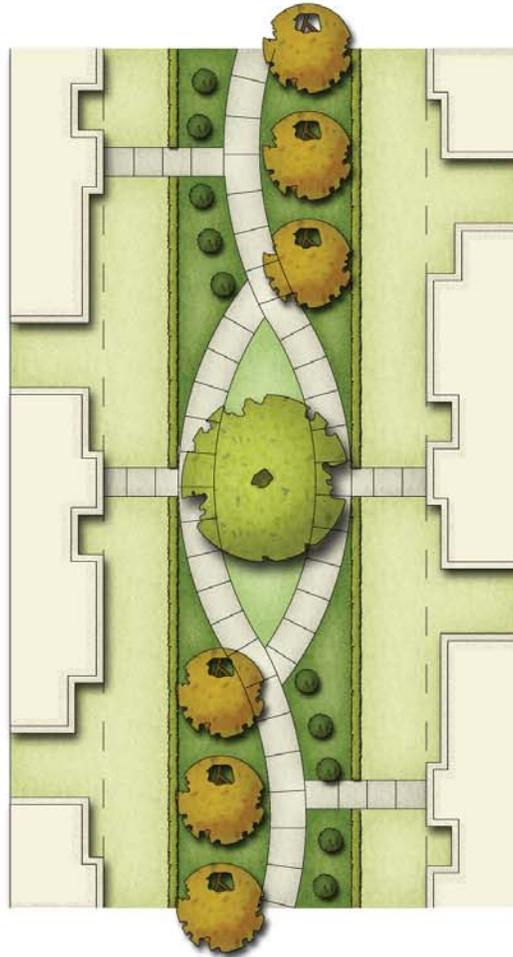
This section details the on- and off-site plans for the grading, potable water, recycled water, drainage, and sewer systems necessary to accommodate build-out of Renaissance. These plans are conceptual in nature and will be refined during the final permitting, grading plan, tract map, and infrastructure plan approval processes.

## *Conceptual Grading Plan*

Renaissance project is underlain by alluvial fan deposits from the Lytle Creek. Based upon a preliminary geotechnical investigation completed by Leighton and Associates, dated November 16, 2005, the site consists of “unconsolidated, gray, sand and silty sand with cobbles and boulders”. The site has a fairly uniform gradient of approximately 2 percent, sloping from the northwest to the southeast. Groundwater is estimated at a depth of 300 feet, so liquefaction is not a consideration for project design. The upper 5 feet is considered compressible. Removal and recompaction of the surface compressible materials will be required during grading. The site is considered to have a very low expansion potential, negligible sulfate exposure, and mild potential for ferrous metal corrosion.

As shown on Figure 3-20, *Conceptual Grading Plan*, the site will be graded at approximately 2 percent from the northwest corner to the southeast corner. The backbone community street grades will be set to match existing topography. Flat graded pads will be established through the grading permit process, but the overall project grading will be in conformance with this conceptual grading plan. The site will be graded to balance onsite. If oversize rocks are encountered, they will either be buried in fill, crushed onsite, or hauled offsite.

Please note that the grading plan is conceptual in nature and is intended to convey major concepts. The grading plan may be adjusted to reflect the final roadway layout, plotting, drainage plans, and design. The grading plan will be finalized during the grading permit process and approved by the City Engineer.

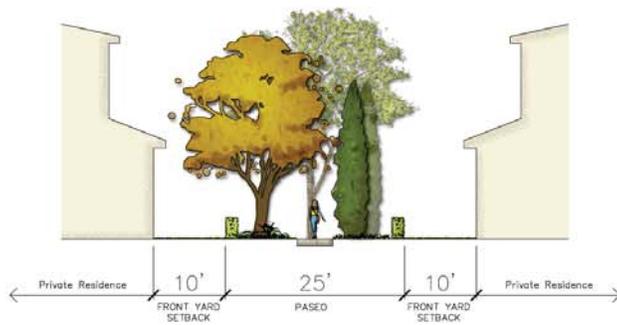
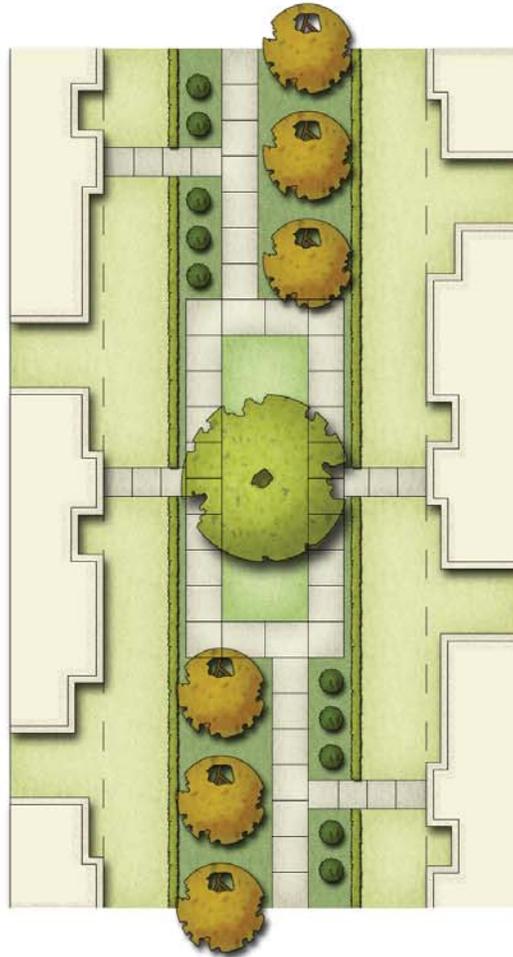


NOTE: Illustration is conceptual in nature and subject to change. Features and alignments will be determined at the time of tract maps.

Figure 3-17 Paseo Alternative 1  
Conceptual Site Plan

## Development Criteria

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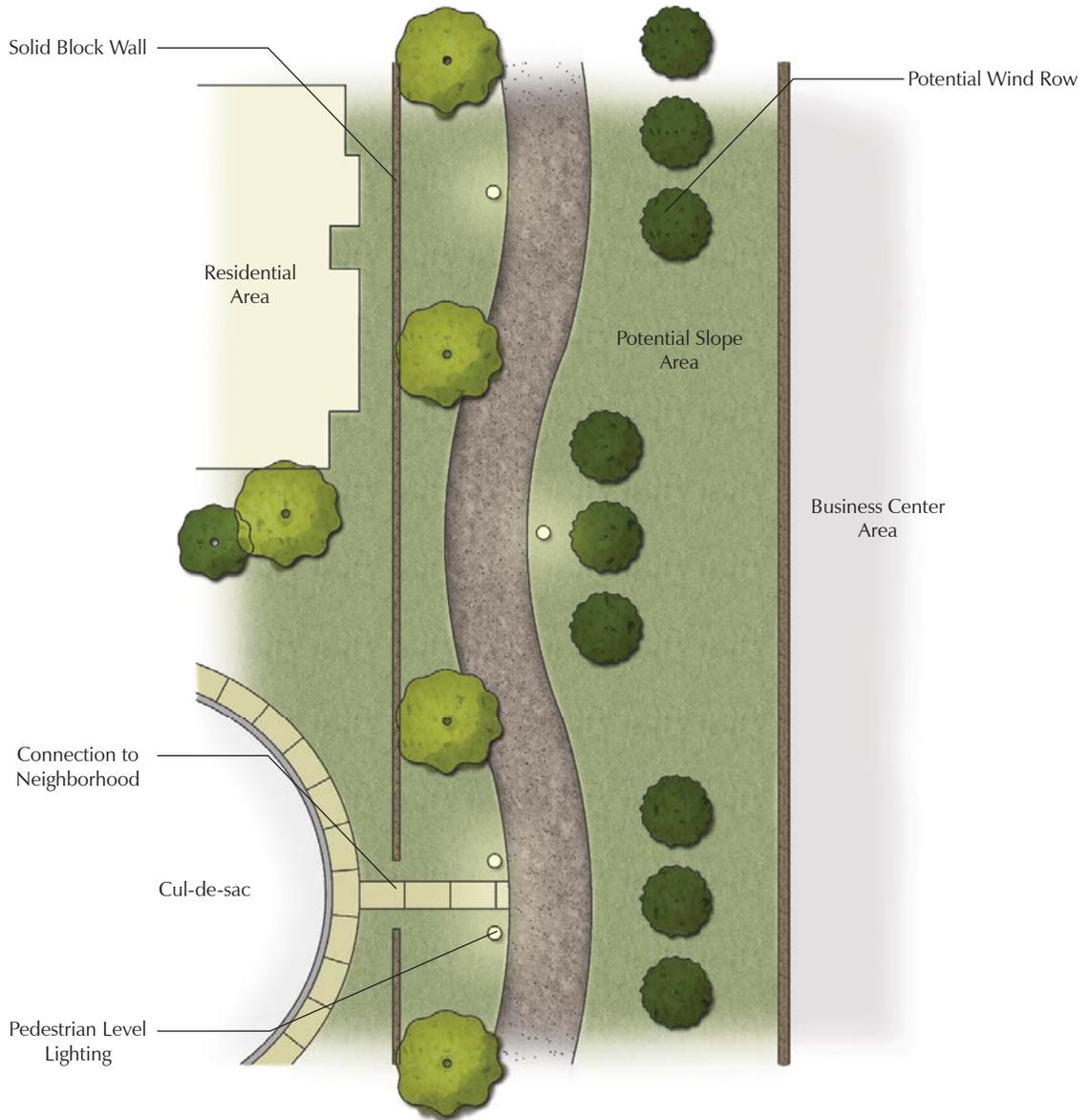


NOTE: Illustration is conceptual in nature and subject to change. Features and alignments will be determined at the time of tract maps.

Figure 3-18 Paseo Alternative 2  
Conceptual Site Plan

## Development Criteria

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NOTE: Illustration is conceptual in nature and subject to change. Features and alignments will be determined at the time of tract maps.

The southern "buffer/potential paseo" shown on Figures 3-12 & 3-13 may be used as a landscaped sloped area, paseo, and/or drainage channel. Final design will be determined through the tract map process.

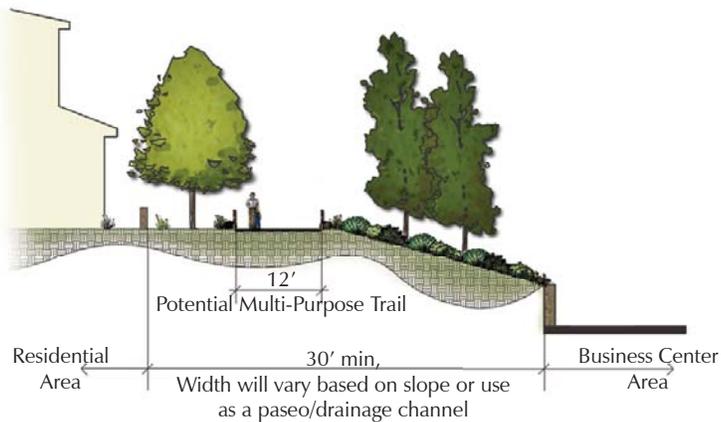
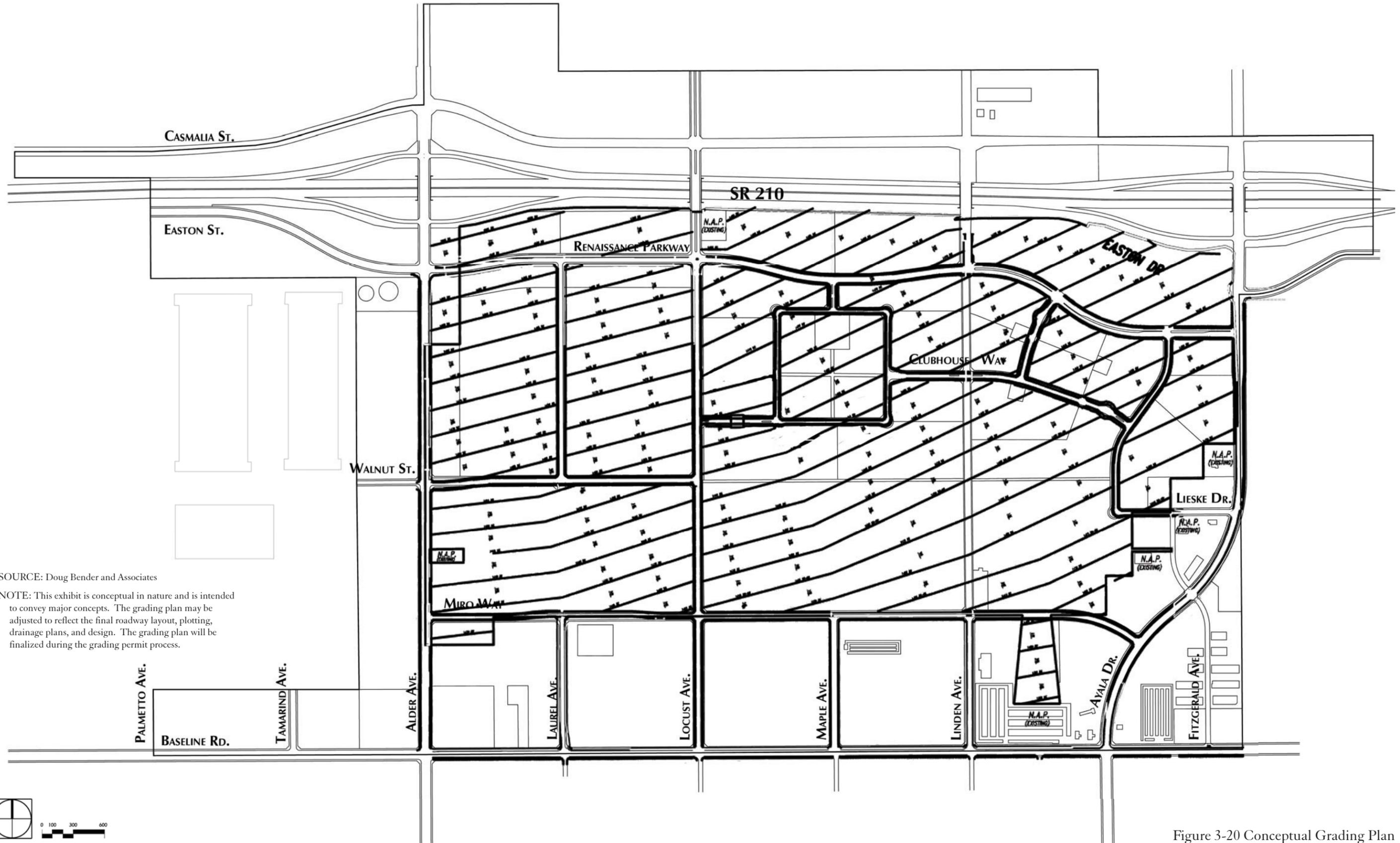


Figure 3-19 Conceptual Buffer/Potential Paseo

## Development Criteria

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SOURCE: Doug Bender and Associates

NOTE: This exhibit is conceptual in nature and is intended to convey major concepts. The grading plan may be adjusted to reflect the final roadway layout, plotting, drainage plans, and design. The grading plan will be finalized during the grading permit process.

Figure 3-20 Conceptual Grading Plan

## Development Criteria

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### ***Conceptual Water Plan***

Renaissance is served by three separate water districts. The area north of I-210 is served by West Valley Water District; the area south of I-210 and west of Linden Avenue is served by the Fontana Water District; and the property south of I-210 and east of Linden Avenue is served by the City of Rialto. There are several existing water lines that traverse the Specific Plan area.

Figure 3-21, *Conceptual Water Plan*, is conceptual in nature and is not intended to show exact locations and alignments of facilities. Infrastructure may be relocated and realigned to follow the final roadway system, plotting, and design. Final design and location will be determined through the tract map and grading permit processes.

### ***Conceptual Drainage Plan***

Renaissance naturally drains from the northwest to the southeast. When I-210 was constructed, an open drainage channel (the Cactus Channel) was constructed on the north side of the freeway. This channel intercepts all runoff within the specific plan area north of the freeway, and outlets into existing San Bernardino Flood Control District Cactus Basin 5.

At the time of the writing of this Specific Plan, the area south of the freeway drains southerly to Baseline Avenue. Baseline Avenue drains easterly toward Cactus Avenue, but currently there are no storm drains in Baseline to intercept site runoff. As shown in the conceptual plan, Figure 3-22, development of Renaissance will require construction of 4 major east west storm drain systems as described below.

- ❖ The most northerly storm drain will be constructed in Renaissance Parkway. This system will intercept flow from development located between the freeway and Renaissance Parkway east of Alder Avenue. This storm drain will be extended southerly in Ayala Drive to its intersection with Fitzgerald.
- ❖ The second east west storm drain will be constructed along the southerly boundary of the residential portion of the development. It will intercept runoff from the residential development south of Renaissance Parkway and east of Locust Avenue. This storm drain will be constructed within the buffer between the residential and area designated as Business Center immediately south of the Village. It will be connected to the storm drain in Ayala Drive that will be extended southerly from Renaissance Parkway.
- ❖ The third east west storm drain will be constructed in Miro Way. It will intercept runoff from the area designated as Business Center located east of Alder Avenue. This storm drain will be extended northerly in Ayala Drive from the Miro Way intersection to Fitzgerald. From there, it will be combined with the storm drain from Renaissance Parkway, and extended

## Development Criteria

easterly through Jerry Eaves Park to Cactus Basin 3<sup>1</sup>. The current plans for Cactus Basin 3 include two 120 inch RCP inlets, which will accept the flow from these storm drains. Either two parallel storm drain pipes will be extended from Ayala Drive to Cactus Basin 3, or a combined single box culvert will be constructed. Cactus Basin 3 has been designed by the County to limit the maximum 100 year outflow to 1250 cfs, which is the capacity of the downstream Rialto Channel.

- ❖ The fourth and most southerly east west storm drain system will be constructed in Baseline Avenue. It will intercept runoff from the area south of Miro Way and north of Baseline Road. This storm drain will outlet into Cactus Basin 2<sup>1</sup>. Development in this area will be required to have temporary detention basins until downstream facilities are completed.

This conceptual plan includes an assumption that all the water quality treatment requirements for the parcels designated as Business Center will be addressed onsite. In addition, it is assumed that the General Commercial, Corporate Center, and Freeway Commercial parcels will also handle their water quality onsite. On-site detention facilities may be permanent or temporary based upon the capacity, status, and connection to downstream drainage facilities.

The storm drainage plan is conceptual in nature and is not intended to show exact locations and alignments of facilities. Infrastructure may be relocated and realigned to follow the final roadway system, plotting, and design. Final design and location will be determined through the tract map and grading permit processes.

### *Conceptual Sewer Plan*

The conceptual sewer plan is shown on Figure 3-23. Within Renaissance, the area north of I-210 will sewer to an existing 18 inch sewer line in Casmalia Street. This sewer drains easterly in Casmalia Street to Ayala Drive, where it connects to an existing 18 inch sewer that crosses under I-210. This sewer continues southerly in Ayala Drive to Fitzgerald, and then southerly in Fitzgerald, where it connects to an existing 24 inch sewer in Baseline Road. The Baseline Road sewer drains easterly to a sewer that continues southerly in Cactus Avenue.

The area south of I-210 will require new sewer lines. As conceptualized, a maximum 15 inch sewer will be constructed in Renaissance Parkway to pick up sewage runoff from the Freeway Commercial, Corporate Center, and retail developments that will be constructed between I-210 and Renaissance Parkway. This sewer will drain easterly to Ayala Drive. Since the existing 18 inch sewer in Ayala is at capacity, a new parallel 15 inch sewer will be constructed to drain southerly in Ayala Drive.

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<sup>1</sup> Discharging flows into Cactus Basin will require approval from the San Bernardino County Flood Control District.



## Development Criteria

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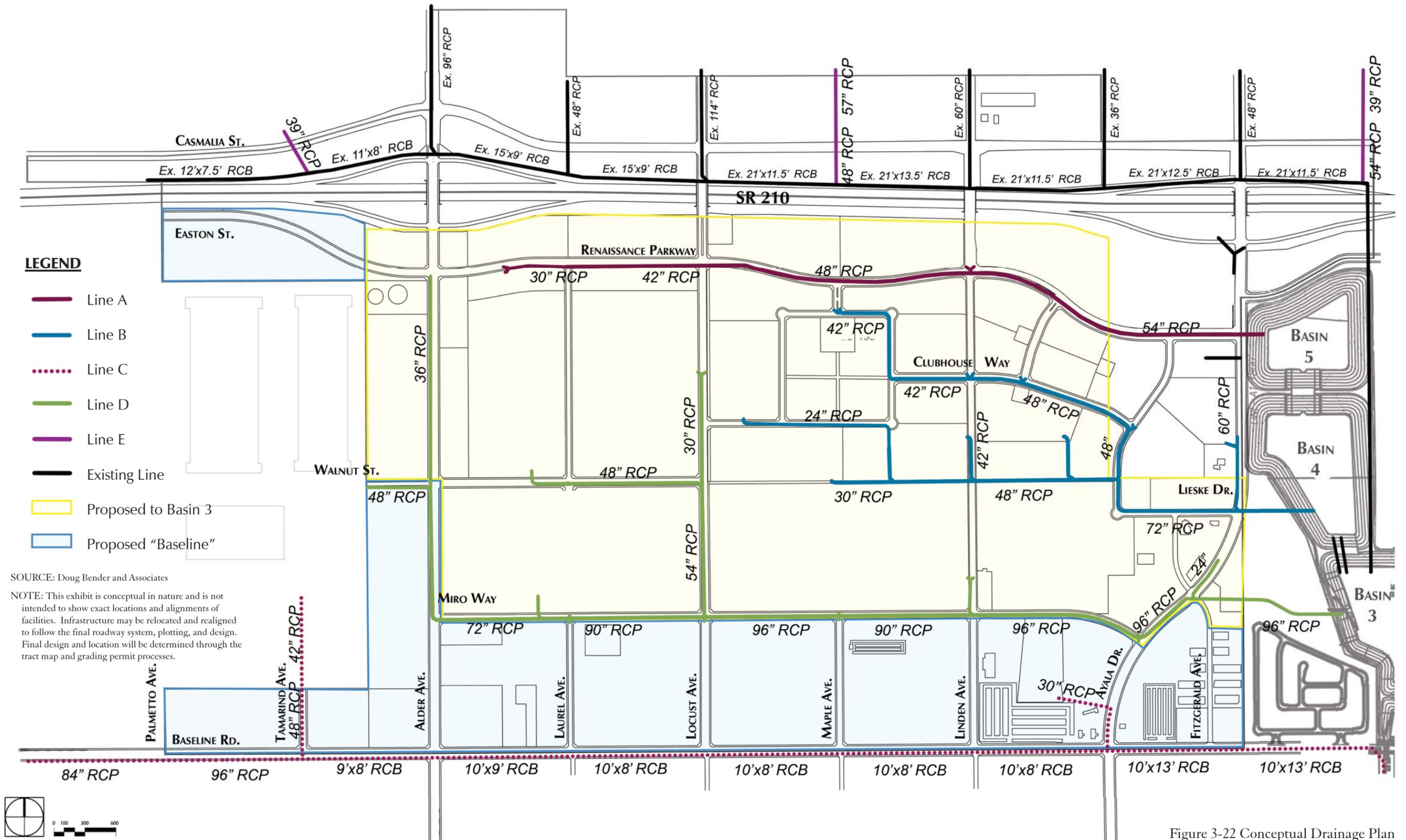
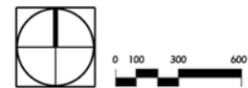


Figure 3-22 Conceptual Drainage Plan



## Development Criteria

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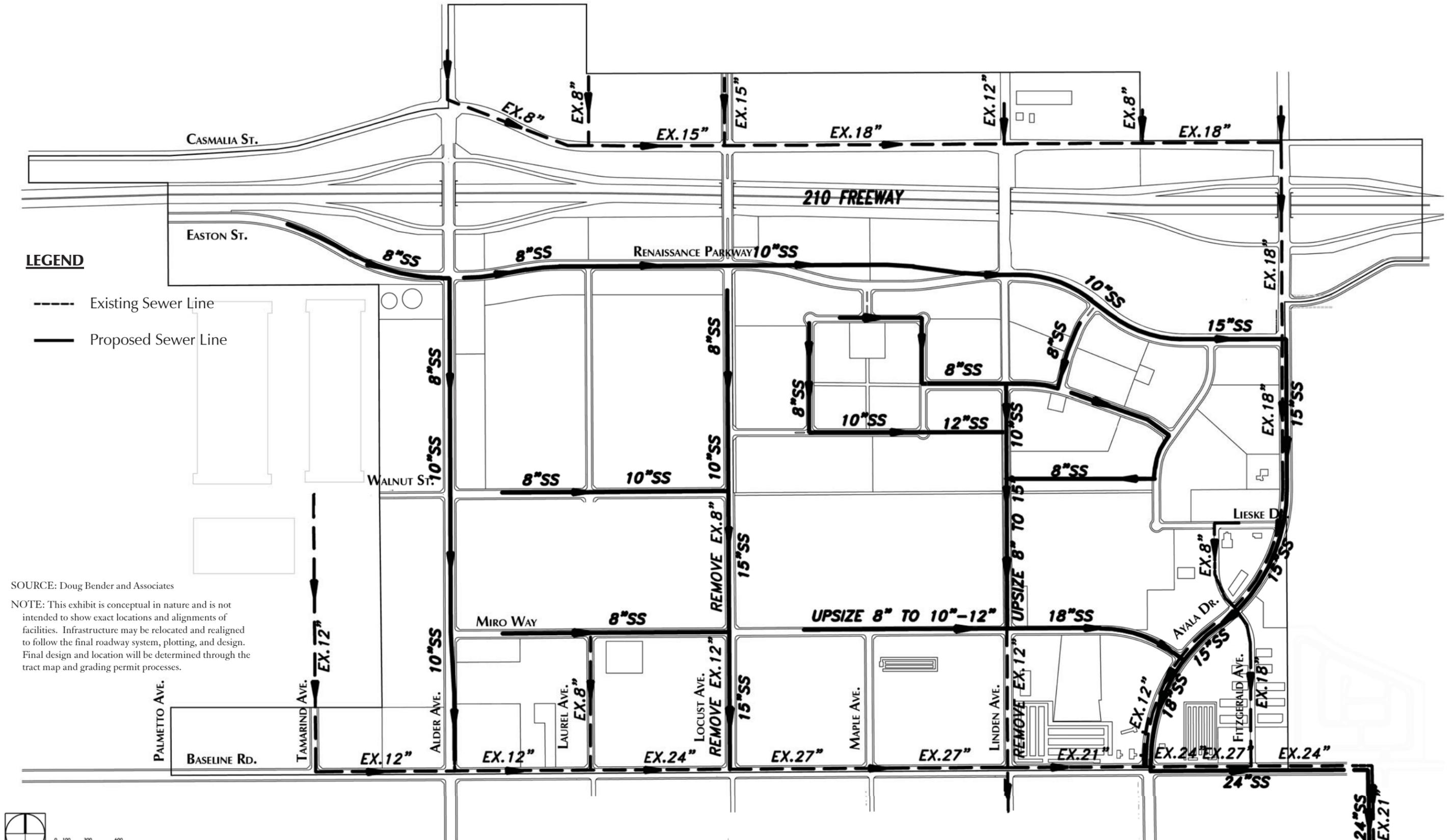


Figure 3-23 Conceptual Sewer Plan

## Development Criteria

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A series of local sewer lines will be constructed within the residential streets. As conceptualized, these sewers will connect to a 15 inch sewer that will be constructed southerly through the area designated as Business Center to Miro Way on an alignment that is parallel to the prolongation of the centerline of Linden Avenue. This sewer will connect to a maximum 18 inch sewer that will be constructed in Miro Way, from westerly of Maple Avenue to Ayala Drive. The sewer in Miro Way will collect sewage runoff from the parcels designated as Business Center north of Miro Way and east of Locust Avenue. It will connect to a new 18 inch sewer in Ayala Drive, which will be constructed from Miro Way to Baseline Road. A new parallel 24 inch sewer will be constructed in Baseline Road, from Ayala Drive to Cactus Avenue.

It is conceptualized that the parcels designated a Business Center north of Baseline Road, east of Alder Avenue, south of Renaissance Parkway, and west of Locust Avenue will be connected to a 15 inch sewer that will be constructed in Locust Avenue. This sewer will be connected to an existing 27 inch sewer that is located in Baseline Road.

It is conceptualized that the parcels designated as Freeway Commercial and Employment located west of Alder Avenue will be sewer to a maximum 12 inch sewer line that will be constructed in Alder. This sewer will be connected to an existing 12 inch sewer in Baseline Road.

The entire Renaissance project will sewer to an existing sewer line that is located in Cactus Avenue. This sewer extends southerly from Baseline Road in Cactus Avenue, easterly in Valley Boulevard, southerly in Riverside Drive, and easterly in Santa Ana Avenue to the City of Rialto sewage treatment plant. TRC prepared a Wastewater Collection System Analysis in 2005 to evaluate the existing and future capacity requirements for the City's sewer system. TRC subsequently prepared the Rialto Airport Redevelopment Wastewater Master Plan Update in 2006. This update evaluated the impacts from development of the Renaissance Specific Plan development. At that time, the plan was to develop a large portion of Renaissance with residential development. With the current specific plan, the number of planned residential units has been significantly decreased, and the amount of business related development has been increased.

The sewer plan is conceptual in nature and is not intended to show exact locations and alignments of facilities. Infrastructure may be relocated and realigned to follow the final roadway system, plotting, and design. Final design and location will be determined through the tract map and grading permit processes.

## ***Dry Utilities Plan***

### ***Electricity***

Southern California Edison will provide electrical service to Renaissance. The source of electricity would be from the substation located on site, at the southeast

## Development Criteria

corner of SR-210 and Locust Avenue. The conceptual plan for the electric system in Renaissance is shown on Figure 3-24, *Conceptual Electricity Plan*.

As shown on the conceptual plan, there are existing 66kV transmission lines in portions of Locust and Laurel Avenues and in an easement, located north of Walnut Avenue, between the two streets. Portions of this line will be relocated to the roadways as shown on the plan. A 66kV line also parallels the south side of the SR-210 from Alder Avenue to the substation. There are existing overhead and underground distribution circuits in most of the streets within the project boundaries. Some of these may have to be relocated and converted to underground per SCE requirements.

### *Gas*

Southern California Gas will provide gas service to the project site. The source of the gas will be from existing high pressure transmission lines and medium pressure distribution mains located in Baseline Road. The conceptual plan for the gas system in Renaissance is shown on Figure 3-25, *Conceptual Gas Plan*.

The pickup point for the existing distribution system is from the pressure regulating station on the north side of Baseline Road and east of Linden Avenue. As conceived, Miro Way, Renaissance Parkway, and Ayala and Locust Avenues would carry a medium pressure distribution main. Distribution at medium pressure would extend from these streets to the rest of the development. There will be no extension of high pressure mains. Some of the existing facilities may be able to be used, as they are, depending upon the ultimate service requirements and phasing of the street improvements. The developer is not responsible to reinforce existing lines. Standard gas extension rules will apply.

### *Telephone*

AT&T will provide telephone service to Renaissance from the Fontana Central Office. The conceptual plan for the gas system in Renaissance is shown on Figure 3-26, *Conceptual Telephone Plan*. As shown on the conceptual plan, the pickup point would be the intersection of Casmalia Street and Locust Avenue on the north side of the SR-210. An extension of primarily fiber-optic cable would be through existing conduit through Locust Avenue.

Existing telephone facilities may be utilized until such time as the street improvements on Locust Avenue occur. Commensurate with street improvements, existing overhead telephone lines will be converted to underground lines. AT&T will install a number of fiber-optic "Pair-Gain" (line concentrator) cabinets and/or controlled environment vaults depending upon the pace of development and the load requirements, as they occur. These are to be built at AT&T's expense. Easements will need to be taken and the exact locations will be determined at the tract map stage. Standard AT&T extension rules will apply for new line extensions.



## Development Criteria

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## Development Criteria

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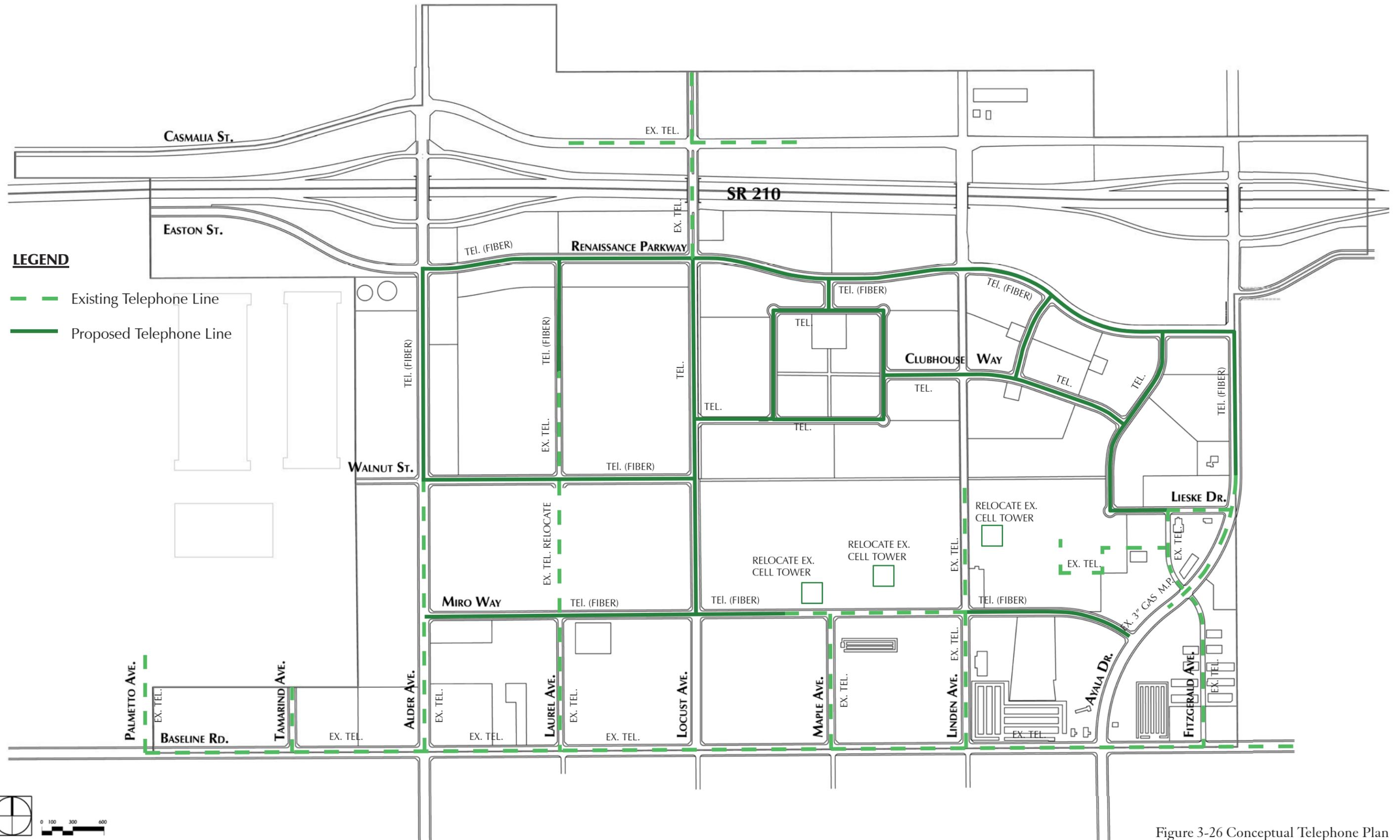


Figure 3-26 Conceptual Telephone Plan

## Development Criteria

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There are plans to convert the entire phone system to fiber-optic cable in this area. Locations and easements will also need to be negotiated for the relocation of the three cell towers in the area. The cell towers will be relocated per the conditional development permit process.

### *Cable*

Time Warner will provide cable service to Renaissance from the line in Baseline Road. Time Warner will be able to provide video, voice, and data services.

The conceptual plan for the gas system in Renaissance is shown on Figure 3-27, *Conceptual Cable Plan*. Existing overhead facilities in will be converted to underground facilities commensurate with street improvements and all new extensions will be in accordance with standard extension policies. The majority of the distribution system will be fiber-optic cable and as many as ten “node” units, which are contained in 17 inch x 30 inch cabinets, could be required.

## Development Criteria

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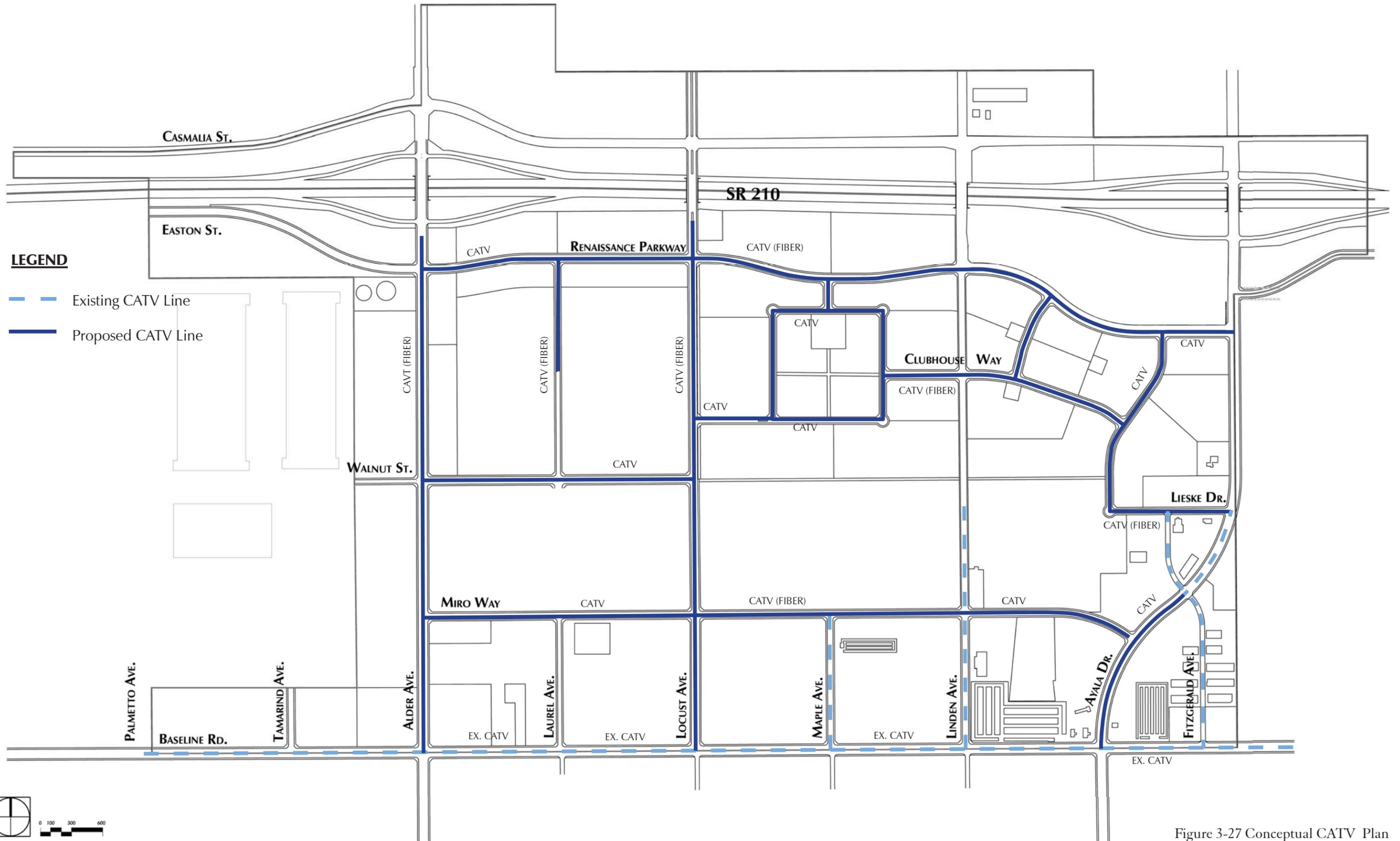


Figure 3-27 Conceptual CATV Plan

## Development Criteria

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## Landscape Plant Palette

The Landscape Plant Palette presents permitted species within the Specific Plan area. Landscaping shall be reviewed and approved by the Public Works Department and the sizes and spacing of plants shall comply with the guidelines in Landscape Plants for Western Regions, by Bob Perry (1992) or City standards, as approved through the landscape plan approval process. Additions to the Landscape Plant Palette are allowed with approval from the Development Review Committee as long as they comply with the guidelines in Landscape Plants for Western Regions, by Bob Perry (1992).

Botanical Names	Common Names
<b>Trees</b>	
<i>Albizia julibrissin</i>	Silk Tree
<i>Alnus rhombifolia</i>	White Alder
<i>Arbutus unedo</i>	Strawberry Tree
<i>Brahea armata</i>	Mexican Blue Palm
<i>Cercis occidentalis</i>	Western Redbud
<i>Chorisia speciosa</i>	Floss Silk Tree
<i>Cinnamomum camphora</i>	Camphor Tree
<i>Citrus</i> sp.	Citrus
<i>Eriobotrya japonica</i>	Bronze Loquat
<i>Eucalyptus</i> sp.	Sweet Gum
<i>Feijoa sellowiana</i>	Pineapple Guava
<i>Gleditsia triacanthos</i>	Honey Locust
<i>Jacaranda mimosifolia</i>	Jacaranda
<i>Juglans californica</i>	California Black Walnut
<i>Koelreuteria bipinata</i>	Chinese Flame
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Laurus nobilis</i>	Sweet Bay
<i>Ligustrum lucidum</i>	Glossy Privet
<i>Liquidambar styraciflua</i>	American Sweet Gum
<i>Liriodendron tulipifera</i>	Tulip Tree
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Magnolia</i> g. 'St. Mary'	NCN
<i>Melaleuca</i> sp.	Melaleuca
<i>Olea europea</i>	European Olive
<i>Phoenix canariensis</i>	Canary Island Palm
<i>Phoenix dactylifera</i>	Date Palm
<i>Photinea x fraseri</i> (Standard)	Photinea

## Development Criteria

Botanical Names	Common Names
<i>Pistachia chinensis</i>	Chinese Pistachio
<i>Platanus acerifolia</i>	London Plane Tree
<i>Platanus racemosa</i>	California Sycamore
<i>Podocarpus gracilior</i>	Fern Pine
<i>Populus fremontii</i>	Western Cottonwood
<i>Prunus caroliniana</i>	Carolina Laurel Cherry
<i>Prunus cerasifera</i> 'Atropurpurea'	Purple Leaf Plum
<i>Prunus ilicifolia</i>	Holly Leaf Cherry
<i>Prunus lyonii</i>	Catalina Cherry
<i>Pyrus calleryana</i>	Pear
<i>Punica granatum</i>	Pomegranate
<i>Quercus agrifolia</i>	Coast Live Oak
<i>Quercus engelmannii</i>	Engelmann Oak
<i>Quercus lobata</i>	Valley Oak
<i>Quercus suber</i>	Cork Oak
<i>Rhus lancea</i>	African Sumac
<i>Rhapiolepis</i> i. 'Majestic Beauty'	Indian Hawthorne
<i>Schinus molle</i>	California Pepper
<i>Tristania conferta</i>	Brisbane Box
<i>Ulmus parvifolia</i>	Evergreen Elm
<i>Umbularia californica</i>	California Laurel
<i>Washingtonia filifera</i>	California Fan Palm
<i>Washingtonia robusta</i>	Mexican Fan Palm
<b>Shrubs</b>	
<i>Acacia redolens</i>	Acacia
<i>Achillea</i> sp.	Yarrow
<i>Agapanthus africanus</i>	Lily of the Nile
<i>Agave americana</i>	Century Plant
<i>Agave attenuata</i>	Foxtail Agave
<i>Aloe</i> sp.	Aloe
<i>Alyogyne huegelii</i>	Blue Hibiscus
<i>Anzigozanthus flavidus</i>	Kangaroo Paw
<i>Arbutus unedo</i> 'Compacta'	Strawberry Tree
<i>Baccharis Salicifolia</i>	Mulefat
<i>Bougainvillea</i> sp.	Bougainvillea
<i>Buddleja davidii</i>	Butterfly Bush
<i>Buxus m. japonica</i> 'Green Beauty'	Japanese Boxwood
<i>Camellia japonica</i>	Camellia
<i>Camellia sasanqua</i>	Camellia
<i>Carissa m.</i> 'Boxwood Beauty'	Natal Plum
<i>Carpenteria californica</i>	Bush Anemone

<b>Botanical Names</b>	<b>Common Names</b>
<i>Chamaerops humilis</i>	Mediterranean Fan Palm
<i>Cistus ssp.</i>	Rockrose
<i>Cotoneaster sp.</i>	Cotoneaster
<i>Dendromecon rigida</i>	Bush Poppy
<i>Dietes bicolor</i>	Fortnight Lily
<i>Diosma pulchrum</i>	Pink Breath of Heaven
<i>Echium candicans</i>	Pride of Madeira
<i>Elaeagnus pungens</i>	Silverberry
<i>Encelia californica</i>	California sunflower
<i>Escallonia sp.</i>	Escallonia
<i>Euonymus japonica</i>	Evergreen Euonymus
<i>Fremontodendron californicum</i>	California Flannel Bush
<i>Garrya elliptica</i>	Silktassel
<i>Grevillea sp.</i>	Grevillea
<i>Hemerocallis spp.</i>	Daylily
<i>Hesperaloe parviflora</i>	Red Yucca
<i>Heteromeles arbutifolia</i>	'Toyon
<i>Ilex vomitoria</i> 'Stokes'	Stokes Dwarf Yaupon Holly
<i>Juncus acutus</i>	Spiny Rush
<i>Keckelia cordifolia</i>	Heart Leafed Penstemon
<i>Kniphofia uvaria</i>	Red Hot Poker
<i>Lantana montevidensis</i>	Lantana
<i>Lavatera assurgentifolia</i>	'Tree Mallow
<i>Lavandula stoechas</i>	Spanish Lavender
<i>Leptospermum scoparium</i>	New Zealand Tea
<i>Leptospermum s. 'Ruby Glow'</i>	New Zealand Tea
<i>Leymus condensatus</i>	Giant Wild Rye
<i>Ligustrum japonicum</i> 'Texanum'	'Texas Wax Leaf Privet
<i>Liriope gigantea</i>	Giant Lily Turf
<i>Mahonia aquifolium</i>	Oregon Grape
<i>Melaleuca nesophila</i>	Pink Melaleuca
<i>Mimulus ssp.</i>	Monkey Flower
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Myrtus communis</i>	Myrtle
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Ochna serrulata</i>	Bird's Eye Bush
<i>Opuntia littoralis</i>	Coastal Prickly Pear
<i>Phormium tenax</i>	Flax
<i>Pitosporum tobira</i>	Mock Orange
<i>Plumbago auriculata</i>	Cape Plumbago
<i>Prunus carolina</i> 'Bright 'n' Tight'	Bright 'n' Tight Carolina Laurel

## Development Criteria

Botanical Names	Common Names
<i>Pyracantha</i> ssp.	Firethorne
<i>Quercus berberdifolia</i>	California Scrub Oak
<i>Rhamnus californica</i>	California Coffeeberry
<i>Rhamnus ilicifolia</i>	Holly Leaf Redberry
<i>Rhaphiolepis</i> i. 'Clara'	India Hawthorne
<i>Rhaphiolepis</i> i. 'Majestic Beauty'	India Hawthorne
<i>Rhus ovata</i>	Sugar Bush
<i>Ribes speciosum</i>	Fuchsia Flowering Gooseberry
<i>Ribes viburnifolium</i>	Evergreen Current
<i>Romneya coulteri</i>	Matilija Poppy
<i>Rosa</i> spp.	Rose
<i>Rosa banksiae</i>	Lady Banks' Rose
<i>Rosmarinus</i> o. 'Tuscan Blue'	'Tuscan Blue Rosemary
<i>Rosmarinus</i> o. 'Prostratus'	Prostrate Rosemary
<i>Salvia greggi</i> 'Furman's Red'	Furman's Red Autumn Sage
<i>Salvia leucantha</i> 'Santa Barbara'	Santa Barbara Sage
<i>Scirpus californica</i>	California Bullrush
<i>Stachys byzantina</i>	Lamb's Ear
<i>Tecoma stans</i>	Yellow Bells
<i>Trachelopermum jasminoides</i>	Star Jasmine
<i>Viburnum</i> sp.	Laurustinus
<i>Westringia rosmariniformis</i>	Coast Rosemary
<i>Yucca</i> sp.	Yucca
<b>Groundcovers</b>	
<i>Artemisia caucasica</i>	Caucasian Artemisia
<i>Baccharis</i> p. 'Pigeon Point'	Twin Peaks
<i>Carissa macrocarpa</i> 'Green Carpet'	Green Carpet Natal Plum
<i>Elymus condensatus</i>	Blue Lyme Grass
<i>Eriodyctyon trichocalyx</i>	Yerba Santa
<i>Eschscholiza californica</i>	California Poppy
<i>Festuca ovina</i> 'Glauca'	Blue Fescue
<i>Festuca rubra</i> 'Moltate'	California Red Fescue
<i>Gazania</i> sp.	South African Daisy
<i>Hedera</i> h. 'Hahnii'	Hahn's Ivy
<i>Lonicera japonica</i> 'Halliana'	Halls Honeysuckle
<i>Marathon II</i> or <i>Medallion II</i>	Turf
<i>Lantana montevidensis</i>	Lantana
<i>Lupinus nanus</i>	Sky Lupine
<i>Myoporum</i> 'Pacificum'	NCN
<i>Nassella lepida</i>	Foothill Needle Grass
<i>Nassella pulchra</i>	Purple Needle Grass

<b>Botanical Names</b>	<b>Common Names</b>
<i>Sisyrinchium bellum</i>	Blue Eyed Grass
<i>Thymus vulgaris</i>	Common Thyme
<i>Trachelospermum jasminoides</i>	Star Jasmine
<b>Vines/Espaliers</b>	
<i>Calliandra haematocephala</i>	Pink Powder Puff
<i>Distictus buccinatoria</i>	Blood-red Trumpet
<i>Gelsemium sempervirens</i>	Carolina Jessamine
<i>Jasminum polyanthemum</i>	Evergreen Jasmine
<i>Osmanthus fragrans</i>	Sweet Olive
<i>Parthenocissus tricuspidata</i>	Boston Ivy
<i>Passiflora alatocaerulea</i>	Passion Vine
<i>Podocarpus gracilior</i>	Fern Pine
<i>Pyracantha 'Graber'</i>	Firethorn
<i>Trachelospermum jasminoides</i>	Star Jasmine

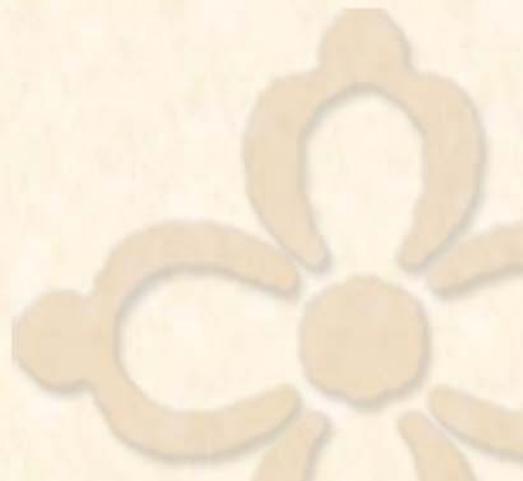
## Development Criteria

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# Design Guidelines

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## SECTION FOUR



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## SECTION 4: DESIGN GUIDELINES

For Renaissance to be an identifiable district within Rialto, it must have a unified and unique physical appearance. At the same time, there must be enough physical variation to create distinct neighborhoods. Renaissance also contains areas that represent different scales of movement and distance that should be appropriately differentiated. For instance, the structures adjacent to SR-210 should be at a different scale and level of detail than those in the pedestrian areas of the Town Center.

These Design Guidelines are intended to create an attractive and cohesive community by providing detailed direction for the physical design of Renaissance. These guidelines are intended to provide a physical framework that unifies Renaissance as well as provide subtle distinctions that lead to unique neighborhoods.

It is important to note that these Design Guidelines, while general and illustrative in nature, are to be used to evaluate development proposals. Every development within Renaissance must incorporate the particular attention to detail reflected in these guidelines and the intent of these guidelines must be met in order for a project to be approved.

### Community Structure Guidelines

Community structure guidelines apply to the public areas of Renaissance. They are intended to create a strong community identity through the use of consistent streetscape, entry monumentation, landscaping, and lighting elements along the public rights-of-way.

#### *Entries and Monuments*

A system of arrival and entry treatments distinguish Renaissance from surrounding developments and announce arrival into the Plan itself. There are various types and scales of the entry treatments, but they are all designed as an integrated family. As detailed below, there are various types of entry treatments in Renaissance at varying sizes and levels of prominence. As shown on Figure 4-1, *Entries and Monuments*, the entry and monument treatments are located at select intersections, with the more prominent intersections receiving a greater level of treatment.

The treatments described below provide the desired quality of the entry and monument types. The exact design, configuration, location, and content of each will be determined in detailed site plans at the tract map level and may be modified with ministerial approval by the Development Review Committee.



*Examples of the streetscape envisioned in Renaissance.*



*Example of a Community Arrival, which identifies and distinguishes Renaissance.*

# Design Guidelines

## *Community Arrivals*

Community Arrivals are the most prominent entries into Renaissance and represent the most significant design treatment. Community Arrivals should incorporate distinctive signage, attractive landscaping, and distinguishing elements such as vertical monuments, monument-style walls, or water features. The architectural treatment of the elements should echo the materials and architectural design treatments of Renaissance. Formal signature groupings of trees and landscaping can be incorporated into the design to create distinctive entries to the community.



Example of a Primary Community Marker.

## *Primary and Secondary Community Markers*

Primary and Secondary Community Markers identify secondary entry points into Renaissance or prominent intersections. Primary Community Markers are larger in size and scale than Secondary Community Markers. Primary and Secondary Community Markers are marked with a monument and garden area on the corners, distinctive signage, attractive landscaping, and distinguishing elements. The elements may be of various sizes, but materials will reflect the character of each area as well as the entirety of Renaissance. Formal signature groupings of trees and landscaping can be incorporated into the design to create distinctive entries.

## *Neighborhood Markers*

Neighborhood Markers identify individual neighborhoods in the Village. Neighborhood Markers are small-scale pilaster monuments in distinctive landscaped areas behind the sidewalk. Neighborhood Markers should reflect the character and materials of the neighborhood. The use of trees, shrubs, groundcover, signage, and lighting will vary by neighborhood.



## *Park and Business Markers*

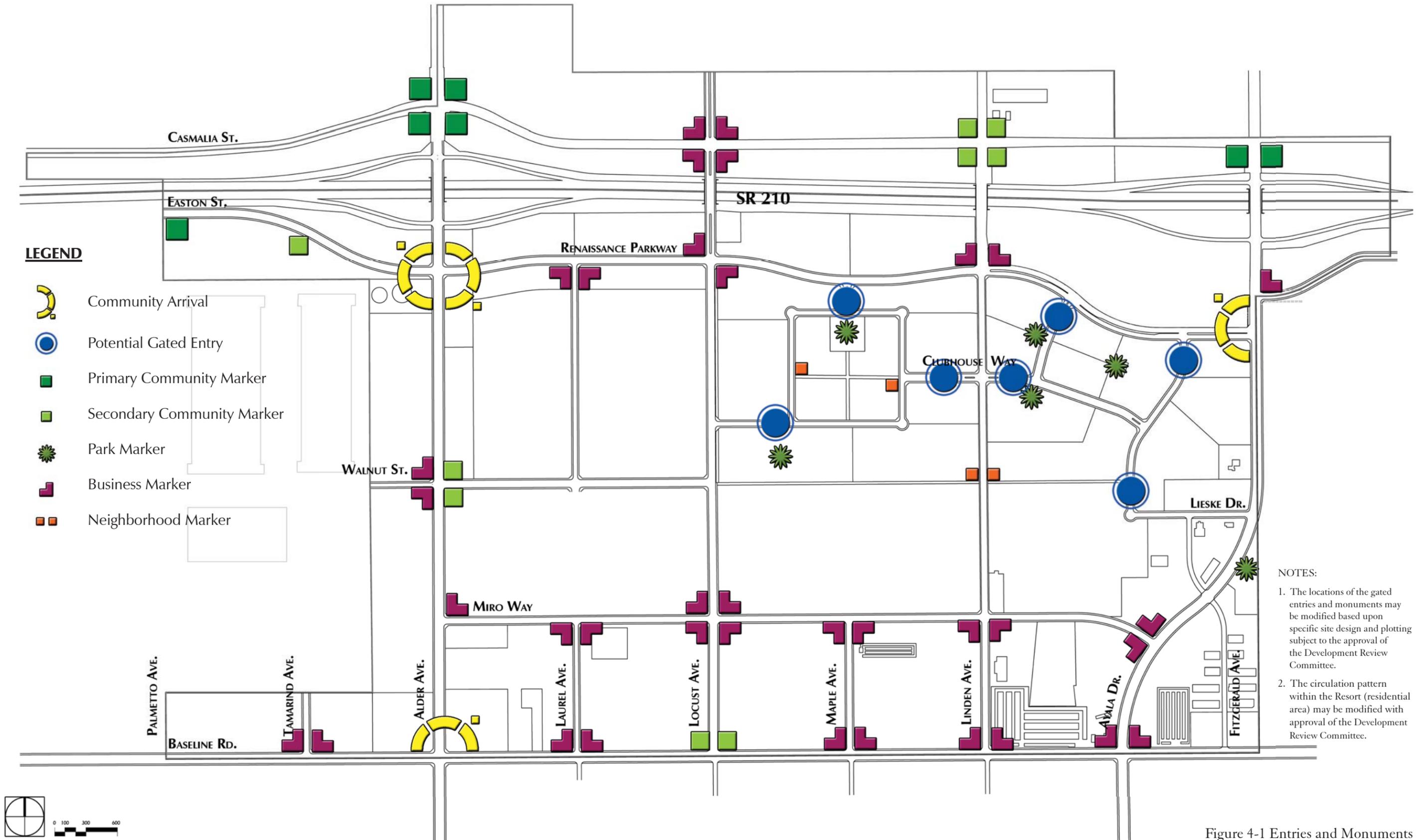
Park and Business Markers identify major recreational amenities and business areas at significant intersections. Park and Business Markers are decorative walls set in distinctive landscaped areas behind the sidewalk to improve intersection visibility and provide a visual cue to help identify the community. The materials and signage will reflect the character of each area and yet be designed as part of the family of monuments for the entirety of Renaissance.



Examples of Neighborhood Marker (top) and Park/Business Marker (above) in Renaissance.

## *Gated Entries*

The potential locations of the Gated Entries are shown on Figure 4-1; however this item addresses the aesthetic qualities of the gated entries. Gated entries will be set back from the main road and highlighted with landscaping. The gates, paving, and lighting will be richly detailed and reflect the architectural character of Renaissance.



- NOTES:
1. The locations of the gated entries and monuments may be modified based upon specific site design and plotting subject to the approval of the Development Review Committee.
  2. The circulation pattern within the Resort (residential area) may be modified with approval of the Development Review Committee.

Figure 4-1 Entries and Monuments

## Design Guidelines

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### *Streetscapes*

- ❖ Streetscape elements, such as landscaping, lighting, street furniture, and signage should create an attractive, consistent, and cohesive community image.
- ❖ Streetscape elements, such as lighting, landscaping, and street furniture, should complement the surrounding architectural styles.
- ❖ Traffic-calming measures, such as sidewalk bulbs and roundabouts, may be used in and adjacent to residential areas to reduce the speed of traffic and create a more pedestrian-friendly environment.

### *Pedestrian and Bicycle Connectivity*

- ❖ Pedestrian and bicycle routes along roadways and paseos should incorporate pedestrian amenities such as benches, shade structures, and lighting where appropriate.
- ❖ Pedestrian connections should link residential neighborhoods with recreational, shopping, and service areas.
- ❖ Bike racks should be located at strategic points throughout the open space system, such as playgrounds, parks, and other recreational amenities, to encourage nonvehicular circulation.
- ❖ Trails and paseos should be clearly marked with consistent signage and well lit with bollard lighting as appropriate.



*Create attractive and safe pedestrian connections in Renaissance.*

### *Landscaping*

- ❖ All landscaping shall comply with the approved trees, shrubs, and groundcovers listed in the Landscape Plant Palette, Section 3 of this Specific Plan.
- ❖ Landscaping along major roadways and at project entries should be tasteful and consistent to create an attractive and cohesive community identity. Formal plantings of nonnative species may be used at key entries and intersections to highlight these areas.
- ❖ Landscaping within paseos should consist of informal groupings of shade trees, shrubs, and plantings. Native and other low-water species should comprise the majority of landscaping within paseos. However, turf areas may be provided in areas intended for active use.
- ❖ Median plantings should reflect the use of the roadway of which they are a part. Median plantings along Renaissance Parkway should be more formal than those along project arterials.



*Examples of landscape and streetscape treatments envisioned in Renaissance.*

## Design Guidelines

- ❖ Water usage should be minimized through the planting of native and low-water species and the utilization of water-efficient and drip irrigation systems.
- ❖ As practical, utilize medians and parkways for water treatment and to reduce runoff.
- ❖ Where possible, landscaping should be used to buffer adjacent land uses that are different in scale, use, or design.

### *Walls and Fences*

- ❖ Solid walls and fences should not dominate the street scene. They should only be used when necessary for noise attenuation, privacy, and shielding of incompatible adjacent uses.
- ❖ View fences provide a visually attractive alternative to solid walls and fences. They allow for safety and privacy while preserving views and creating a more visually appealing neighborhood. View fences should be used instead of solid walls when feasible, especially when facing onto parks and paseos.
- ❖ Wall faces that are visible to the public should be constructed of attractive materials and finished with architectural detailing or articulation. The incorporation of high quality materials and surface articulation are strongly encouraged.
- ❖ Walls and/or wall surfaces not visible to the public do not need the same high level of detail. Pilasters should be incorporated into wall design, especially at entries and important community intersections.
- ❖ Trees, vines, and landscaping should be used to soften the visual appearance of the walls.
- ❖ Walls and fences should be made of durable yet attractive materials that complement the adjacent architecture and are resistant to graffiti. When solid walls are necessary, split-face block, stone, or materials with similar visual qualities should be used. Likewise, view fences should incorporate visually attractive materials such as tubular steel, decorative metal, and stone (or faux-stone).
- ❖ Long, monotonous walls are to be avoided. Walls should be undulated with breaks, recesses, and offsets, especially at entries and important intersections. Long walls should be made more attractive and visually interesting through the incorporation of surface articulation and pilasters.



*Examples of perimeter walls (top) and screening walls (bottom) appropriate for Renaissance.*

## Lighting

- ❖ Attractive and consistent lighting elements should be provided along roadways within the neighborhood. The height, brightness, and spacing of the lighting elements should be appropriate to the scale and speed of the roadway.
- ❖ Pedestrian-scale lighting should be provided in areas with nighttime use, such as paseos and walkways. The type, style, and intensity of pedestrian lighting should reflect the use of the character of the area.
- ❖ Lighting elements should be shielded or downward facing to minimize glare, spillover, and light pollution.
- ❖ Lighting fixtures should be compatible with the architectural styles of surrounding buildings and yet consistent throughout the community.
- ❖ Lighting standards should be consistent with City safety and illumination requirements.



*Use themed and pedestrian-level lighting in Renaissance.*



*Water features are desirable elements in Renaissance.*

## Water Features

- ❖ Water features improve property values, act to cool their surrounding environment, drown out ambient noise, and act as interesting focal points. Water features are highly encouraged in visually prominent and/or “special” locations such as pedestrian plazas and gathering areas and entry gateways to distinguish neighborhoods and developments.
- ❖ Water features must use water recirculation systems.
- ❖ When employed, position water features to maximize their audible and cooling properties around seating areas, plazas, and public gathering areas.
- ❖ Water features may be decorative or interactive, but must be designed to meet current safety requirements.
- ❖ Landscape treatments that serve to mimic water elements, such as dry stream beds, and those that serve to capture water, such as bioswales, are also highly encouraged as they evoke qualities of water environments.

# Design Guidelines

## Business-Related Design Guidelines

The following design guidelines apply to the business-related land use categories in Renaissance. Specifically, these guidelines relate to the physical development in the Town Center, Corporate Center, Freeway Commercial, Employment, Business Center, Freeway Incubator, and General Commercial (GC) land use categories.

### General Design Elements

- ❖ Avoid long, monotonous building facades.
- ❖ Create diversity by clustering buildings around courtyards and open areas where possible.
- ❖ Establish a visual link in multibuilding complexes by using architectural and site design elements to unify the project.
- ❖ Roof styles and materials should be architecturally and aesthetically compatible, not uniformly consistent.
- ❖ Attention should be paid to building at a “human scale” to perpetuate the user-friendly atmosphere of any commercial activity.
- ❖ As much as possible, architectural styles and details should be authentic. In this regard, the mixing of styles is encouraged where details are complementary and continue to foster a community atmosphere.
- ❖ Signage and landscaping shall complement the building’s architecture.
- ❖ Above all, design of the commercial development should create an inviting place to shop.



*Massing and color changes reduce the scale of a building. Examples of coordinated architecture and landscaping as well as the creation of gathering spaces between buildings.*



*Example of the effective treatment of the spaces between buildings to unify and add value to a project.*



*Example of a well-designed project with unified architectural theme, unique corner treatment, coordinated signage, and clean landscaping.*

### Site Planning

#### Orientation

- ❖ All buildings within each commercial development should face onto a street, parking lot, or connecting internal walkway. Where highway frontage is available, buildings should take this into account in terms of attractive building design and visible signage.
- ❖ When possible, windows and entries should face the street or pedestrian walkways, avoiding dominant blank walls.

### *Setback Areas*

All setbacks along street edges should be landscaped keeping in mind that retail buildings and signs must remain, for the most part, visible from the street and not blocked by trees or other landscape or hardscape elements.

### *Parking Areas*

- ❖ Large expanses of parking lots adjoining street frontage should be broken up with buildings, plazas, landscaping, or other pedestrian-scale features.
- ❖ Parking areas should be designed to minimize the conflict between pedestrian and vehicular traffic.
- ❖ Parking areas should provide clearly marked and well-lit pedestrian walkways to ensure pedestrian safety.
- ❖ Parking areas should be tucked behind buildings when possible. Doing so will minimize the visual impact of these areas along the roadway.
- ❖ Shade trees and structures should be incorporated into parking lot design.



*Parking lots should incorporate screening, shade trees, and illuminated pedestrian walkways.*

### *Service and Storage Areas*

- ❖ Whenever possible, commercial loading docks, garages, and storage areas should be located either behind or to the side of the building served. These services should have their access from secondary rather than primary streets and should not interact with any pedestrian areas.
- ❖ Service entrances shall be sited so that they do not interfere with customer access.
- ❖ Appropriate on-site service vehicle parking/turnouts shall be provided in an efficient, nonobtrusive location.
- ❖ Loading vehicles, when parked, shall not impede normal traffic flow.



*Example of landscaping used to screen and soften a long expanse of wall.*

### *Screening*

Solid walls, landscaping, or trellis work must screen any refuse containers, utility equipment, mechanical equipment, or loading areas from street, parking, or public view. This screening should be conceived as part of the building architecture to best blend with the overall development.

### **Screening of Refuse/Recycling Containers, Utility Equipment, and Mechanical Equipment**

- ❖ Whenever possible, refuse/recycling collection or storage areas should be located behind or to the side of the building served.



*Example of appropriate rooftop screening.*

# Design Guidelines

- ❖ Refuse/recycling collection areas should be designed to contain all waste generated on-site between collections
- ❖ All roof-mounted equipment shall be screened so that it is not visible from public streets, walks, or parking lots. Screening of rooftop equipment shall be achieved by the building parapet or some other screen wall component that is consistent with the structure's architecture. This applies to all types of equipment, including mechanical equipment, communication dishes, skylights, exhaust fans, ducts, or any other nonarchitectural elements.
- ❖ All flashing and sheet metal materials shall be articulated and painted to be consistent with the overall architectural expression. Exterior downspouts should be avoided along façades that are visible from streets or walkways.
- ❖ All wall- or ground-mounted equipment shall be screened so that it is not visible from public streets or walkways in accordance with utility requirements. Screening equipment shall be achieved by the building itself, a screen wall component that is consistent with the structure's architecture, and/or landscaping.
- ❖ Screen walls should be treated similarly to building façades, especially as they become tall and long.



*Example of appropriate screening for refuse and recycling enclosures.*



*Mixing of horizontal and vertical design elements helps create balance while bold color change denotes entry at a large scale.*

## Screening of Loading Areas

- ❖ Loading docks and doors should be oriented away from street view and buffered and/or setback from incompatible adjacent uses.
- ❖ Truck loading docks shall be effectively screened from public view. Screening shall be aesthetically compatible landscaping, and/or comparable materials that effectively obscure loading dock areas. On-grade loading areas not related to large trucks do not need to be screened.
- ❖ Enclosed service areas or service private drives serving multiple tenants need only screen access points of the service area.

## Exterior Design

The architectural style of the commercial developments should respond to the area in which they reside. As noted, frontages along pedestrian areas should have a greater level of detail, especially at the sidewalk level, while those along SR-210 should have bold changes in massing and color. Likewise, those portions of the building that are exposed to public view should have a greater level of treatment than those portions of the building that are screened from public view.

### *Primary Exposure*

The primary exposure(s) of buildings should receive greater attention to design detail than those that are screened from public view. This can be achieved through variation in form, mass, scale, modulation, and architectural detail.

#### **Building Form, Mass, and Scale**

- ❖ Primary exposures/elevations shall be appropriately detailed and articulated consistent with the architectural style and character of the development.
- ❖ Building forms should be simple and well proportioned, resulting in a balanced composition of elements.
- ❖ Articulate planar façades through the use of reveals and color and material changes.
- ❖ Building massing should consist of a mix of building heights to provide visual interest.
- ❖ Towers and other vertical/prominent building features should be used to accentuate key elements such as building entries, pedestrian nodes, plazas, or courtyards.
- ❖ The primary exposure of commercial buildings greater than one story should use architectural elements, signage, and details consistent with the architectural style to reduce the impact of higher volume spaces.



*Examples of articulated building façades, interesting building masses, varied rooflines, and appropriate use of colors and materials.*

#### **Freeway Adjacent Form, Mass, and Scale**

- ❖ Structures adjacent to SR-210 are encouraged to take advantage of increased height and signage opportunities.
- ❖ The design of structures along SR-210 should take into account the speed and distance at which they will be viewed. This means that on most of the buildings fronting on the freeway, small-scale articulation and texture will be lost. Developments along the freeway should rely on bold changes in massing and color to establish identity. Small-scale articulation of these buildings and textures will still be appropriate at locations where pedestrians will experience them.

#### **Town Center and Corporate Center Form, Mass, and Scale**

- ❖ Except for the freeway frontages, the Town Center and Corporate Center areas are intended to create a pedestrian-friendly environment. Small-scale articulation of building facades, roofs, materials, colors, and textures are appropriate at locations where pedestrians will experience them.

# Design Guidelines



*Selectively placed insets and outssets create visual interest. Repetitive elements create a smaller scale and provide continuity in a pedestrian area.*



*Example of façade articulation and building massing appropriate in large-scale areas.*

- ❖ In pedestrian areas, great care should be taken to provide small-scale articulation and textures.
- ❖ Variety in massing and articulation emphasizes pedestrian scale. This includes materials used for both the base and cornice treatments of a development. Base materials have the illusion of “grounding” the building and setting one space apart from another, particularly as they face a street or walkway. Appropriate materials may include:
  - Plaster
  - Ceramic tile
  - Granite
  - Stone
  - Marble
  - Split-face concrete block
- ❖ Cornice treatments at street or walkway frontages are encouraged to tie the development together. Suggested treatments include:
  - Shaped, foam-based, plaster-covered cornice
  - Roof overhangs with brackets
  - Stepped parapets
  - Textured materials

## Building Modulation

- ❖ Large-scale wall planes should be treated with techniques to avoid creating a monotonous effect. Long walls of a single plane, color, material, and height, with no articulation or texture variation, are not appropriate. Long walls that have some variation in plane, height, color, texture, or material, and some articulation are appropriate. The scale of relief along SR-210 should be large-scale while the scale of relief in the pedestrian areas should be small-scale.
- ❖ Offset wall planes should be used, where appropriate, as an integral part of the building design.
- ❖ Building offsets or recesses should be used to accentuate building entries and form pedestrian nodes.
- ❖ No building façade visible to the public shall have a blank uninterrupted length of wall measuring greater than 50 feet without including one of the following:
  - Change in color
  - Change in material

- Change in plane
- Lattice work and trellis elements
- Pilasters
- Decorative light fixtures
- Material inlays
- Variation of architectural styles
- Murals, graphics, or other visual variations
- A tree or equivalent landscape element

❖ Along streets or walkways, buildings should be articulated to create a visual interest. This can be accomplished through the application of windows, arcades, trellises, awnings, and other architectural features, as well as color blocking, the appropriate application of detail elements, and breaking up the massing of the building. This is not required on the freeway side of a commercial development, though encouraged for architectural interest.

### Building Entry Treatment

- ❖ Entries should be visually appealing and identifiable to users. Each commercial building shall provide a well-articulated, identifiable path of entry for pedestrian and vehicular users. Elements such as massing or color change, variation in materials, or signage may prove effective in announcing entry.
- ❖ Mass elements taller than the primary building walls, and recessed areas set back from the primary walls can help identify entry points.
- ❖ In the pedestrian areas, less space is necessary to define entries. Entry conditions in these areas do not need large-scale massing elements and can rely on small-scale signage and material changes.
- ❖ Articulation of major tenant entries for pedestrian identification should be achieved through the use of enriched materials, architectural detailing, and color schemes that offset the entry from the rest of the building.



*Examples of decorative wall finishes and awnings that denote building entries.*



*Example of front entrance façade articulation.*

# Design Guidelines



Vertical masses and bold elements denote entry conditions and provide identity at a freeway scale.



Changing rooflines create interest and can denote entry.



Variation of materials and color provide interest. Small-scale textures like brick are appropriate in pedestrian areas.



Use of varying colors, patterns, and landscaping can create visual interest.

## Secondary Exposure

- ❖ Secondary exposures shall be architecturally compatible, though not as detailed as primary exposures.
- ❖ Secondary exposure of commercial buildings greater than one story shall provide an appropriate level of articulation to engage the street scene.
- ❖ Articulation can be achieved through wall plane offsets, breaking of parapet lines, and details such as windows and shutters, material inlays, and color or texture changes.

## Roofs

- ❖ Building height shall be varied to provide visual interest.
- ❖ Variety in roof forms, ridge heights, and direction of gables is required.
- ❖ Tower elements or other prominent building features should be used to accentuate key elements such as building entries, pedestrian nodes, plazas, or courtyards.
- ❖ Although the majority of commercial roof area may be flat, the massing of visible elevations should be broken up with the addition of sloping roof elements, including hips or gable forms (as appropriate to the primary architectural style), or parapet treatments with architectural details.
- ❖ Roof pitch shall be in proportion to the design of the building and in conformance with code regulations for the roof material.

## Architectural Features and Accents

### Materials and Colors

Building materials and colors are key design elements in establishing an identity. While variations of materials and colors are appropriate, the number of different materials and colors should be limited on a building.

- ❖ All visible components of the building shall be treated and finished in a manner consistent with the overall architectural expression.
- ❖ Acceptable exterior finish materials include concrete with textured, sandblasted, or painted finishes; stone or brick; stucco; or masonry with textured or sandblasted finishes. Glass, glazing systems, glass block, ceramic or natural stone tile, and metal panel systems are appropriate when used as accents.

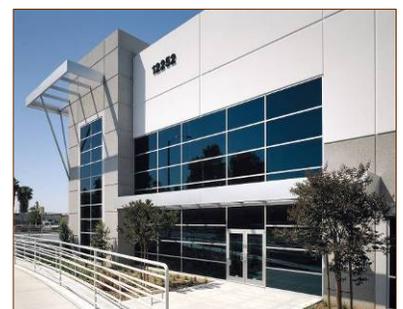
- ❖ All facilities will be constructed of permanent finished materials such as concrete, masonry, and glass. Prefabricated metal or exposed precision concrete masonry are not allowed, but metal siding may be used only when it serves a practical purpose (e.g. refrigeration units) and is limited to 10 percent of an elevation area.
- ❖ A dominant building material and color should be clearly established in each building. Accents and variations may then occur within the background established by that dominant base. The dominant colors for buildings should be more neutral in nature, with the more intense colors used in the accents.
- ❖ Color and material should be used in a way that is consistent with the overall architectural expression. Masses denoting entry points should be differentiated in material or color. Articulation in large wall surfaces should be reinforced with variations in color. Entry areas should have finer-textured materials and more intense colors.
- ❖ Materials and colors should be applied to create authentic architectural styles and streetscapes.
- ❖ All surface treatments or materials should be designed to appear as integral parts of the design, not merely applications.
- ❖ All materials should wrap architectural elements in their entirety on primary elevations and where exposed to primary public spaces.
- ❖ Material changes should occur at inside corners or other logical termination points.
- ❖ Materials applied to any elevations should turn the corner of the building to a logical termination point in relation to architectural features or massing.



*Typical painted finish articulated for pedestrian scale.*



*Example of fine-grained window articulation that is appropriate in pedestrian areas.*



*Large expanses of glass work well for large-scale environments but should be avoided in pedestrian areas.*

### ***Arcades, Trellises, and Awnings***

In an effort to protect pedestrians from weather elements, arcades, trellises, and awnings may be used as part of the design.

### ***Windows and Doors***

Window and door details create a strong visual impact through placement and configuration. Office buildings will make extensive use of glass windows on all sides, while industrial facilities are likely to have glass windows on only a portion of the building near an entry. In either case, windows should be treated consistently within a single building regarding their placement and detailing. Recessing or projecting glass at entry areas is an effective way of emphasizing these points architecturally.

## Design Guidelines

- ❖ A base window glass in terms of tint and reflectivity should be established, but variations on the theme may be used to reflect the intentions of massing and scale in the architectural expression.
- ❖ Accenting of display windows on the first floor is a strong tool for the articulation of store frontage. Display windows can be at pedestrian eye level to stimulate street-scene interest and promote a viable business climate.
- ❖ 40 percent glazing is encouraged on all multitenant commercial buildings and the commercial base of a multistory mixed-use building.
- ❖ Window treatments, where feasible, are encouraged. Exterior window treatments include, but are not limited to:
  - Recessing/surrounds of not less than four inches.
  - Trim elements
  - Headers and sills
  - Awnings (cloth awnings should be carefully designed due to high winds.)
  - Shutters (proportional to window and consistent with architectural style).
  - Mullion patterns as appropriate to the architectural style.
- ❖ Awnings, when provided, should be designed to be consistent with the architectural style and color palette of the main structure.
- ❖ Unacceptable awning treatments include: metal louvers (except Bermuda-style shutters) or untreated fabric.
- ❖ Large expanses of glass are appropriate in the large-scale developments on SR-210.
- ❖ Large expanses of glass are inappropriate for pedestrian areas. In pedestrian areas, the building wall should include openings for windows and doors.
- ❖ Emergency exit/egress-only doors should be treated to blend in with the adjacent walls or surfaces to discourage their perception as entries.

### *Exterior Lighting*

Exterior lighting fixtures should be compatible with the architectural style of the building and proportional to the scale of the building.

### *Building Systems*

- ❖ All electrical meters should be located on the rear or side elevation of a commercial development and screened from public view.
- ❖ Natural-gas meters should be grouped and screened from public view by walls or landscaping on the side or rear elevations of a building.
- ❖ Electrical meters should be located on the rear or side elevation and should be clustered and located behind doors. Screening electrical meters behind doors is not required where meters are located in screened service areas and service courts or in “back of house” areas not intended for public access.
- ❖ Screening walls and electrical enclosures should be designed as integral architectural features of the building.
- ❖ Ventilation and HVAC systems should be designed to meet or exceed the minimum outdoor air ventilation rates described in the American Society of Heating, Refrigeration, and Air Conditioning Engineers (ASHREA) standards.

## **Private Parks and Open Spaces**

- ❖ Recreation and open space areas should be designed accommodate the needs of different ages and abilities.
- ❖ Canopy trees should be used to provide shade. Informal groupings create visual interest and are encouraged.
- ❖ Ample outdoor furniture should be provided. This furniture should match the surrounding architectural styles, materials, and colors.
- ❖ A combination of hard and soft paving may be used depending upon the function of the recreational amenity.
- ❖ Active areas may utilize turf, grasses, and ornamental plantings. Passive areas should primarily be composed of non-irrigated or drought-tolerant species.



*Examples of the treatment of private parks and open spaces envisioned in Renaissance.*

## Residential Design Guidelines

Building-level design guidelines provide important design criteria for structures within Renaissance.

### *General*

#### ❖ Architectural Style

- The choice of architectural expression must be derived primarily from the respective building typology (e.g. row towns, courtyard buildings, single-family homes, etc.). Architectural styles should be accurate and appropriate for the building typology.
- Use architectural elements that form an integral part of the building and avoid ornamentation and features that appear to be cheap and tacked on.

#### ❖ Building Orientation

- Use residential entrances to activate the street, and utilize elements such as canopies, porches, stoops, trellises, and courtyards as transitional spaces between the private and public realms.
- Orient buildings to face onto streets, parks, and open spaces/paseos. This orientation will create more attractive, safe, and pedestrian-friendly streetscapes and public spaces.

#### ❖ Variety

- Create identity and interest by varying floor plans and unit types.
- Adjacent homes of the same architectural style should not have identical elevations or colors. Rather, a rich variety of architectural styles, elevations, colors, and detailing is encouraged.
- Porches, detailed entries, courtyards, and stoops add to the character of a neighborhood and should be incorporated. These features should be varied along the street to create visual interest. If possible, these features should project forward of a front-entry garage door.
- Entry features, such as gates, trellises, arches, and arbors should be employed to add visual interest and variety within the neighborhood.
- Variation in floor plans, unit types, colors, and materials adds character and visual interest to a neighborhood. Two identical units may not be placed adjacent to each other.



*Varied architectural treatments create a unique living environment.*



*Examples of products oriented toward open space.*



*Use varied setbacks and architectural styles to create visual interest in a neighborhood.*

- ❖ Exercise creativity and individual expression in conceiving and interpreting architectural form.
- ❖ Apply massing breaks, such as entry courts, to promote visibility and allow block transparency. Create variety in building mass by providing adequate vertical and horizontal offsets.
- ❖ Environmental Considerations
  - Where possible, building articulation and form should be expressive of and driven by environmental and site conditions such as solar orientation, views, noise, prevailing winds and local climate. Plan forms that employ features such as courtyards, plazas, and patios are encouraged.
  - Builders are encouraged to incorporate sustainable design features.

### *Building Level*

- ❖ Enhanced Architectural Treatment
  - Enhanced architecture is encouraged for planning parcel edges along neighborhood streets and parks. Special architectural treatment such as tower elements, enhanced entries and roof forms, window trim exterior details, and wall finishes should be used at street corners and other important site locations, such as at the end of paseos and view corridors.
  - Buildings plotted at corner locations become important design features. These areas are focal points in the streetscape and as such should be places for architectural elements such as articulation, corner glazing, color, and material accents.
- ❖ Roofs
  - Roof forms of each home should be appropriate to the architectural style.
  - A variety of roof forms is encouraged to provide visual interest along collector streets.
  - Overly complex and distracting roof forms are discouraged.
- ❖ Garages
  - The front elevation should focus on the home, not the garage.
  - Garage wall planes on front elevations should be furred out a minimum of 6 inches on front facing garages.
  - As appropriate to the architectural style, garage door surrounds should be articulating with trellises, trim, enhanced materials, or



*Vary roof forms, materials, and colors as well as entry features to add variety to a neighborhood.*

# Design Guidelines

other methods to help minimize the architectural impact of the garage door.

- A mix of garage door patterns (including garage doors with lights) should be used to add variety.

## ❖ Colors and Materials

- Color schemes should be appropriate to the architectural style. Color should be used to add variety and richness to the architecture. Color changes should occur at inside corners only.
- Each elevation shall have a minimum of three colors.
- Each neighborhood shall have a minimum of three different roof colors.
- Use materials, colors, and details to enrich building character and emphasize human scale by employing rich, durable, and high quality finishes at the street level.
- Individual single-family homes shall not have identical color schemes adjacent to one another.

## ❖ Articulation and Detailing

- Articulate elements such as roof overhangs, canopies, and parapets to establish hierarchy and add interest to building silhouettes.
- Varied architectural detailing and projections should be used to accentuate specific features and ensure a visually pleasing and varied experience.
- Row townhouses and other buildings with the potential for long building walls shall provide articulation and variation in their façades through the use of features such as offsets, stoops, columns, window and door reveals, porches, and changes in height, materials and colors, and landscaping.
- Establish a clear pattern, and provide articulation and rhythm of windows, doors, and balcony openings, utilizing a variety of devices such as canopies, awnings, or railings.



*Example of varied use of material, corner treatment, and articulated façades, which add interest to a building.*

- ❖ Building interaction with the street level is important to a well-defined street scene. Stairways, entry stoops, balconies, arcades, colonnades, and retail storefronts will provide a strong transition from the street to each building.

- ❖ Trash collection, service, and loading areas must be located and designed so that service vehicles have clear and convenient access and do not block adjacent vehicular or pedestrian circulation or vehicular parking. Final location of loading/service areas will be approved by the City during the review process.

❖ Screening

- Storage and maintenance areas and other ancillary uses shall be screened from public view.
- Accessory structures, such as storage areas, refuse receptacles, mechanical equipment, parking structures, backflow preventers, loading docks, security fences, and similar uses can seriously detract from the visual quality of an area. Therefore, care must be taken to minimize the visual impact of these uses through site design and visual shielding. To the extent possible, these uses should be located away from roadways, behind buildings, or in enclosed structures, and screened from public views. Effective shielding methods include landscaping, berms, walls and fences, and ornamental screening.
- Accessory structures should be designed to look like a continuation or extension of the primary structure. They should have architectural detailing and landscaping similar to the primary structure.

❖ Multifamily Residential Treatment

- Utilize a variety of architectural features and detailing methods to differentiate adjacent units. Features such as articulated entry features (porches and stoops), bay windows, and chimneys are encouraged.
- Large multiple-family dwellings should have articulated façades, including recesses and architectural detailing, to avoid a monotonous streetscape.
- In multifamily developments, adjacent buildings should have varied setbacks, floor plans, color and material palettes, and architectural detailing.
- In multifamily developments, the architectural building edge should be used in place of walls whenever feasible.

## Design Guidelines

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## **Menu of Residential Architectural Styles**

The following is a menu of compatible architectural styles that are consistent with the buyer profile and landscape theme for Renaissance. The architectural styles have also been selected to provide variety and individuality for each neighborhood. The menu of architectural styles is not an exclusive list and other acceptable styles may be approved through the development review process with an accompanying description of style elements.

# Design Guidelines

## *Santa Barbara*

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Asymmetrical massing</li><li>• Simple plan</li><li>• Arched arcades</li></ul>
Entry	<ul style="list-style-type: none"><li>• Covered porch</li><li>• Recessed entry</li><li>• Arcade</li></ul>
Roof	<ul style="list-style-type: none"><li>• Gable roof with approximate pitch of 5:12</li><li>• Widely overhanging eaves (open)</li><li>• Tight rake</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Red tile roof</li><li>• Light sand stucco finish</li></ul>
Details	<ul style="list-style-type: none"><li>• Chimneys with decorative cap</li><li>• Decorative metal or wood balconies</li><li>• Decorative patterned tile</li><li>• Decorative metal window grills</li><li>• Textured doors</li><li>• Recessed windows</li></ul>

Typical Elevation



*Santa Barbara Style Elements*



*Textured wood door*



*Decorative patterned tile*



*Tight rake*



*Decorative chimney cap*



*Asymmetrical massing*



*Whitewashed stucco*



*Balcony*



*Recessed entry*

# Design Guidelines

## *Spanish Colonial*

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Asymmetrical massing</li></ul>
Entry	<ul style="list-style-type: none"><li>• Covered porch</li><li>• Arcade</li><li>• Detailed door surround</li></ul>
Roof	<ul style="list-style-type: none"><li>• Gable or hip roof with approximate pitch of 4:12–5:12</li><li>• Little or no overhang</li><li>• Exposed rafter tails</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete S-tile or barrel tile roof</li><li>• Light sand stucco finish</li><li>• White and light earthtone colors</li></ul>
Details	<ul style="list-style-type: none"><li>• Chimneys with decorative cap</li><li>• Decorative shutters</li><li>• Decorative metal window grills</li><li>• Vertical window proportions</li><li>• Arched window or door openings</li><li>• Textured wood doors</li></ul>

Typical Elevation



*Spanish Colonial Style Elements*



*Little or no roof overhang at rakes*



*Vertical window proportions*



*Decorative shutters*



*Decorative metal grills*



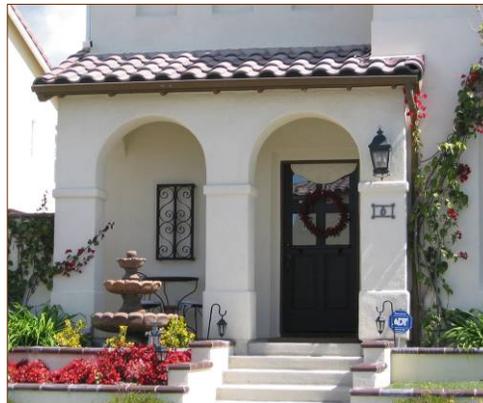
*Detailed door surround*



*Decorative chimney cap*



*Arched window/door openings*



*Covered arcade at entry*



*Asymmetrical massing*

# Design Guidelines

## Monterey

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Cantilevered covered balcony at the second floor</li></ul>
Entry	<ul style="list-style-type: none"><li>• Entry covered by balcony above</li></ul>
Roof	<ul style="list-style-type: none"><li>• Shallow gable roof with approximate pitch of 5:12</li><li>• Shed roof break over balcony with an approximate pitch of 3.5:12</li><li>• Exposed rafter tails</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete S-tile or flat tile roof</li><li>• Light sand stucco finish</li><li>• Brick or stone wainscot at the first floor</li></ul>
Details	<ul style="list-style-type: none"><li>• Chimneys with decorative cap</li><li>• Decorative metal or wood balconies</li><li>• Decorative patterned tile</li><li>• Decorative metal window grills</li></ul>

Typical Elevation



*Monterey Style Elements*



*Cantilevered balcony with articulated floor joists*



*Decorative shutters*



*Exposed rafter tails*



*Shallow gabled roof pitch*



*Second-story balcony*



*Balcony-covered entry*



*Siding at second floor*



*Wood corbels with decorative metal balcony railing*

# Design Guidelines

## *Tuscan Style*

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Informal building forms</li><li>• Asymmetrical massing</li></ul>
Entry	<ul style="list-style-type: none"><li>• Arched door treatments</li></ul>
Roof	<ul style="list-style-type: none"><li>• Shallow pitched gabled or hip roofs (approximately 5:12 pitch)</li><li>• Exposed rafter tails</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete S-tiles</li><li>• Light sand finish stucco in rich earthtone colors</li><li>• Rustic stone/brick veneer</li></ul>
Details	<ul style="list-style-type: none"><li>• Decorative shutters</li><li>• Decorative metal details</li><li>• Decorative metal or wood balcony railing</li><li>• Arched windows and window treatments</li></ul>

Typical Elevation



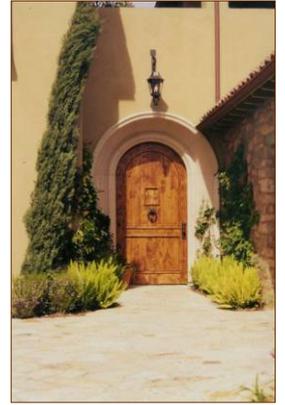
*Tuscan Style Elements*



*Rustic stone veneer*



*Asymmetrical massing*



*Recessed/arched door treatments*



*Arched windows*



*Balcony with decorative metal railings*



*Light sand finish stucco in rich earthtones*



*Decorative shutters*



*Decorative metal detail*



*Informal building forms*

# Design Guidelines

## *Italianate Style*

<b>Style Elements</b>	<b>Components</b>
Form/Massing	<ul style="list-style-type: none"><li>• Symmetrical or asymmetrical building massing</li><li>• Varied building massing</li></ul>
Entry	<ul style="list-style-type: none"><li>• Full or partial width front porch</li></ul>
Roof	<ul style="list-style-type: none"><li>• Shallow pitched gable or hip roofs (approximately 4:12 pitch)</li><li>• Large overhangs of 12–24” with decorative brackets below</li><li>• Parapet with detailed cornice</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete flat-tile or barrel tile</li><li>• Light sand finish stucco in rich earthtone colors</li><li>• Rustic stone veneer</li><li>• Quoins at corners, window, or entry doors</li><li>• Stone or brick accents</li></ul>
Details	<ul style="list-style-type: none"><li>• Dentals or brackets at eaves</li><li>• Paired or triple windows</li><li>• Pediment window and door treatments</li><li>• Rusticated base</li></ul>

Typical Elevation



*Italianate Style Elements*



*Asymmetrical building massing*



*Deep overhangs with decorative brackets below*



*Arched door treatment*



*Covered entry*



*Shallow-pitched hip roofs*



*Pediment window*



*Quoins*



*Dentils at eaves*



*Light sand finish stucco in rich earthtone colors*

# Design Guidelines

## *California Craftsman Style*

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Symmetrical or asymmetrical plan form</li></ul>
Entry	<ul style="list-style-type: none"><li>• Full-or partial-width front porch</li></ul>
Roof	<ul style="list-style-type: none"><li>• Shallow-pitched front, side, or cross gable roofs (approximately 4:12 pitch)</li><li>• Large overhangs of 12–24” with decorative brackets below</li><li>• Exposed rafter tails</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete flat-tile or asphalt-shingle roof</li><li>• Light sand finish stucco with siding accents</li><li>• Stone or brick accents</li></ul>
Details	<ul style="list-style-type: none"><li>• Battered, square, or 4-post columns</li><li>• Bracket or knee braces at gabled ends</li><li>• Paired or triple windows</li></ul>

Typical Elevation



*California Craftsman Style Elements*



*Stone accents*



*Exposed rafter tails*



*Battered columns with stone base*



*Stucco with siding accents*



*Shallow-pitched roofs with deep overhangs*



*Paired and tripled windows*



*Symmetrical or asymmetrical plan form*

# Design Guidelines

## *Provence Style*

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Symmetrical or asymmetrical building massing</li></ul>
Entry	<ul style="list-style-type: none"><li>• Arched door treatments</li><li>• Shallow porch</li><li>• Quoins at entry doors</li></ul>
Roof	<ul style="list-style-type: none"><li>• Steep pitched hip and gable roofs (approximately 6:12–12:12 pitch)</li><li>• Short 0–12” overhangs</li><li>• Curved/varying roof line</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete flat-tile roof</li><li>• Light sand finish stucco in rich earth tone colors</li><li>• Rustic stone veneer</li><li>• Quoins at corners, windows, and entry doors</li><li>• Stone and brick accents</li></ul>
Details	<ul style="list-style-type: none"><li>• Decorative shutters</li><li>• Decorative metal details</li><li>• Decorative metal or wood balcony railing</li><li>• Dormers</li><li>• Arched window and door treatments</li></ul>

Typical Elevation



*Provence Style Elements*



*Arched Window and door treatments*



*Curved and varying roof line*



*Decorative pot shelves*



*Decorative shutters*



*Rich earthtone colors*



*Rustic stone veneer*



*Shallow porch*



*Steep pitched hip and gabled roofs*

# Design Guidelines

## *European Cottage Style*

Style Elements	Components
Form/Massing	<ul style="list-style-type: none"><li>• Asymmetrical building massing</li><li>• Recessed 2<sup>nd</sup> or 3<sup>rd</sup> floor</li></ul>
Entry	<ul style="list-style-type: none"><li>• Articulated door surround or covered porch</li></ul>
Roof	<ul style="list-style-type: none"><li>• Hip and gable roofs (approximately 4:12–8:12 pitch)</li><li>• 16–18” overhangs</li><li>• Tight rake</li></ul>
Materials and Colors	<ul style="list-style-type: none"><li>• Concrete flat-tile or asphalt roof shingle</li><li>• Sand finish stucco</li><li>• Rustic stone veneer</li></ul>
Details	<ul style="list-style-type: none"><li>• Decorative shutters</li><li>• Roof dormers</li><li>• Chimneys</li><li>• Tower elements</li><li>• Planter boxes and pot shelves</li></ul>

Typical Elevation



*European Cottage Style Elements*



*Stone wainscot*



*Decorative pot shelves*



*Recessed second floor*



*Asymmetrical massing*



*Articulated door surround*



*Hip and gable roofs*



*Decorative shutters*



*Tight rake*

# Design Guidelines

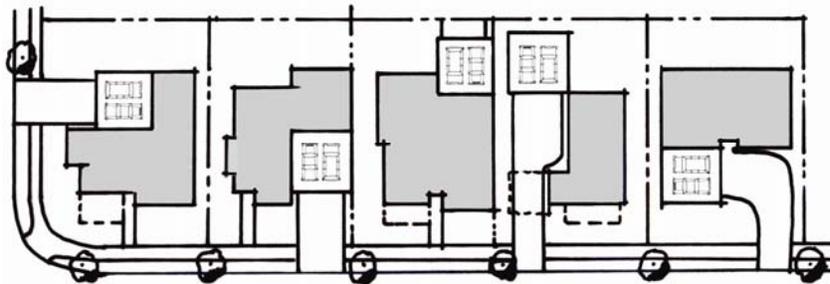
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## Residential Home Types

A variety of home types will create an interesting community and provide for a wide range of homebuyers and lifestyles. The following residential home types are examples of housing products that could be developed in Renaissance. This is not an exclusive list of home types and other products may be proposed. The following examples of home types are intended to provide the reader with an understanding of current (as of 2008) products that are typical of each density range.

<b>Single Family Detached</b>	
<i>Style Elements</i>	<i>Components</i>
Characteristics	<ul style="list-style-type: none"> <li>▪ Homes are oriented to face the street.</li> <li>▪ Alternating architectural styles, colors, and garage orientations create a more interesting streetscape.</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>▪ Front loaded or private drive loaded</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>▪ Sometimes</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>▪ Street facing</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>▪ Low Density Residential and Medium Density Residential</li> </ul>
Typical Photo	

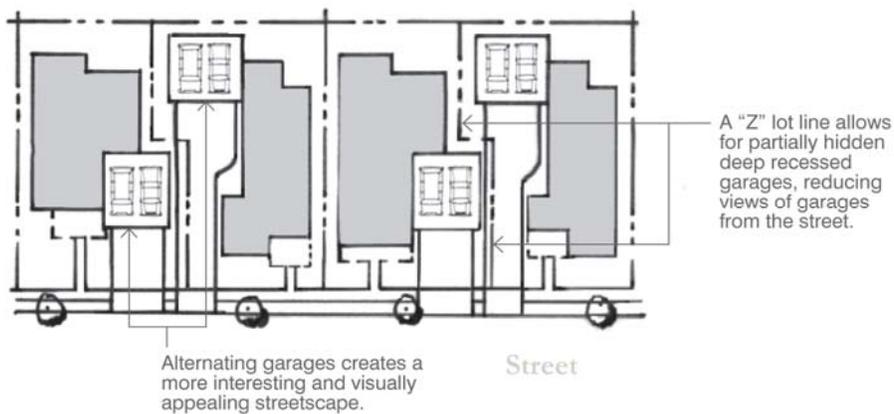
### Typical Site Plan



# Design Guidelines

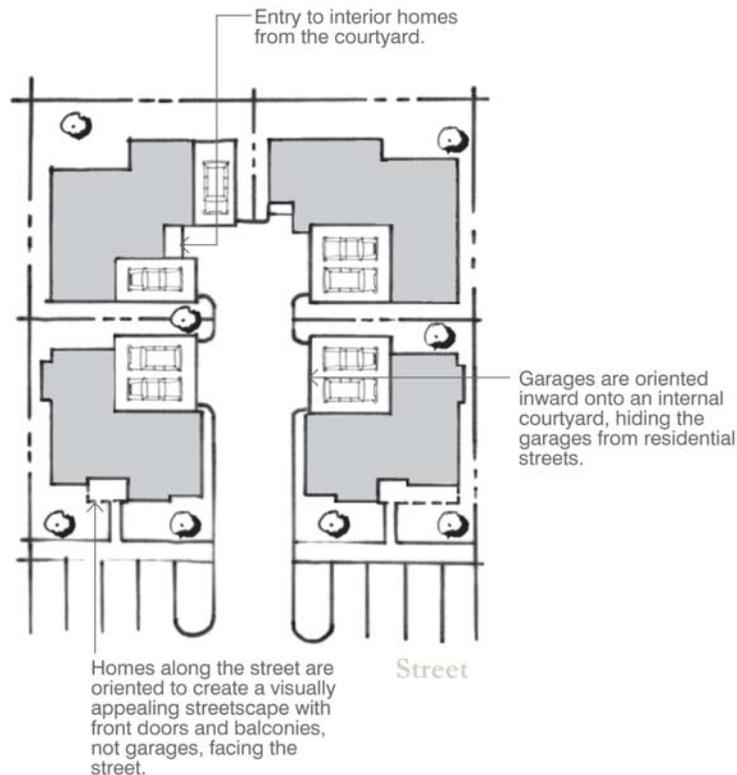
<b>2-Pack Single Family Detached</b>	
<b>Style Elements</b>	<b>Components</b>
Characteristics	<ul style="list-style-type: none"> <li>▪ A "Z" lot line allows for alternating garages to be deeply recessed, decreasing the visual effect of garages on the streetscape.</li> <li>▪ Garages are alternatively located in the front and rear portions of the lot, creating variation along the street front.</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>▪ Front loaded</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>▪ No</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>▪ Street facing</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>▪ Low Density Residential and Medium Density Residential</li> </ul>
Typical Photo	

## Typical Site Plan



<b>4-Pack Courtyard Single Family Detached</b>	
<b>Style Elements</b>	<b>Components</b>
Characteristics	<ul style="list-style-type: none"> <li>Four single-family detached homes clustered around an interior courtyard</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>Onto interior courtyard</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>No</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>Street facing for homes along street</li> <li>Into interior courtyard or onto greenbelts/paseos/parks for interior homes</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>Medium Density Residential</li> </ul>
Typical Photo	

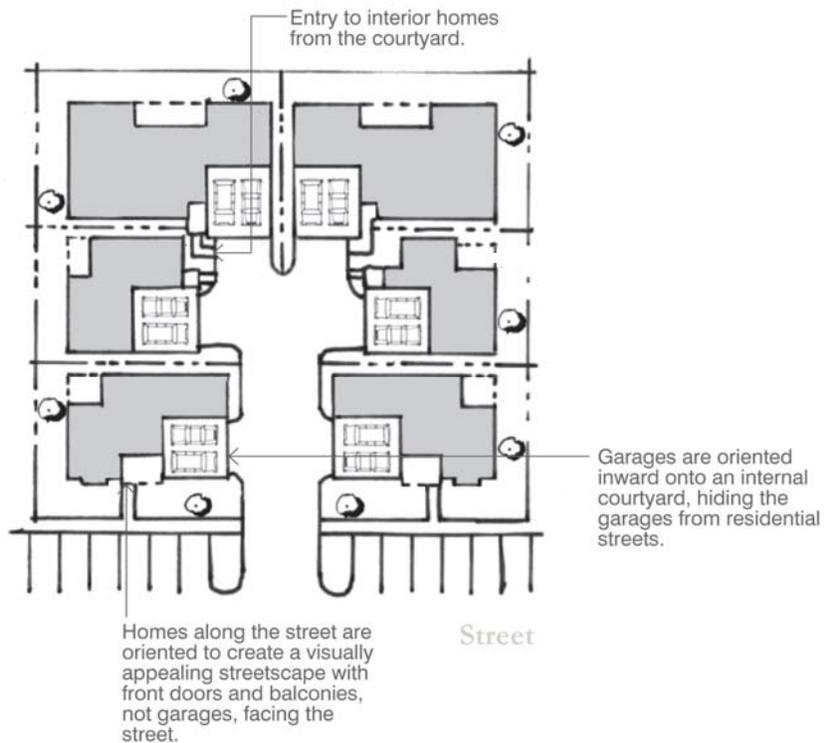
### Typical Site Plan



# Design Guidelines

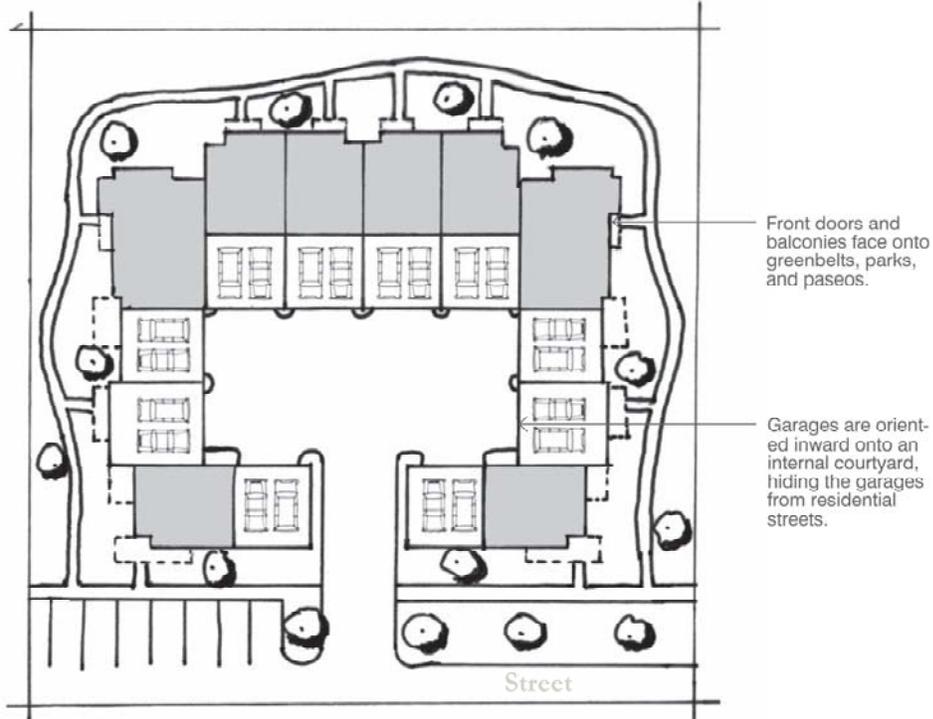
<b>6-Pack Courtyard Single Family Detached</b>	
<b>Style Elements</b>	<b>Components</b>
Characteristics	<ul style="list-style-type: none"> <li>▪ Six single-family detached homes clustered around an interior courtyard</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>▪ Onto interior courtyard</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>▪ No</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>▪ Street facing for homes along street</li> <li>▪ Into interior courtyard or onto greenbelts/paseos/parks for interior homes</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>▪ Medium Density Residential</li> </ul>
Typical Photo	

## Typical Site Plan



<b>Motorcourt Townhome</b>	
<i>Style Elements</i>	<i>Components</i>
Characteristics	<ul style="list-style-type: none"> <li>▪ Townhomes clustered around a central courtyard</li> <li>▪ Homes face onto greenbelts, paseos, or parks</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>▪ Onto interior courtyard</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>▪ No</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>▪ Street facing for homes along street</li> <li>▪ Onto greenbelts/paseos/parks for interior homes</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>▪ Medium Density Residential, Medium High Density Residential, and High Density Residential</li> </ul>
Typical Photo	

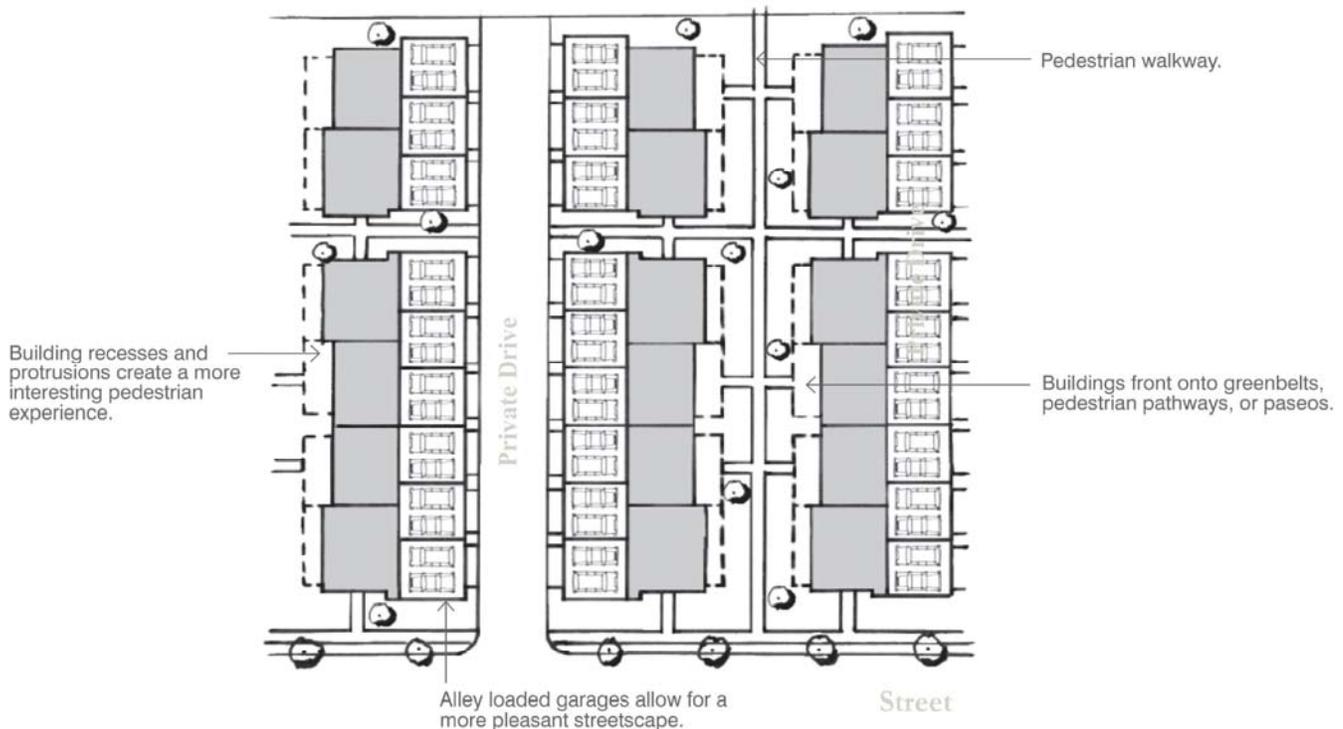
**Typical Site Plan**



# Design Guidelines

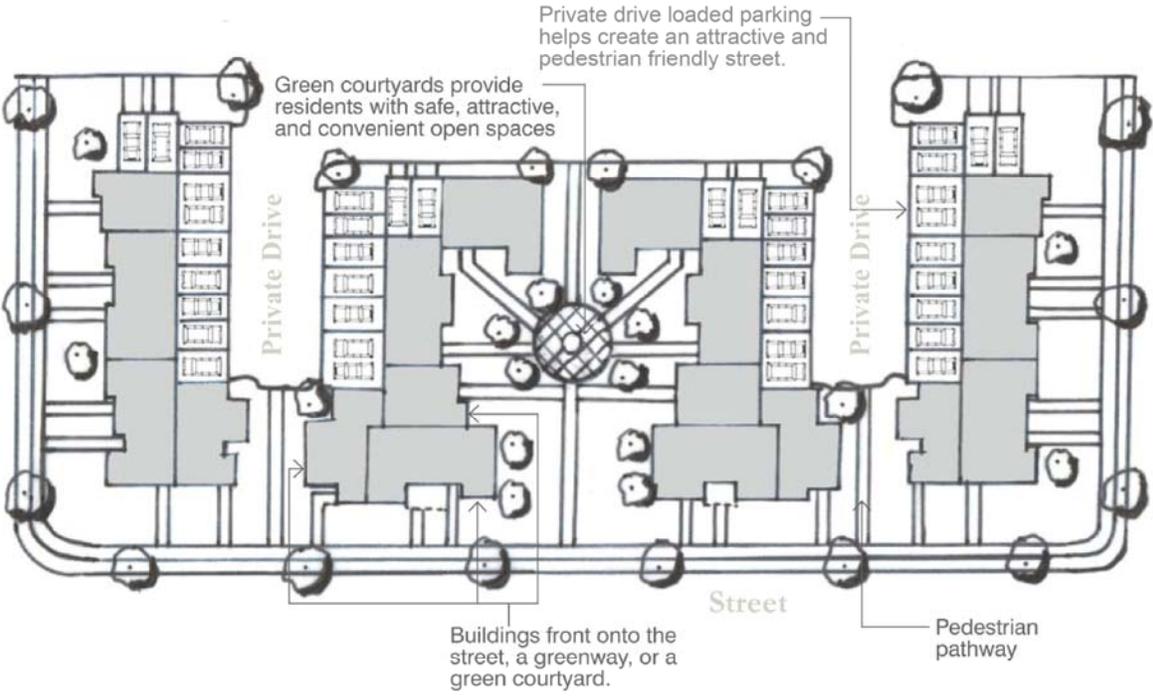
<b>Row Townhome</b>	
<i>Style Elements</i>	<i>Components</i>
Characteristics	<ul style="list-style-type: none"> <li>▪ Townhomes lined along private drives fronting onto a greenbelt or paseo</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>▪ Onto private drive</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>▪ Yes</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>▪ Street facing for homes along street</li> <li>▪ Onto greenbelts/paseos/parks for interior homes</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>▪ Medium Density Residential, Medium High Density Residential, and High Density Residential</li> </ul>
Typical Photo	

## Typical Site Plan



<b>Greencourt Townhome</b>	
<i>Style Elements</i>	<i>Components</i>
Characteristics	<ul style="list-style-type: none"> <li>▪ Townhomes clustered around a central courtyard</li> <li>▪ Homes face onto greenbelts, paseos, or parks</li> <li>▪ Often separated by green pedestrian pathways/paseos</li> </ul>
Garage Orientation	<ul style="list-style-type: none"> <li>▪ Onto interior private drive</li> </ul>
Private drives	<ul style="list-style-type: none"> <li>▪ Yes</li> </ul>
Front Door Orientation	<ul style="list-style-type: none"> <li>▪ Street facing for homes along street</li> <li>▪ Onto greenbelts/paseos/parks for interior homes</li> </ul>
Land Use Categories	<ul style="list-style-type: none"> <li>▪ Medium Density Residential, Medium High Density Residential, and High Density Residential</li> </ul>
Typical Photo	

**Typical Site Plan**



# Design Guidelines

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Signs

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SECTION FIVE



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## SECTION 5: SIGNS

The purpose of this section is to promote an overall “sense of place” through signage that is architecturally integrated, visually coordinated, balanced, and appealing, while conforming to applicable code requirements. The signage and graphics described in this section have been designed to enhance Renaissance and encourage unity within its various villages and neighborhoods.

### Definitions

*Sign.* Any arrangement of letters, words, numerals, emblems, artwork, pictures, paintings, or other design shown, superimposed, suspended from, or incised into any surface of any kind and used as a display or notice, pictorial or otherwise, for the purpose of showing identity, advertising available services and products, or for providing instructions, direction, or information.

*Advertising Sign.* On or off-site sign structures that direct attention to the goods or services sold, leased, or otherwise provided and made available, which shall include the name of the leasehold premises and may include names or sub-tenancies located thereon.

*Awning Sign.* A message integrated into the surface of an architectural awning structure mounted parallel to the building façade.

*Blade Sign.* A wall-mounted projecting or canopy-suspended sign at the pedestrian level adjacent to a building entry.

*Construction Sign.* A sign stating the names of those individuals or businesses directly connected with the construction of a project, their addresses and their telephone numbers.

*Directional Sign.* A sign that contains words such as “entrance,” “enter,” “exit,” “in,” “out,” or other similar words, or a sign containing arrows or characters indicating traffic directions.

*Façade.* The exterior wall of a building exclusive of projecting signs, columns, pilasters, canopies, marquees, decorations, or the like.

*Identification Sign.* A sign that advertises or directs attention to an occupancy, major business, commodity, service, or entertainment conducted, sold, or offered only upon the premises where the sign is located

# Signs



Examples of monument signs.



Example of a pylon sign

**Marquee.** A rigid canopy extending outward from the building façade, generally over the main entrance or along a principal façade.

**Monument Sign.** Signs that are freestanding, integrated into the landscape, and provide primary or secondary identification of tenants.

**Multiface Sign.** Signs having more than one face, each of which fronts in a different direction.

**Parapet.** That portion of the exterior wall of a building occurring above the roof.

**Permanent Sign.** Signs of substantial, durable materials and finishes intended for long-term use.

**Promotional Activity Sign.** Temporary banners, flags, pennants, or signs (portable or otherwise) intended to be displayed to advertise an initial opening of a business, change in ownership, special merchandise sales or promotional activities, sales specials, and special events such as seasonal events, grand openings, subject to approval of a temporary sign permit by the planning division

**Portable Sign.** A sign which is not designed to be permanently attached to a structure or to the ground. A portable sign is considered a Promotional Activity Sign for purposes of this Specific Plan.

**Pylon Sign.** Signs that are freestanding and provide site and major tenant identification oriented to freeways, principal vehicle thoroughfares, and entries.

**Real Estate Sign.** Temporary signs that advertise lots or structures for sale.

**Sign Area.** Shall be the sum of the areas enclosed within parallelograms drawn around each letter and/or pictorial or architectural embellishment. Where letters or embellishments are connected, as in script writing, the parallelograms shall encompass each group connected. Where a frame or backing for the letters, embellishments, etc., constitutes an integral part of the sign, the total area enclosed shall be considered.

**Temporary Sign.** Those intended for short duration, normally during the planning and construction phase of development.

**Vehicle Signs.** Advertising signs on or affixed to trucks, automobiles, trailers, or other vehicles parked on the property.

## General Sign Criteria

### *Design*

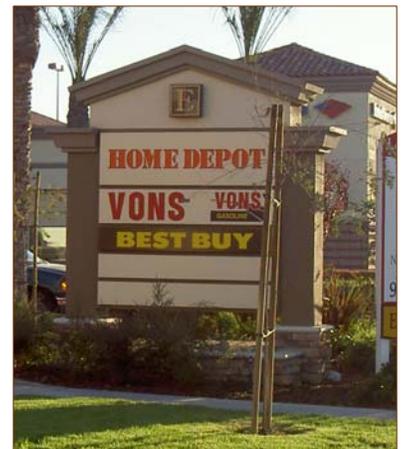
- ❖ The City of Rialto must approve all signs based on these sign criteria.
- ❖ Signs shall be in keeping with the character established for Renaissance with variations to include individual tenant identities.
- ❖ Residential identity signs shall adhere to the comprehensive sign program approved for each project, if applicable.
- ❖ Except as specifically permitted herein, signs shall be limited to the display of the business name, federal or state-registered name, logo, and/or corporate logo or trademark providing the signs conform to the criteria described herein.
- ❖ Logo cabinet plaques used in conjunction with individual letters will be considered a part of the sign area.
- ❖ The dimension and shape of signs mounted on building façades or marquees shall be in proportion to the dimensions of the surface on which mounted.
- ❖ Where more than one identification, advertising, or directional sign occurs on the premises, they shall be of nearly identical size, shape, and/or scale.
- ❖ Double or multiface signs shall count as one unit when computing number of signs allowed. The area of one face only shall be used in computing sign area of double or multiface signs.

### *Materials*

- ❖ Attached signage materials should be of a high quality such as stainless steel, acrylic, aluminum, painted metal, or high quality plastic.
- ❖ Appropriate materials for monument signs include natural stone, brick, masonry, metal, glass, Lexan high-density plastic, and other high-quality materials. The material selected for the base of the sign should be the same as the primary material used for the building façade.

### *Location*

- ❖ All signs shall either be contained on the legal parcel to which applicable or, if part of an integrated center, within perimeter of the center itself, except as allowed in Table 3-9 for the Town Center.
- ❖ Signs shall be located to prevent obstruction and impair vision of streets and sidewalks. As measured from the back-of-curb, no sign shall be located within 5 feet of a local street or within 8 feet of any other street or highway.



*An example of a monument sign located within a landscaped area*

## *Prohibited Signs*

### *Prohibited Designs*

- ❖ Any sign constituting a traffic hazard.
- ❖ Immoral or unlawful advertising.
- ❖ Illuminated awnings and rooftop signs.
- ❖ Digital pylon and monument signs, except as provided in this Specific Plan.
- ❖ Sign that imitates any traffic sign or signal that interferes with, misleads, or confuses traffic.
- ❖ Signs advertising merchandise or services carried by tenants.
- ❖ Moveable/portable signs, except as provided in this Specific Plan.
- ❖ Illuminated signs or light device directed upon a public street, highway, sidewalk, or adjacent residence that may constitute a traffic hazard or nuisance.
- ❖ Temporary portable-type signs within the public right-of-way.
- ❖ Permanent or temporary advertising devices such as attraction boards, posters, banners, and flags, except where specifically approved by the City.
- ❖ Animated, flashing, or audible signs.
- ❖ Signs consisting of any moving, swinging, rotating, flashing, blinking, scintillating, fluctuating, or otherwise animated lighting.
- ❖ Light bulb strings for external displays, other than temporary decorative holiday lighting.
- ❖ Vehicle signs.

### *Prohibited Materials*

- ❖ Luminous vacuum-formed acrylic signs and letters, including pre-manufactured and franchise signs.
- ❖ Paper, cardboard, or Styrofoam, stickers, or decals, except for vinyl lettering on doors and windows that provide store name, hours of operation, and contact information.
- ❖ Inflatable displays, of any size or type.
- ❖ Simulated materials (e.g., wood-grained plastic laminates) or wall coverings.
- ❖ Box-type signs except for federally registered logos.
- ❖ Banners attached to a building by screws, nails, or anchors of any type.
- ❖ If tenant signage consists of a large logo, a frame or backing for channel letters may be used subject to City approval. Channel letters may be mounted to contrasting colored background materials such as plaster, tile, or metal. The background shall not be internally illuminated and is subject to City approval.
- ❖ Channel lettering with any exposed neon or other lighting.
- ❖ Letters with exposed fastening and unfinished edges (unless architecturally consistent).

## Sign Regulations

### *Directional Signs*

- ❖ Directional signs shall be located at appropriate vehicular or pedestrian decision points as necessary.
- ❖ Vehicular directional signs shall be consistent in size, shape, and design throughout a project.
- ❖ Vehicular directional signs should be legible and have enough contrast to be read through a windshield from an appropriate distance.
- ❖ Vehicular directional signs shall incorporate reflective vinyl copy for nighttime illumination.
- ❖ Directional signs shall not contain any advertising or trade name information.
- ❖ Vehicular signs should have no more than three messages per sign.
- ❖ All directional signs should incorporate the appropriate symbols as established by the Society for Environmental Graphic Design (SEGD) and comply with all state, local, and federal regulations.
- ❖ Directional signs may be double-faced and shall not exceed 10 square feet per face or 5 feet in height as measured from the adjacent grade.



*Examples of appropriate directional signage.*

### *Informational Signs*

- ❖ Wall-mounted or freestanding directories are appropriate within the Town Center or any residential district.
- ❖ Parking informational signs should be located at entrances for any structured parking.
- ❖ Informational signs may be double-faced and shall not exceed 8 square feet per face or 4 feet in height as measured from the adjacent grade.



*An example of informational signage.*

### *Regulatory Signs*

- ❖ Regulatory signs required within the project may include the following: nonsmoking, no parking, do not enter, no dogs, no skateboarding, and accessibility-related (ADA) signs.
- ❖ Vehicle regulatory signs, including stop, yield, do not enter, wrong way, speed limit, no parking, and one way, shall be authorized by the City and conform with the Manual on Uniform Traffic Control Devices. A traffic engineering consultant is required to verify location of regulatory signs on public rights-of-way.
- ❖ All code-required signs for items such as restrooms, telephones, fire extinguishers, elevators, escalators, and stairs shall incorporate the appropriate international symbols as established by the Society of Environmental Graphic Design (SEGD).



*An example of regulatory signage.*

# Signs

- ❖ Signs within the project must be in compliance with the following city and national guidelines and shall be approved by the appropriate City department:
  - CalDAG 96 – Combined ADA and CA Title 24
  - AASHTO Part 1 Guidelines for Supplemental signs
  - MUTCD – Manual of Uniform Traffic Control Devices



An example of amenity signage.

## *Amenity Signs*

Ground amenity signs are permitted which utilize materials and colors conforming to the character of Renaissance.

## *Signs in the Employment and Business Center Land Use Categories*

Businesses in the Employment and Business Center land use categories are allowed the following signs:

- ❖ Wall sign on each elevation facing a parking lot or street
- ❖ Single- or multitenant monument sign

## *Wall Signs (Single Tenant)*

- ❖ Single-tenant buildings are allowed one internally illuminated channel letter wall sign per street or parking lot frontage.
- ❖ Single-tenant buildings are allotted 2 square feet of wall signage per linear foot of street or parking lot frontage.
- ❖ Single-tenant buildings are allowed a maximum overall letter height of 4 feet.
- ❖ The total area of wall signs for single-tenant buildings shall not exceed 80 percent of any street or parking lot frontage.
- ❖ The maximum length of wall signs for single-tenant buildings shall not exceed 80 percent of any street or parking lot frontage.
- ❖ For wall signs in multitenant buildings, see the requirements for Corporate Center, Freeway Commercial, Commercial, and Freeway Incubator land use categories below.



An example of a multitenant monument sign.

## *Monument Signs*

- ❖ One monument sign is allowed per parcel or multitenant building/center.
- ❖ Monument signs are allowed a maximum sign area of 80 square feet per side and a maximum height of 8 feet.
- ❖ All letters shall be individually mounted and shall not exceed 18 inches in height.
- ❖ Monument signs shall be externally illuminated and located within a landscaped area.

***Signs in the Corporate Center, Freeway Commercial, Town Center, General Commercial, and Freeway Incubator Land Use Categories***

Businesses in the Corporate Center, Freeway Commercial, Town Center, General Commercial, and Freeway Incubator land use categories and multitenant signs in the Employment and Business Center land use categories are allowed the following signs:

- ❖ Single- or multitenant monument sign
- ❖ Wall sign on each elevation facing a parking lot or street
- ❖ Blade signs. Require 8 foot clearance above grade.
- ❖ Multitenant pylon signs
- ❖ Vinyl lettering on the storefront showing the hours of operation, emergency contact numbers, etc. Maximum area of these signs shall be 2 feet by 2 feet.

***Monument Signs***

- ❖ For businesses on individual parcels (not part of a multitenant building/center), one monument sign is allowed per parcel.
- ❖ For multitenant building/center, one monument sign per driveway entry.
- ❖ Monument signs are allowed a maximum sign area of 80 square feet per side and a maximum height of 8 feet.
- ❖ All letters shall not exceed 18 inches in height.
- ❖ Monument signs may be internally and externally illuminated and located within a landscaped area.

***Signs on SR-210***

- ❖ One freestanding pylon sign is permitted for each multitenant center or stand alone parcel (not within a multitenant center) located immediately adjacent to the SR-210.
- ❖ Freestanding multitenant pylon signs are allowed a maximum sign area of 750 square feet per face (a maximum of 1,500 square feet of sign area for double-faced signs) and a maximum height of 50 feet. Freestanding single-tenant pylon signs are allowed a maximum sign area of 375 square feet per face and a maximum height of 50 feet.
- ❖ Freeway-adjacent, off-site pylon signs are permitted in the approximate locations depicted on Figure 5-1. These pylon signs may be a maximum 750 square feet per face and a maximum height of 50 feet and may be digital. The location of the pylon signs may be adjusted with approval of the Development Review Committee.
- ❖ For wall signs on secondary elevations that face the freeway, the letter height and sign area may be increased to match that of the primary entry elevation.



*Examples of pylon signage, which is appropriate along SR-210.*

## *Wall Signs*

### **Primary Tenant (over 75,000 square feet)**

#### ❖ Primary entry elevations:

- 2.5 square feet of wall signage per linear foot of leasehold frontage.
- One internally illuminated channel letter wall sign with a maximum letter height of 6 feet, a maximum 18.5-inch second line of copy below, and up to a 12 foot tall logo.
- Up to three secondary wall signs displaying additional services (e.g. car service, pharmacy, etc...) with a maximum letter height of 12 inches and a maximum sign area of 15 square feet.

#### ❖ Secondary elevations (side and rear elevations):

- One square foot of wall signage per linear foot of secondary elevation leasehold frontage.
- One internally illuminated channel letter sign on each visible secondary elevation with a maximum letter height of 4 feet.

#### ❖ General criteria (applicable to all wall signs):

- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- One sign per co-brand subtenant, e.g., bank or coffee provider, is permitted as described in the Subtenant section below.
- All signage shall be located within the space and surface specifically provided for signage on the building.
- If a primary tenant has a nationally recognized logo and/or established signage layout that cannot be modified to meet the criteria standards, the Planning Commission may approve a larger sign area and letter height if the sign is well designed and is in keeping with the character of the center.

### **Major Tenant (19,000 to 74,999 square feet)**

#### ❖ Primary entry elevations:

- Two square feet of wall signage per linear foot of primary entry leasehold frontage.
- One internally illuminated channel letter wall sign on the primary entry elevation with a maximum height of 5 feet for a single-line copy sign and a maximum height of 6 feet for any double-line copy sign.

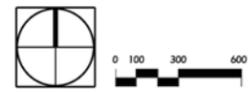
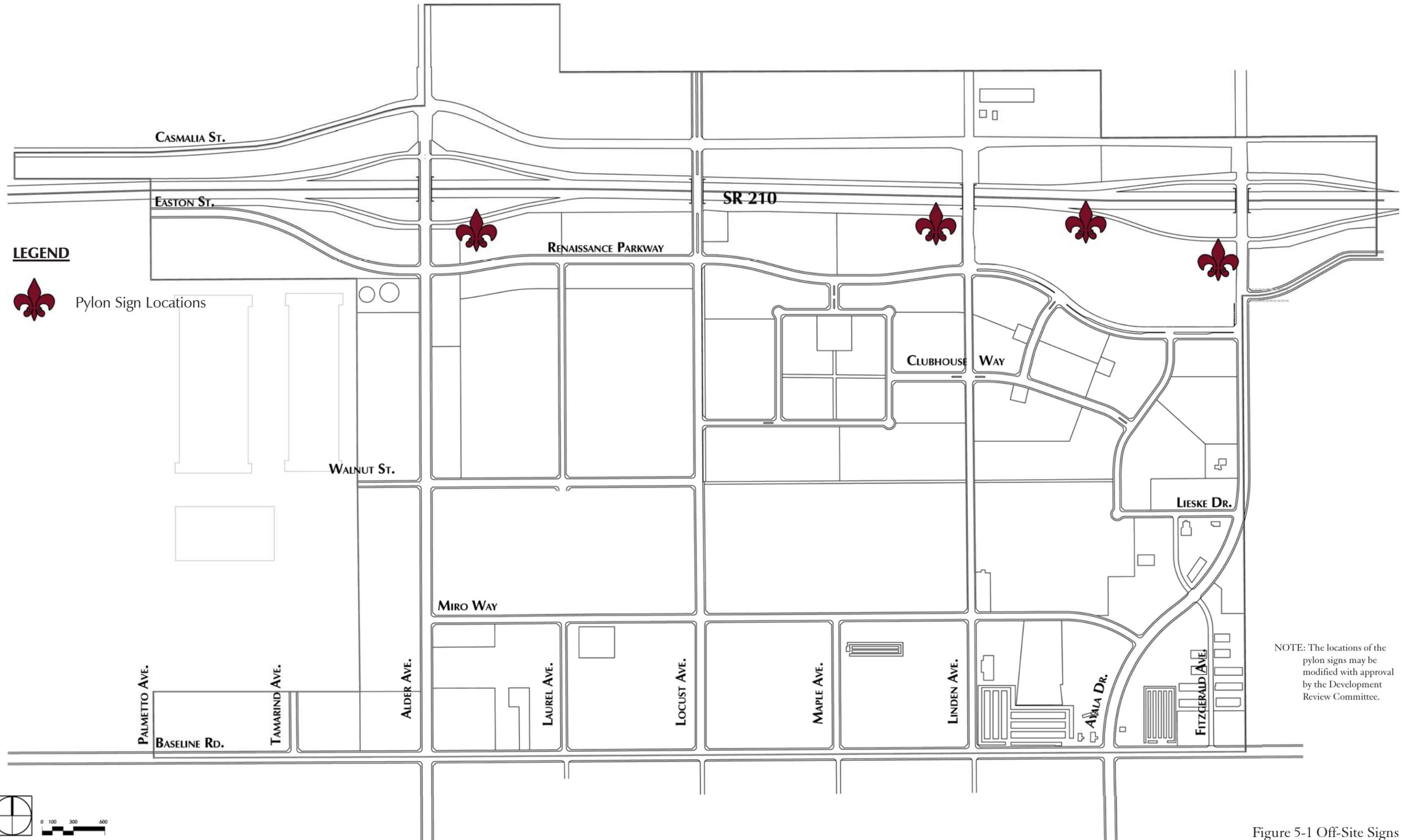


Figure 5-1 Off-Site Signs

## Signs

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❖ Secondary elevations (side and rear elevations):

- One square foot of wall signage per linear foot of secondary elevation leasehold frontage.
- One internally illuminated channel letter sign on each visible secondary elevation with a maximum letter height of 3 feet.

❖ General criteria (applicable to all wall signs):

- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- One sign per co-brand subtenant, e.g., bank or coffee provider, is permitted as described in the Sub-Major Tenant section below.
- All signage shall be located within the space and surface specifically provided for signage on the building.

### **Sub-Major Tenant (10,000 to 18,999 square feet)**

❖ Primary entry elevations:

- 1.5 square feet of wall signage per linear foot of primary entry leasehold frontage.
- One internally illuminated channel letter wall sign on the primary entry elevation with a maximum height of 4 feet for a single-line copy sign and a maximum height of 5 feet for any double-line copy sign.

❖ Secondary elevations (side and rear elevations):

- One square foot of wall signage per linear foot of secondary elevation leasehold frontage.
- One internally illuminated channel letter sign on each visible secondary elevation with a maximum letter height of 3 feet.

❖ General criteria (applicable to all wall signs):

- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- All signage shall be located within the space and surface specifically provided for signage on the building.

## **Retail Tenant (5,000 to 9,999 square feet)**

### ❖ Primary entry elevations:

- 1.5 square feet of wall signage per linear foot of primary entry leasehold frontage.
- One internally illuminated channel letter wall sign on the primary entry elevation with a maximum height of 3'-6" for a single-line copy sign and a maximum height of 4'-6" for any double-line copy sign.

### ❖ Secondary elevations (side and rear elevations):

- One square foot of wall signage per linear foot of leasehold frontage.
- One internally illuminated channel letter wall sign on each visible elevation with a maximum height of 30".

### ❖ General criteria (applicable to all wall signs):

- No more than three signs are allowed per business.
- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- All signage shall be located within the space and surface specifically provided for signage on the building.

## **In-Line Shop Tenant (up to 4,999 square feet)**

### ❖ All visible leasehold frontages:

- One square foot of wall signage per linear foot of leasehold frontage.
- One internally illuminated channel letter wall sign per elevation with a maximum height of 3 feet for a single-line copy sign and a maximum height of 3'-6" feet for a double-line copy sign.

### ❖ General criteria (applicable to all wall signs):

- No more than three signs are allowed per business.
- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- All signage shall be located within the space and surface specifically provided for signage on the building.

### **Pad Building/Multitenant**

❖ All visible leasehold frontages:

- One square foot of wall signage per linear foot of leasehold frontage.
- One internally illuminated channel letter wall sign per elevation with a maximum height of 2 feet for a single-line copy sign and a maximum height of 30” for a double-line copy sign.

❖ General criteria (applicable to all wall signs):

- No more than three signs are allowed per business.
- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- If the business has a nationally recognized logo and/or established signage layout that cannot be modified to meet the height standard, a maximum logo/copy height of 3 feet may be permitted.
- All signage shall be located within the space and surface specifically provided for signage on the building.

### **Pad Building/Single User**

❖ Primary entry elevations:

- 1.5 square feet of wall signage per linear foot of primary entry leasehold frontage.
- One internally illuminated channel letter wall sign on the primary entry elevation with a maximum height of 3 feet for a single-line copy sign and a maximum height of 4 feet for any double-line copy sign.

❖ Secondary elevations (side and rear elevations):

- One square foot of wall signage per linear foot of leasehold frontage.
- One internally illuminated channel letter wall sign on each visible secondary elevation with a maximum height of 30” for a single-line copy sign and a maximum height of 3 feet for any double-line copy sign.

❖ General criteria (applicable to all wall signs):

- No more than four signs are allowed per business.

# Signs

- The total area of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- The maximum length of wall signs shall not exceed 80 percent of any street or parking lot frontage.
- If the business has a nationally recognized logo and/or established signage layout that cannot be modified to meet the height standard, a maximum logo/copy height of 3 feet may be permitted.
- All signage shall be located within the space and surface specifically provided for signage on the building.

## **Fuel Center/Pad Tenant**

- ❖ A fuel center shall be permitted four internally illuminated channel letter and/or logo signs, a maximum of one sign per elevation. For example, this means that one canopy sign can be exchanged for a sign on the kiosk, convenience store, or carwash building or visa-versa.
- ❖ One square foot of wall signage per linear foot of frontage.
- ❖ Maximum letter/logo height is 2'-6".
- ❖ Signs shall not exceed 30 percent of the leasehold width per elevation.
- ❖ Nonilluminated graphic striping shall not be considered signage.
- ❖ No backlit or otherwise lit colored stripe of any sort will be allowed around the canopy or building.
- ❖ All signage shall be located within the space and surface specifically provided for the building.
- ❖ One double-faced, monument sign is allowed per parcel to identify the tenant with a maximum sign area of 60 square feet per side and a maximum height of 6 feet. All letters shall be individually mounted and shall not exceed 18 inches in height.
- ❖ In addition, two double-faced, monument style, fuel pricing signs are allowed per parcel each with a maximum sign area of 50 square feet per side and a maximum height of 5 feet.
- ❖ Monument signs shall be externally illuminated and located within a landscaped area.

## ***Residential Sign Standards***

- ❖ Residential neighborhoods or multifamily developments are allowed the following signs:
  - Monument sign (at vehicular entrances to developments)
  - Entry wall or façade identity sign
  - Building/tenant address system
- ❖ Residential neighborhoods or multifamily developments are allotted two monument signs per street frontage to identify project name and address.

- ❖ Externally illuminated monument signs are permitted within a landscaped area, with a maximum sign area of 50 square feet, a maximum height of 5 feet, and individually mounted letters not to exceed 18 inches in height.
- ❖ Façade or other wall-mounted signs shall be externally illuminated, with a maximum sign area of 50 square feet, and individually mounted letters not to exceed 18 inches in height.
- ❖ Any external lighting structures shall be hidden from the public line of sight.
- ❖ Signage is not permitted on individual residences.

### *Temporary Signs*

The Planning Division may issue a temporary sign permit valid for up to 60 days, if it is found that the temporary sign is necessary to establish or maintain identity until a permanent sign can be erected. Extensions of the 60 day permit may be granted at the discretion of the Planning Division. The Planning Division may also approve a temporary sign permit for the following temporary signs:

- ❖ Signs necessary to avoid a dangerous condition, including directional signs during construction.
- ❖ A-frame signs.
- ❖ Signs pertaining to a use permitted by a temporary use permit.
- ❖ Branded construction fence or storefront barricade may be used as a communication device to generate excitement for the project.

### *Promotional Activity Signs*

Temporary banners, flags, pennants or signs (portable or otherwise) intended to be displayed to advertise an initial opening of a business, change in ownership, special merchandise sales or promotional activities, subject to approval of a temporary sign permit by the planning division and the following provisions:

- ❖ Promotional activity signs may be placed on a site subject to the approval of the Director of Development Services, provided that temporary signs comply with this section, do not create safety hazards, or block signs identifying adjoining establishments.
- ❖ A temporary sign permit for promotional activity banners and pennant(s) shall be valid for a maximum 30 days in any 90-day period.
- ❖ A promotional activity sign or signs shall not exceed one square foot of banner area for each linear foot of building frontage and in no case shall the total banner area exceed 100 square feet. Temporary window signs totaling no more than 4 square feet each are permitted.
- ❖ A temporary sign permit for grand opening promotional activities shall be allowed for 90 days, and the permit is not renewable. A promotional activity banner, as permitted above, shall not be affected by the issuance of a grand opening sign permit during the same calendar year. The size of a grand opening promotional activity sign shall not exceed 100 square feet.

# Signs

- ❖ Promotional activity signs shall not be in a condition of disrepair. Disrepair shall include torn, faded, or sagging signs. Any unmaintained or damaged signs shall be repaired or replaced within 10 days following notification by the planning division.

## *Tenant Responsibilities*

- ❖ Tenants shall be responsible for fulfillment of all requirements of the sign criteria.
- ❖ The tenant shall pay for all signs and their installation, including final connections and maintenance.
- ❖ Tenant's sign contractor must file and obtain any licenses and permits as required for sign installation by the City.
- ❖ Each tenant or its sign contractor shall be responsible for the repair of any damage to the building caused by the installation of said tenant's sign. Only state-licensed and-insured sign contractors shall perform sign manufacturing and installations.
- ❖ Each tenant shall be responsible for the performance of its sign contractor.
- ❖ Tenant shall be responsible for removal of said sign within 30 days after vacating the site. Removal of the sign shall include the repair and restoration of the wall surface back to its original condition.
- ❖ Each tenant shall be responsible for keeping its signage in good working order. See Sign Maintenance guidelines in this section.

## *Fabrication and Installation*

### *General Fabrication Specification*

- ❖ It is intended that all finished work be of high quality.
- ❖ Construct all work to eliminate burrs, dents, cutting edges, and sharp corners.
- ❖ Finish welds on exposed surfaces shall be imperceptible in the finished work.
- ❖ Surfaces that are intended to be flat shall be without dents, bulges, oil canning, gaps, or other physical deformities.
- ❖ Except where approved otherwise, conceal all fasteners.
- ❖ Make access panels tight fitting, light proof, and flush with adjacent surfaces.
- ❖ Carefully follow manufacturers' recommended fabrication procedures regarding expansion/contraction, fastening, and restraining of acrylic plastic.
- ❖ Assure that painted, polished, and plated surfaces are unblemished in the finished work.
- ❖ Wall signs shall be affixed without visible means of attachment, unless attachments make an intentional statement.

### *Suggested Sign Constructions*

The following construction methods are permitted; other conditions are subject to approval:

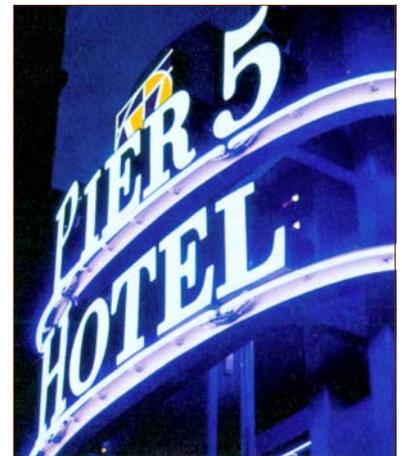
- ❖ Aluminum channel letters with internally illuminated faces.
- ❖ Pin-mounted dimensional halo-illuminated letterforms.
- ❖ Reverse-channel metal halo-illuminated letterforms with additional externally lit face.
- ❖ Metal channel halo-illuminated letterforms with illuminated face.
- ❖ Punched metal face with push-through acrylic letterforms. (Monument sign use only.)
- ❖ Punched metal face with flush acrylic letterforms. (Monument sign use only.)
- ❖ Painted metal channel letters with vinyl acrylic face.

### *Sign Illumination*

- ❖ All sign elements must be internally and/or externally illuminated. Hot spots and light leaks are not permitted and must be repaired. All illuminated signs shall be fabricated, installed, and compliant with national/local building and electrical codes and shall bear the U.L. label. All signs shall conceal all identification labels and U.L. labels to conform to U.L. codes. All conductors, transformers, cabinets, housings, and other equipment shall be concealed and/or incorporated into storefront and/or sign components.
- ❖ To protect the visual environment, all leaseholders' light fixtures, with regard to brightness and glare, shall be subject to approval by the City.
- ❖ Any illuminated sign or lighting device shall emit a constant intensity of light and no sign shall be illuminated by or contain flashing, intermittent, rotating, or moving lighting.
- ❖ Lighting is required to be circuited and switched separately from other building fixtures on the leaseholders' panel and controlled by a time clock. Leaseholder shall provide a disconnect switch at sign transformer or near electrical junction box.

### *Sign Maintenance*

- ❖ All signs shall be kept in "like new" condition and shall be promptly restored to such condition if damaged or otherwise marred. Copy and text employed on signs shall be kept accurate and current.
- ❖ City and/or property owner shall inform tenant, in writing, of non-acceptable conditions, if lighting is out, or if the sign is in poor visual condition. Repairs shall be made by tenant within 30 days from receipt of such written notice.



*Examples of appropriate illumination of signage.*

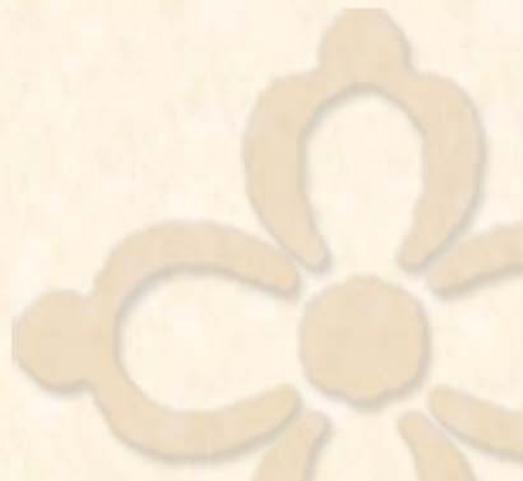
# Signs

## *Nonconformance*

- ❖ No field installation changes are permitted without first notifying the City in writing. If the placement or location of any sign differs from the plan, the tenant's sign contractor shall be responsible to remove and relocate the sign to its proper placement at the sign company's expense. The sign company shall also repair the façade from which the sign was moved.
- ❖ Any sign installed by tenant that does not conform to the approved drawings shall be corrected by the tenant within 15 days after written notice by the City. In the event that the tenant's sign is not brought into compliance within this 15-day period, then the landlord and/or City shall have the option to correct said sign at tenant's expense.

# Sustainability

## SECTION SIX



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## SECTION 6: SUSTAINABILITY

All development within Renaissance shall be reviewed for conformance with the provisions of this Chapter during the development review process.

### Overarching Performance Standard

- ❖ Development in Renaissance shall exceed the 2010 Title 24 energy efficiency standards by 10%.

While not a requirement, development in Renaissance is encouraged to achieve LEED certification (or equivalent, measurable, and verifiable rating system). Compliance with the overarching performance standard and the requirements of this Specific Plan should help development be in a position to achieve certification.

### Green Infrastructure

The use of green infrastructure is intended to counterbalance the effects of more traditional land development patterns and associated infrastructure that often fragments and degrades natural open spaces and ecosystems with minimal consideration to long-term sustainability. Green infrastructure integrates natural systems, and capitalizes on opportunities for creating multipurpose systems, thereby using land and resources more efficiently.

Implementing green infrastructure and related methods for watershed management improves water quality, conserves water, and reduces runoff volumes as well as peak flows and durations. In addition to these direct benefits to the watershed, implementing such methods also benefits the quality and availability of biological habitat, provides energy conservation by reducing the heat trapping and impervious areas of typical land development, and can be aesthetically pleasing.

- ❖ Suggested—Collect rainwater on site through the use of stormwater management practices such as the incorporation of infiltration basins and bioswales.
- ❖ Suggested—Grade property to divert stormwater flow to permeable areas, following natural drainage contours to the greatest extent possible.
- ❖ Suggested—Where applicable, create curb cuts to allow stormwater flows to drain to permeable or landscaped areas.

## Sustainability

- ❖ Suggested—Where possible, use pervious or open grid paving for driveways, walkways, plazas, and parking areas. Implement small-scale design features throughout the development, such as “Hollywood” or dual-track driveways for single-family homes.
- ❖ Suggested—Use pervious paving materials wherever possible to reduce the negative effects of stormwater runoff and to facilitate groundwater recharge.
- ❖ Suggested—Utilize bioswales, particularly with native or drought-tolerant grasses, to collect and filter water runoff.

## Landscaping

Sustainable landscaping practices and techniques help promote water conservation and reduce water demand as well as help to control water and irrigation costs. Efficient irrigation techniques help reduce water demand while sustainable landscape design can lead to the reduction of the heat-island effect (the absorption of solar heat in paved surfaces), improved environmental habitat, and reduced overall maintenance and replacement cost.

- ❖ Required—Comply with Chapter 12.50, *Water Efficient Landscaping*.
- ❖ Required—Install high efficiency, xeriscape irrigation systems to reduce the amount of water devoted to landscaped areas, such as drip and bubbler irrigation and low-angle, low-flow nozzles on sprayheads.
- ❖ Required—Install and correctly program automated irrigation systems to reduce water use.
- ❖ Suggested—Install properly programmed EvapoTranspiration (ET) based controllers on homeowner’s properties, which are weather based controllers with greater efficiency. In addition, supply homeowners with information on how to properly program their controller using the Metropolitan Water District’s guidelines as a reference tool.
- ❖ Suggested—Install moisture sensors and other similar irrigation technology to ensure that landscaping is watered only as needed.
- ❖ Required—Plant selection should be based on species that are drought tolerant, heat resistant, and hardy. Native plant material should also be closely examined and considered for most landscape areas. Sources of reference for such plant materials can be obtained from the Metropolitan Water District and the California Department of Water Resources. Plant selection should strive to use up to 75 percent water-wise/drought-tolerant, native, or Mediterranean plant materials.

- ❖ Required—Prohibit the use of large turf areas in landscaping by substituting water-conserving native groundcovers or perennial grasses, shrubs, and trees.
- ❖ Suggested—Trails shall be constructed of pervious materials such as decomposed granite or existing earth.
- ❖ Required—Group plants with similar water requirements together, a technique known as hydrozoning. A plant reference is available from the California Department of Water Resources.
- ❖ Suggested—Mulch planting beds and apply compost and environmentally friendly fertilizers to promote healthy topsoil, maximize plant growth, and reduce plant replacement. This will also reduce the need for longer or more frequent irrigation run times.
- ❖ Required—Where available, recycled water will be used in residential front and back yards (and private common areas), as well as in adjacent public street parkways. Where recycled water is not used, turf is limited to 33 percent of the landscaped area of a conventional single family development lot.
- ❖ Required—A demonstration garden will be incorporated into one of the project's parks to educate the community on California-appropriate, drought-tolerant landscaping. It will display a variety of trees, shrubs, and grasses along with irrigation techniques.

## Building-Level Sustainability

There are many sustainable building practices and techniques that offer safe and healthy living environments. Materials and actions that improve indoor air quality and the comfort of homes as well as reduce the impacts of light pollution are critical to community health and well-being.

### *Building Materials*

- ❖ Suggested—Use 20 percent locally manufactured and produced building materials, defined as materials manufactured or produced within a 500-mile radius of the project.
- ❖ Suggested—Strive to utilize rapidly renewable or recycled building materials and products for at least 5 percent of the total value of materials. Flooring alternatives like bamboo, wheatboard, and cork are rapidly renewable materials. Linoleum, exposed concrete, and recycled-content ceramic tiles are also desirable materials.

# Sustainability

- ❖ Suggested—Encourage the installation of insulation with at least 75 percent recycled content, such as cellulose, newspaper, or recycled cotton.

## *Indoor/Outdoor Air Quality*

- ❖ Required—Use only flooring and insulation products that are low emitters of volatile organic compounds (VOC) and formaldehyde.
- ❖ Required—Use only low- and zero-VOC paints, finishes, adhesives, caulks, and other substances to improve indoor air quality and reduce the harmful health effects of off-gassing.
- ❖ Required—All homes should use only gas fireplaces to minimize smoke from wood burning fireplaces and pollutants (e.g. CO, NO, and VOC's).

## *Lighting*

- ❖ Required—Utilize shielded fixtures, avoiding overhead lighting of areas such as walkways.
- ❖ Required—Provide low-contrast lighting, and use low-voltage fixtures and energy-efficient bulbs, such as compact fluorescent (CFL) and light emitting diode (LED) bulbs.
- ❖ Required—Use automated occupancy sensors in non-residential buildings that automatically shut off lights when rooms are unoccupied.
- ❖ Required—Install only energy efficient street lighting.

## *Building Envelope*

- ❖ Required—Install radiant barriers to reduce summer heat gain and winter heat loss. Radiant barriers consist of a highly reflective material, such as aluminum, that prevents radiant heat from being absorbed by the interior of the home.
- ❖ Required—Use natural ventilation techniques, such as operable windows, to take advantage of airflow for cooling residential interiors, reducing the amount of energy needed for cooling.
- ❖ Suggested—Consider the use of “cool roofs,” which are painted with a highly reflective coating or employ light-colored materials, or “green roofs,” vegetated areas on roofs that contain plants in engineered soil, to cool building interiors and increase stormwater retention.

- ❖ Required—Install water and energy saving fixtures and appliances, such as showerheads, toilets, washing machines, clothes dryers, refrigerators, and dishwashers certified as EnergyStar compliant.
- ❖ Suggested—Install re-circulating hot water systems to reduce the need to heat water, or tankless water heaters that heat water as needed instead of storing hot water in tanks, thus reducing standby energy use.
- ❖ Required—Utilize a minimum insulation value of R30 in ceilings.
- ❖ Required—Install programmable thermostats in all units.

## Resource Conservation

Actions that increase water and energy efficiency and conserve resources offer tremendous cost savings to both builders and future residents. Through techniques such as building design that maximizes shading and insulation, high performance HVAC systems, and use of natural daylighting, a substantial reduction in energy use can be achieved. The use of high performance appliances and irrigation systems that minimize water and energy use can substantially impact the amount of resources that flow into and out of the community. Providing access to transportation alternatives reduces natural resources used in transportation.

### *Water*

- ❖ Required—Install only low water consumption, EnergyStar-compliant appliances and fixtures.
- ❖ Required—Install only sensor operated faucets in nonresidential buildings.
- ❖ Required—Install dual flush or other toilets using less than 1.6 GPF.
- ❖ Required—Install waterless urinals in nonresidential buildings.
- ❖ Required—Install faucets and showerheads using 2.5 GPM or less.
- ❖ Required—Use water-saving landscaping techniques, such as drip irrigation systems and drought tolerant plant species. (For a more detailed list of water-saving techniques and practices see the Landscaping section of this Section.)

### *Energy*

- ❖ Required—Install only energy-efficient windows, such as models with spectrally selective low-e glass and with wood, vinyl, or fiberglass frames.

# Sustainability

- ❖ Required—Incorporate building materials that take advantage of heat storage or thermal mass to reduce energy needed for heating and cooling interiors. Materials such as concrete, masonry, and wallboard store heat absorbed during the day and slowly release it throughout the evening, thereby moderating indoor temperatures over a 24-hour period.
- ❖ Suggested—Encourage participation in energy efficiency rebate programs offered by utility providers and government agencies.

## *Heating, Ventilation and Air Conditioning (HVAC)*

- ❖ Required—Design and install HVAC systems according to the standards provided by the Air Conditioning Contractors of America (ACCA) handbooks or other comparable high-performance HVAC standards.
- ❖ Required—Install sealed-combustion/sealed-duct furnaces and water heaters for increased efficiency and indoor air quality.
- ❖ Required—Install only EnergyStar-qualified ceiling fans to circulate air, improve comfort, and reduce the demand on heating and cooling systems.

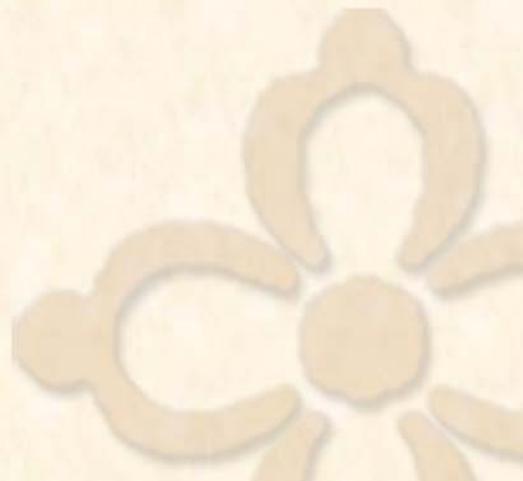
## *Transportation*

- ❖ Required—Preferred parking for high-occupancy vehicles/carpool/vanpool will be provided at nonresidential uses.
- ❖ Required—Bicycle parking shall be provided at nonresidential uses.
- ❖ Required—Transit facilities shall be provided throughout the project site including bus turnouts, benches, and shelters if required by Omnitrans.

# Implementation

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## SECTION SEVEN



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## SECTION 7: IMPLEMENTATION

Whether regulatory or policy oriented, all specific plans must contain a “program of implementation measures, including regulations, programs, public works projects, and financing measures” pursuant to California Government Code, Section 65451(a)(4).

Other documents have been prepared and processed concurrently with the adoption of the Renaissance Specific Plan, including a General Plan amendment and a certified environmental impact report (EIR). These documents will form the basic framework to guide future development within the Specific Plan area.

### Administration

#### *Responsibility*

The City’s Director of Development Services shall be responsible for administering the provisions of the Renaissance Specific Plan in accordance with the provisions of this Specific Plan, the State of California Government Code, and the Subdivision Map Act.

#### *Applicability*

All development within the Specific Plan area shall comply with the requirements and standards set forth in this document. Where conflicts exist between the following standards and those found in the City of Rialto Municipal Code, the standards contained in this document shall apply. Any area of site development, administration, review procedures, environmental review, landscaping requirements, and parking regulations not expressly addressed by this Specific Plan document shall be subject to the provisions of the Municipal Code using the Vision and Guiding Objectives of the Renaissance Specific Plan as a guide.

#### *Interpretations*

When there is a question or ambiguity regarding the interpretation of any provision of this Specific Plan, the provisions of the Municipal Code shall govern. If the Municipal Code does not provide clear direction, then the Director of Development Services has the authority to interpret the intent of such provision; however, the Vision and Guiding Objectives of the Renaissance Specific Plan shall be used in making interpretations.

# Implementation

The Director of Development Services may, at his/her discretion, refer interpretations to the Planning Commission for its consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. The Planning Commission action may be appealed to the City Council.

All interpretations made by the Director of Development Services may be appealed to the Planning Commission.

It is the expressed intent of the Renaissance Specific Plan to encourage the achievement of LEED certification (or equivalent, measurable, and verifiable rating system). However, in some cases a provision and/or its interpretation may hinder achievement of this goal. For instance, the proposal to include solar panels may be perceived to conflict with aesthetic considerations.

In the instance where City or Specific Plan provisions and/or interpretations of provisions prevent achievement of LEED certification, the Director of Development Services and developer shall enter into negotiations to resolve the issue. The interpretation of the Director of Development Services may be appealed to the Planning Commission.

## *Specific Plan Amendment*

### *Minor Amendments*

Minor amendments include simple modifications to text or graphics that do not change the meaning or intent of the Specific Plan. Minor modifications may be accomplished administratively by the Development Review Committee and Economic Development Committee and may be appealed to the Planning Commission.

### *Major Amendments*

Major modifications are amendments to exhibits or text that change the intent and/or development standards or other provisions of the Specific Plan. Major modifications require an amendment to the Specific Plan and approval by the Planning Commission and City Council.

### *Ministerial Changes*

It is the expressed direction of this Specific Plan that the following actions will be considered ministerial (defined as a mandatory act admitting of no personal discretion or judgment in its performance) and shall be approved administratively by the Development Review Committee and Economic Development Committee. Decisions of the Development Review Committee and Economic Development Committee may be appealed to the Planning Commission.

- ❖ Density transfers per the provisions of this section.
- ❖ Modifications to the location, scale, style, and number of gated entries, pedestrian access-ways, and locations of Community Arrivals, Primary and Secondary Community Markers, Neighborhood Markers, and Park and Business Markers.
- ❖ Alternations to accommodate the relocation or elimination of the school, the resulting conversion to the Medium Density Residential (MDR) land use category, and transfer of units per the provisions of the density transfer provision of this section.
- ❖ Conversion of Planning Area 55 from the High Density Residential to the General Commercial land use category and resulting transfer of residential units per the provisions of the density transfer provision of this section.
- ❖ Conversion of individual parcels within Planning Area 5 from the Freeway Incubator to the Employment land use category. If requested by the property owner, the applicant shall be allowed to utilize the uses and standards of the Employment land use category.

### ***Severability***

If any section, subsection, sentence, clause, or phrase of this Specific Plan, or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this plan.

### ***Boundaries of Land Use Categories***

The boundaries of the land use categories are approximate and generally follow streets and property lines. Minor changes in boundary alignment and location are permissible with approval by the Development Review Committee. However, the intended character and overall location of the land use categories must be maintained.

### ***EDC Review Procedure***

In order to expedite investment and development, it is the City's expressed intent to streamline the review of development proposals within Renaissance. Accordingly, any proposed building on Planning Areas 60b or 60c that is larger than 500,000 square feet shall be subject to the following review process:

- ❖ Prior to submittal of the formal application, a preliminary site plan, elevation, and use description shall be reviewed by the Economic Development Committee (EDC) for consistency with the goals contained in this Specific Plan. The recommendation of the EDC shall be forwarded

## Implementation

to the City Council. The City Council may recommend that the application proceed as presented, or the City Council may: 1) provide comments and direction to help the application proceed per the processing requirements of this Specific Plan, or 2) require that a Conditional Development Permit application be processed per Chapter 18.66 of the Municipal Code.

### *Transfer of Units*

The ability to transfer residential units provides flexibility to respond to site constraints and market demands yet ensures that the overall goals of the Renaissance Specific Plan are maintained. The transfer of residential units is allowable within the Specific Plan subject to the following criteria:

- ❖ Any unused residential units from an entitled/developed Planning Area may be transferred to another residentially designated Planning Area.
- ❖ The transfer of units shall not result in a uniform residential density, instead a blending of densities and products shall be maintained. Accordingly, there shall be at least one area designated as LDR, MDR, and MHDR on each side of the Village that is separated by Linden Avenue.
- ❖ Residential unit transfers are permitted to/from any residential planning area; however, the maximum number of residential units in the Specific Plan shall not exceed 1,667 units.
- ❖ Residential units may not be transferred into any nonresidential land use category (e.g., Public Park, Freeway Incubator, or Town Center), except for the School category.
- ❖ If the amount of units transferred results in an increase or decrease in the planning area's density to that of another land use category, then the land use category and governing standards in that planning area shall be changed to match a corresponding land use category. As an example, if a density transfer increases density of the "receiving planning area" from LDR to MDR, then the MDR category and standards shall apply and govern. Conversely, if the transfer decreases the density in the "sending planning area" from MHDR to MDR, then the MDR category and standards shall apply.
- ❖ The maximum residential density of 30 dwelling units per acre may not be exceeded.
- ❖ Density transfers shall be considered ministerial actions approved by the Development Review Committee.

- ❖ If Planning Area 55 is developed as General Commercial per the Commercial Overlay, the residential units may be transferred to other residentially designated areas within Renaissance as long as the other provisions of this section are complied with.
- ❖ In the event that the school relocates or the school district does not choose to develop a school within Renaissance, then the underlying land use will revert to the Medium Density Residential (MDR) land use category and can accept transferred residential units from other areas of the Specific Plan.
- ❖ A development monitoring system, which can be used by the City to track residential development, is available in Appendix C.

## Phasing, Capital Improvements, and Maintenance

### *Infrastructure Improvements*

The developer shall be responsible for constructing or participating in their fair-share of the required infrastructure improvements, such as streets, water lines, sewers, and storm drains necessary to service Renaissance.

All necessary infrastructure improvements will be developed in conjunction with the roadway improvements.

### *Development Phasing*

Given the variety of ownerships within Renaissance, phasing for the entire 1,439 acres cannot be determined. Phasing can be estimated for the areas shown on Figure 7-1, *Phasing Plan*. Those areas outside of the phasing boundaries shown on Figure 7-1 will be developed over time as individual property owners propose projects.

The roadways and infrastructure necessary to serve and provide access will be developed to provide for the development of each phase.

It should be emphasized that the phasing program described in this section is a projection based on a judgment of future planning and market factors. Therefore, this phasing program is not to be taken as a compulsory development sequence. Development area sequencing may change as the result of future conditions that neither the City nor the developer has knowledge of as of the date of this submittal.

However, the basic standards will not change and are required to be complied with regardless of shifts in the composition of each development phase. The developers of property within the Renaissance Specific Plan will be required to comply with all grading, drainage, and road improvements as identified in the Specific Plan.

# Implementation

## Maintenance

The creation and operation of a maintenance assessment district will be an important factor in maintaining the aesthetic quality of Renaissance. Maintenance responsibilities may be divided between a Master Homeowners Association, Neighborhood Associations, Community Facilities Districts (CFD), Landscape Maintenance District(s) and/or other maintenance mechanisms. The public and private entities are described below and in Table 7-1, *Maintenance Plan*.

**Table 7-1  
Maintenance Plan**

<i>Type</i>	<i>Developed by</i>	<i>Owned by</i>	<i>Maintained by</i>
<b>Streetscape</b>			
Arterials, secondary arterials, public collectors, and local streets (curb-to-curb)	Master Developer	City of Rialto	City of Rialto
Private Streets	Guest Builder	HOA	HOA
Medians (arterials, secondary arterials, local streets, and public collectors)	Master Developer	City of Rialto	LLD
Medians (private streets)	Master Developer	HOA	HOA
Private drives	Guest Builder	HOA	HOA
Gated Entries	Master Developer	HOA	HOA
Community Arrivals, Primary and Secondary Community Markers, Neighborhood Markers, and Park and Business Markers	Guest Builder	HOA	HOA
Street Lighting (community and neighborhood roadways)	Master Developer	City of Rialto	LLD
Community Walls and Fences	Master Developer	HOA/Homeowner	HOA/LLD
Interior Neighborhood Walls and Fences	Guest Builder	Homeowners	Homeowners
<b>Parks</b>			
Public Parks (Planning area 40, 16.0 acres)	City of Rialto	City of Rialto	City of Rialto
Public Park (Planning area 81, 3.3 acres)	City of Rialto	City of Rialto	City of Rialto
Private Parks (developed per City specifications)	Master Developer	HOA	HOA
<b>Paseos, Buffers, and Easements</b>			
Private Paseos (Planning area 39)	Master Developer	HOA	HOA
Buffers/easements (Planning Areas 61a, 61b, 82a, 82b) <sup>1</sup>	Master Developer	HOA	HOA
<b>Infrastructure</b>			
Water systems (on and off-site)	Master Developer or water purveyors	City of Rialto or water purveyors	City of Rialto or water purveyors
Sewer systems (on and off-site)	Master Developer	City of Rialto	City of Rialto
Drainage systems (on and off-site)	Master Developer or SBCFCD	City of Rialto or SBCFCD	City of Rialto or SBCFCD

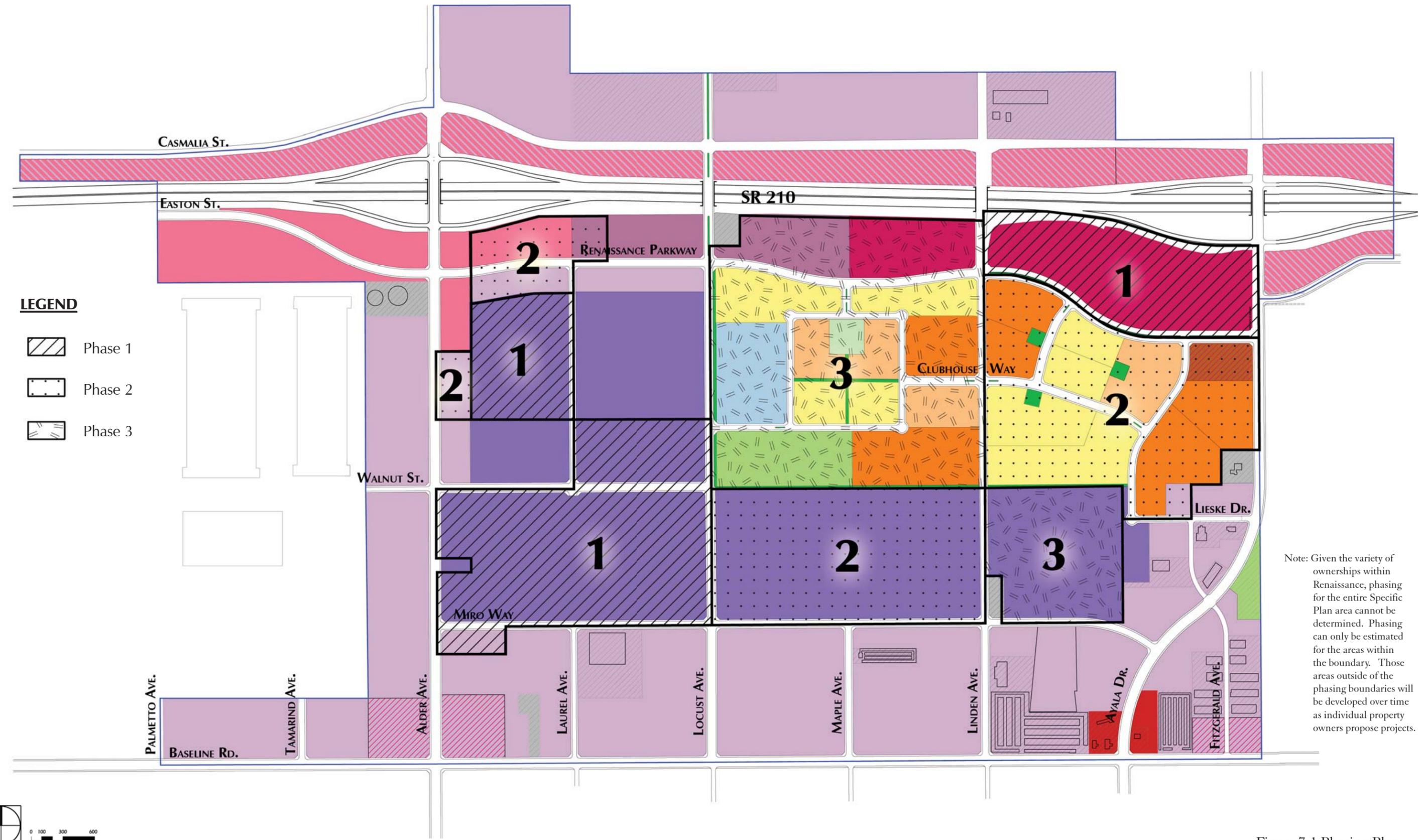
Notes:

LLD = Landscape and Lighting District or special maintenance district

HOA = Homeowners Association (Master or Neighborhood)

SBCFCD = San Bernardino County Flood Control District

<sup>1</sup> These Planning Areas are currently assumed to be private, but may be change to public. If these are public, the responsibility for development, ownership, and maintenance noted herein will be adjusted accordingly.



Note: Given the variety of ownerships within Renaissance, phasing for the entire Specific Plan area cannot be determined. Phasing can only be estimated for the areas within the boundary. Those areas outside of the phasing boundaries will be developed over time as individual property owners propose projects.

Figure 7-1 Phasing Plan

## Implementation

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### *Master Homeowners Associations*

Common areas identified within the Specific Plan shall be owned and maintained by a permanent private maintenance organization. Common areas owned and maintained by the Master Homeowners Association may include common recreation areas, open space, circulation systems, landscaped easements, landscaped areas at entryways and roadways, paseos, and amenities.

### *Neighborhood Homeowners Associations*

In certain residential areas of the project, smaller “neighborhood” Homeowners Associations may be created to provide maintenance for common areas and facilities that only benefit residents who reside in the immediate area.

### *Open Space and Parks*

Open space and parks not directly associated with a particular neighborhood, such as the 9-acre public park at Renaissance Parkway and Locust Avenue and the 3.3-acre public park at the intersection of Fitzgerald Avenue and Ayala Drive, shall be the responsibility of a citywide landscape district or a public facilities maintenance district.

### *Project Roadways*

All public roadways shall be incorporated into the City’s system of roads for operation and maintenance. All private roads shall be owned and maintained by either the Master Homeowners Association or a Neighborhood Association.

### *Financing Strategies*

The financing of construction, operation, and maintenance of public improvements and facilities will include funding through a combination of financing mechanisms. However, the developer shall ultimately be responsible for all fair-share costs and/or conditions of approval (including those in excess of fair share costs but with fee credits or reimbursement agreements) associated with implementing the project, including but not limited to the costs of providing infrastructure and complying with all mitigation measures, conditions of approval, and other requirements of the project.

Various financing strategies may be utilized to fund the public facility improvements specified by the Specific Plan. Financing may involve a combination of impact fees and exactions, fee credits, reimbursement agreements, special assessment districts, landscaping and lighting districts, community facilities districts, and other mechanisms as agreed to by the developer and City.

The City and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan. A Development Agreement and conditions of approval may be used to facilitate this process. The appropriate mechanism for each particular improvement shall be tied to the phasing, established Conditions of Approval and site plan/design review approval.

# Implementation

The following is a summary of possible methods for financing improvements identified in this Specific Plan.

## *Impact Fees and Exactions*

Dedications of land and impact fees are exactions that lessen the impacts of new development resulting from increased demand on services. Local governments derive their authority to impose exactions from the "police power" granted to them by the State Constitution and/or specific state enabling statutes such as the Subdivision Map Act.

A legally defensible exaction must (a) advance a legitimate state interest such as protection of the public health, safety, and welfare, (b) mitigate the adverse impacts to that interest that would otherwise result from the project, and (c) be in "rough proportionality" to the impacts. In addition, the City must identify the purpose of the fee and the use to which it will be put and specify the nexus between the development project and the improvement being financed (§66001). It must further establish that the amount of funds being collected will not exceed that needed to pay for the improvement (§66005).

## *Special Assessment Districts (1911, 1913, 1915 ACT)*

A Special Assessment District is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The City may form a Special Assessment District under one of several different statutory acts to construct public improvements such as streets, storm drains, sidewalks, streetlights, sewers, parks, landscape, and other similar capital facilities. The Special Assessment District can then issue bonds to finance those improvements and to levy a special assessment to pay debt service on those bonds.

A Special Assessment District may fund improvements within the entire Specific Plan area or within smaller Planning areas in the Specific Plan where special improvements directly benefiting only certain property owners are constructed. Special Assessment Districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.

## *Area of Benefit Fees*

Area of benefit fees may be enacted by the City through adoption of an ordinance without voter approval. The fee must be directly related to the benefit received. It does not create a lien against property, but must be paid in full as a condition of approval. Its principle use is for encumbering properties that do not voluntarily enter into an assessment of a Community Facilities District (CFD), so that they pay their fair share at the time they are ready to be developed. Proceeds may be used to reimburse property owners who pay up-front cost for facilities benefiting other

properties. Benefiting properties may be given the option to finance the fees by entering into an assessment district (1913/1911 Act or Mello-Roos CFD).

### ***Mello-Roos Community Facilities Districts (CFD)***

A Mello-Roos Community Facilities District (CFD) may be created to fund infrastructure and other public improvements of a much broader type than those that can be funded by traditional Special Assessment Districts. Not only improvements that have direct benefit, but improvements that have general benefit such as schools, police stations, fire stations, and libraries can be constructed with proceeds from CFD bonds as well as streets, water lines, and other traditional types of public improvements.

Additionally, CFDs can be formed for the purpose of public facility maintenance. When a CFD is formed, bonds may be issued that are sold to private investors who purchase them for tax-free interest income. A CFD can be formed and either the property owners or registered voters (depending upon the type of CFD created) authorize bonds to be sold to generate capital for the initial infrastructure improvement costs. The money raised through the bond sale becomes the debt obligation of the CFD. That debt service on the bonds is repaid by the levy of a special tax on property within the CFD. Those bonds are sold to private investors who purchase them for tax-free interest income and the money raised through the bonds becomes a debt obligation of the CFD. The usual term for such bonds may run anywhere from 20 to 40 years.

### ***Landscaping and Lighting Districts***

Landscaping and Lighting Districts (LLD) may be used for installation, maintenance, and servicing of landscaping and lighting, through annual assessments on benefiting properties. LLDs also may provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities. They also may be used to fund and maintain parks above normal park standards maintained from general fund revenues.

### ***General Obligation Bonds***

General Obligation bonds may be used to fund such things as schools, libraries, jails, fire protection, and capital improvements. General obligation bonds require approval by 2/3 of the jurisdiction's voters and are used to finance the acquisition and construction of public capital facilities and real estate (see §29900 et seq., 43600 et seq., and Education Code §15100 et seq.). G.O. bonds are repaid through an increase in the ad valorem property tax being levied by the issuing jurisdiction.

### ***Public Enterprise Revenue Bonds***

Local governments have the ability to issue bonds to finance facilities for revenue producing public enterprises. The enterprises developed under these funds are financed by user charges that, in turn, are applied to bond debt service payments.

## Implementation

Revenue bonds do not require approval by 2/3 vote since they are neither payable from taxes nor from the general fund. The Revenue Bond Act of 1941 (§54300 et seq.) is the most commonly used bond act. Under this act, bonds may be issued for revenue producing facilities such as airports, harbors, hospitals, parking, and garbage collection. Bonds under this act are adopted by resolution of the legislative body and subject to approval by a simple majority of the voters voting on the bond measure.

### *Developer Funding*

In many cases, certain on-site facilities are tied directly to individual projects. In these cases, it is reasonable to expect the developer (or property owner) to pay the entire costs of the facility in order to secure development rights. On-site local streets, utility connections from main trunk lines, and drainage facilities are good examples of facilities that are normally required concurrent with development of an individual parcel funded by the developer.

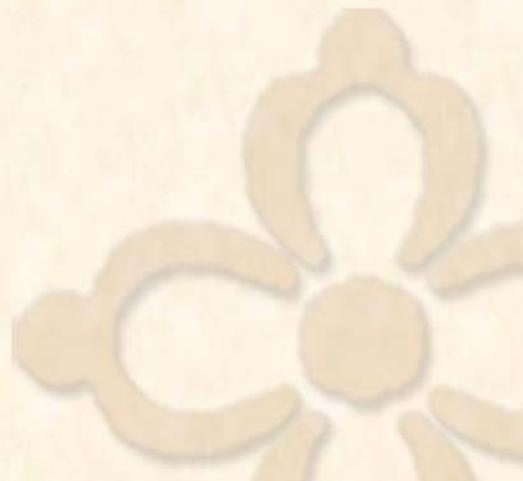
### *Other Funding Sources*

There may be other sources available to finance improvement projects such as special assessment districts, government grants, or various types of bonds not listed above that may be used to fund improvements.

Appendix A  

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GLOSSARY OF TERMS



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## APPENDIX A: GLOSSARY OF TERMS

**Accessory living quarters:** Living quarters within an accessory building for the sole use of persons employed on the premises or for temporary use by guests of the occupants of the premises, having no kitchen or cooking facilities and not rented or otherwise used as a separate dwelling.

**Accessory use:** A use incidental and subordinate to the principle use of a lot or building located upon the same lot as the accessory use.

**Acres, gross:** The entire acreage of a site. Gross acreage is calculated to the centerline of proposed bounding streets and to the edge of the right-of-way of existing or dedicated streets.

**Acres, net:** The portion of a site that can actually be built upon. This includes private parks, private streets, and private open space but does not include public rights-of-way, public open-space, and publicly owned floodways.

**Arterial highway:** See “Street, arterial.”

**Apartment:** A room or suite of rooms in a multiple dwelling, designed for, intended for, suitable as a residence for and/or occupied by one household unit.

**Bikeways:** A term that encompasses bicycle lanes, bicycle paths, and bicycle routes.

**Buffer zone:** An area of land separating two distinct land uses that acts to soften or mitigate the effects of one land use on the other.

**Buildout:** Development of land to its full potential or theoretical capacity as permitted under current or proposed planning or zoning designations.

**Clustered development:** Development in which a number of dwelling units are placed in closer proximity than usual, or are attached, with the purpose of retaining some feature, such as recreational areas or open-space.

**Density, employment:** A measure of the number of employed persons per specific area (for example, employees/acre).

**Density, residential:** A measurement of the number of permanent residential dwelling units per acre of land. Densities specified in the General Plan may be expressed in units per gross acre or per net developable acre. (See “Acres, gross” and “Developable acres, net.”)

## Appendices

***Developable acres, net:*** The portion of a site that can be developed and is assumed for the purpose of density calculations. This area would include the building pad but not public road rights-of-way and flood control channels.

***Developable land:*** Land that is suitable as a location for structures and that can be developed free of hazards to, and without disruption of, or significant impact on, natural resource areas.

***Dwelling unit:*** A room or group of rooms (including sleeping, eating, cooking, and sanitation facilities, but not more than one kitchen), that constitutes an independent housekeeping unit, occupied or intended for occupancy by one household on a long-term basis.

***Family:*** (1) Two or more persons related by birth, marriage, or adoption [U.S. Bureau of the Census]. (2) An individual or a group of persons living together who constitute a bona fide single-family housekeeping unit in a dwelling unit, not including a fraternity, sorority, club, or other group of persons occupying a hotel, lodging house or institution of any kind [California].

***Floor Area Ratio (FAR):*** The gross floor area permitted on a site divided by the total net area of the site, expressed in decimals to one or two places. For example, on a site with 10,000 net square feet of land area, a Floor Area Ratio of 1.0 will allow a maximum of 10,000 gross square feet of building floor area to be built. On the same site, an FAR of 1.5 would allow 15,000 square feet of floor area; an FAR of 2.0 would allow 20,000 square feet; and an FAR of 0.5 would allow only 5,000 square feet.

***Gateways:*** Denotes the locations of prominent structural and/or landscape features that announce the arrival into a particular neighborhood, district, or activity area. Gateways are typically located at the corners of key roadway intersections or in the median of a divided highway.

***Granny flat:*** See “Second unit.”

***Household:*** All those persons (related or unrelated), who occupy a single housing unit. (See “Family.”)

***Housing unit:*** The place of permanent or customary abode of a person or family. A housing unit may be a single-family dwelling, a multifamily dwelling, a condominium, a modular home, a mobile home, a cooperative, or any other residential unit considered real property under State law. A housing unit has, at least, cooking facilities, a bathroom, and a place to sleep. It also is a dwelling that cannot be moved without substantial damage or unreasonable cost. (See “Dwelling unit” “Family” and “Household.”)

**Intensity, building:** For residential uses, the actual number or the allowable range of dwelling units per net or gross acre. For nonresidential uses, the actual or the maximum permitted floor area ratios (FARs).

**Jobs/housing balance; jobs/housing ratio:** The availability of housing for employees. The jobs/housing ratio divides the number of jobs in an area by the number of employed residents. A ratio of 1.0 indicates a balance. A ratio greater than 1.0 indicates a net in-commute and a ratio of less than 1.0 indicates a net out-commute.

**Landmark:** (1) A building, site, object, structure, or significant tree, having historical, architectural, social, or cultural significance and marked for preservation by the local, state, or federal government. (2) A visually prominent or outstanding structure or natural feature that functions as a point of orientation or identification.

**Land use classification:** A system for classifying and designating the appropriate use of properties. The land use classifications for the Southeast Urban Center are defined in the Specific Plan itself.

**Live-work quarters:** Buildings or spaces within buildings that are used jointly for commercial and residential purposes where the residential use of the space is secondary or accessory to the primary use as a place of work.

**Median strip:** The dividing area, either paved or landscaped, between opposing lanes of traffic on a roadway.

**Mixed use:** Properties on which various uses, such as office, commercial, institutional, and residential, are combined in a single building, single site, or group of contiguous properties in an integrated fashion and with a coherent physical design.

**Neighborhood:** A grouping of residential, commercial, service, and recreational uses that are related by their orientation, design, or access points.

**Nonconforming structure:** A building lawfully existing at the time of the adoption of this title which does not conform to the regulations for the district in which it is located as set forth in this title.

**Nonconforming use:** A lawful use of a building or land, or any part thereof, existing at the time of the adoption of this title which does not conform to the regulations for the district in which it is located as set forth in this title.

**Open-space land:** Any parcel or area of land or water that is essentially unimproved and devoted to an open-space use for the purposes of (1) the preservation of natural resources, (2) the managed production of resources, (3) outdoor recreation, or (4) public health and safety.

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***Open space, private:*** Those areas within the development that are designed and intended to be used exclusively by the individual homeowner. Private open space shall include patios, balconies, fenced private yards and other private areas. Private open space may include ground floor patios or courtyards, second- or third-floor balconies or decks, and rooftop decks. Private open space may be covered, but must be open on at least one side.

***Open space, common:*** Those areas designated for the use and enjoyment by all residents and developed for recreational or leisure time activities. These common areas may include game courts, trails, sidewalks, swimming pools, garden grounds, landscaped areas, sauna baths, tennis courts, basketball courts, volleyball courts, putting greens, play lots, and clubhouse facilities.

***Overlay:*** A land use designation on the General Plan Land Use Map, or a zoning designation on a zoning map, that modifies the basic underlying designation in some manner.

***Parcel:*** A lot in single ownership or under single control, usually considered a unit for purposes of development.

***Parkland:*** Land that is publicly owned or controlled for the purpose of providing parks, recreation, or open-space for public use.

***Parking, shared:*** A public or private parking area used jointly by two or more uses.

***Parking area, public:*** An open area, excluding a street or other public way, used for the parking of automobiles and available to the public, whether for free or for compensation.

***Parks:*** Open-space lands for the primary purpose of recreation.

***Parkway strip:*** A piece of land located between the rear of a curb and the front of a sidewalk, usually used for planting low ground cover and/or street trees, also known as “planter strip.”

***Paseo:*** A system of pedestrian and bicycle trails that emanates from the center of residential quarter-sections providing connection to nearby schools, activity areas, and signalized intersections

***Recreation, active:*** A type of recreation or activity that requires the use of organized play areas including, but not limited to, softball, baseball, football and soccer fields, tennis and basketball courts and various forms of children’s play equipment.

***Recreation, passive:*** Type of recreation or activity that does not require the use of organized play areas and includes multipurpose trails and picnic areas.

**Right-of-way:** A strip of land occupied or intended to be occupied by certain transportation and public use facilities, such as roads, railroads, and utility lines.

**Second unit:** A self-contained living unit, either attached to or detached from, and in addition to, the primary residential unit on a single lot. A “granny flat” is one type of second unit intended for the elderly.

**Sign:** See definitions in Section 5, *Signage Guidelines*.

**Street, private drive:** A public way at the rear or side of property, not exceeding thirty feet in width for the use of pedestrians and/or vehicles, which affords only a secondary means of access to the abutting property.

**Street, arterial:** A roadway that supports medium to higher speeds (30–55 mph), medium to higher capacity (10,000–50,000 average daily trips) and provides intra- and inter-community travel and access to the regional highway and freeway system. Access to community arterials should be provided at collector roads and local streets, discouraging direct access from parcels to existing arterials.

**Street, collector:** A relatively low speed (25–30 mph), relatively low volume (5,000–20,000 average daily trips) street that provides circulation within and between neighborhoods. Collectors usually serve short trips and are intended for collecting trips from local streets and distributing them to the arterial network.

**Street, local:** A low-speed (15–25 mph), low-volume (less than 5,000 average daily trips) street that provides circulation within neighborhoods. Local streets provide direct access to fronting properties and are not intended for through-traffic. Local streets are typically not shown on the Circulation Plan, Map, or Diagram.

**Streets, major:** The transportation network that includes a hierarchy of freeways, arterials, and collectors to service through traffic.

**Street, private/private road:** Privately owned (and usually privately maintained) motor vehicle access that is not dedicated as a public street. Typically the owner posts a sign indicating that the street is private property and limits traffic in some fashion. For density calculation purposes, private roads are excluded when establishing the total acreage of the site.

**Streets, through:** Streets that extend continuously between other major streets in the community.

**Structure:** Anything constructed or erected that requires location on the ground (excluding swimming pools, fences, and walls used as fences).

**Subdivision:** The division of a tract of land into defined lots, either improved or unimproved, which can be separately conveyed by sale or lease, and which can be

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altered or developed. “Subdivision” includes a condominium project as defined in Section 1350 of the California Civil Code and a community apartment project as defined in Section 11004 of the Business and Professions Code.

*Urban design:* The attempt to give form, in terms of both beauty and function, to selected urban areas or to whole cities. Urban design is concerned with the location, mass, and design of various urban components and combines elements of urban planning, architecture, and landscape architecture.

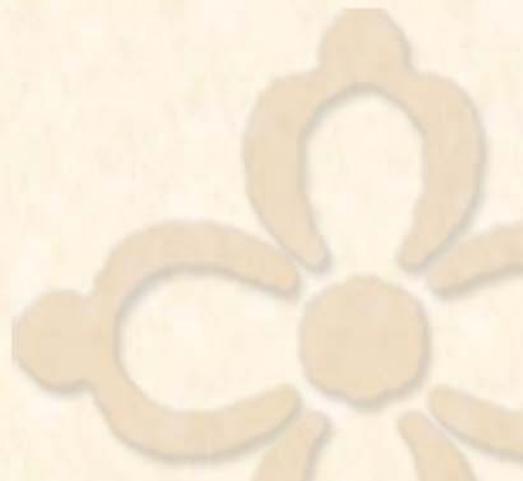
*Zoning:* The division of a city or county by legislative regulations into areas, or zones, that specify allowable uses for real property and size restrictions for buildings within these areas; a program that implements policies of the General Plan.

*Zoning, incentive:* The awarding of altered development standards to a development for public benefits (such as the provision of public plazas and courts at ground level) included in a project.

Appendix B

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GENERAL PLAN  
CONSISTENCY ANALYSIS



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## APPENDIX B: GENERAL PLAN CONSISTENCY ANALYSIS

### Consistency with Adopted General Plan

The Renaissance Specific Plan is consistent with and supportive of the goals and policies of the adopted City of Rialto General Plan to reflect the comprehensive and unified master plan for the project site. A discussion of the relationship of this Specific Plan to the applicable General Plan goals follows.

#### *Land Use Element*

The goals that directly relate to the Specific Plan are presented below.

*Goal 4.1.5: Develop, protect and enhance high quality residential and industrial land uses in Rialto.*

*Goal 4.1.6: Maximize the opportunities and benefits to the City of Rialto associated with State Route 30.*

*Goal 4.1.7: Ensure that all developed areas of the City are adequately served with essential public services and infrastructure, including, but not limited to, streets, water, surface drainage, sanitary sewers, law enforcement, fire protection and public schools.*

*Goal 4.2.2: Meet adopted City standards for the provision of park lands and open space.*

*Goal 4.2.4: All streetscapes in Rialto shall support and enhance the City image as a desirable place in which to live or work.*

#### *Specific Plan Response*

The Specific Plan accommodates a range of land uses that will provide a balance of market driven land uses. A variety of housing opportunities, employment, and recreational opportunities will strengthen the economic base of the City and provide housing opportunities. In all, the plan accommodates 1,667 residential units and 16.2 million square feet of existing and future businesses. The Renaissance Specific Plan contains detailed development standards, design guidelines, and use restrictions to ensure high quality structures and compatible uses.

State Route 210 (SR-210) runs through the northern portion of Renaissance with on-/off- ramps at Ayala and Alder Avenues. Renaissance takes advantage of the adjacent SR-210 and provides employment, retail, and residential opportunities along both sides of the freeway. The uses north of SR-210 include employment-generating uses such as manufacturing, light industrial, distribution, warehousing, as well as wholesalers, furniture stores, and car, RV, and boat sales. The uses along the southern edge of SR-210 include retail centers and office complexes. The Town

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Center, which is located at the southwest intersection of Ayala and SR-210, accommodates an array of commercial uses. The corporate office center is envisioned as an intense office center, reinforcing the business corridor gateway into Rialto, maximizing freeway visibility, and strengthening the surrounding commercial uses. The uses at the intersection of Renaissance Parkway and Alder Avenue are retail and entertainment related.

Renaissance includes the infrastructure necessary to support the planned land uses. The Specific Plan and accompanying EIR contain detailed infrastructure and service analysis that addresses the provision of water, sewer, drainage, circulation, dry utilities, fire protection, police, and schools. The Village includes a 15-acre elementary school in the Rialto Unified School District. As currently envisioned, the school is located in the interior of the community; however, the school site may be relocated within the Village depending upon negotiations with the school district and in response to the California Department of Education requirements. An existing fire station is located on the eastern portion of the site along Ayala Drive.

The Specific Plan includes recreational facilities necessary to support the planned land uses. Maximum buildout of the Renaissance Specific Plan accommodates approximately 1,667 units and a population of approximately 5,167 residents. Based on the City's standard of 3 acres of parkland per 1,000 residents, full buildout of the Specific Plan would result in the need to provide 15.5 acres of parkland or an equivalent fee in-lieu of dedicated parkland. Renaissance exceeds the City's requirement and provides 24.7 total acres of public and private parkland and paseos (does not include a 3.3 acre existing portion of the Jerry Eaves Park that is developed as a parking lot).

Design guidelines and development standards within the Specific Plan address the aesthetic integration of uses within the site and with surrounding uses. The focus is to provide for architectural, landscape, streetscape, and site design enhancements to ensure quality development and a unique setting. Detailed street sections provide guidance for the aesthetic treatment of the roadways within Renaissance.

Please note that given the Council's action to close the Rialto Airport, Goal 4.6.1 and its associated policies regarding the Rialto Airport are no longer valid and must be amended to reflect the provisions and direction of the Renaissance Specific Plan.

### ***Economic Development Element***

The goals that directly relate to the Specific Plan are presented below.

*Goal 1.1: Promote an economic base and positive business climate providing primary commercial services to the resident population.*

*Goal 1.2: Provide a broader base of employment opportunities for Rialto and the west San Bernardino Valley.*

*Goal 1.4: Strengthen and diversify existing principal commercial areas.*

*Goal 1.6: Utilize the Specific Plan process for planning in established areas of Rialto which may undergo land use transformations.*

### ***Specific Plan Response***

Renaissance provides a broad base of employment, retail, and services to Rialto. The Specific Plan accommodates a mixture of retail shopping, restaurants, entertainment, regional shopping, professional office, research and development, medical uses, light industrial, business park, light manufacturing, assembly, distribution, and manufacturing uses. In all, Renaissance accommodates 16.2 million square feet of business and commercial uses (835,200 square feet of which is existing and expected to remain). The Specific Plan accommodates approximately 14,468 jobs (850 existing and expected to remain), which equates to over eight jobs for every housing unit. This employment base improves both the City's and region's jobs-to-housing balance.

SR-210 runs through the northern portion of Renaissance with on-/off-ramps at Ayala and Alder Avenues. Renaissance takes advantage of the adjacent SR-210 and provides employment, retail, and offices uses at or near the on-ramps. The uses north of SR-210 include employment-generating uses such as manufacturing, light industrial, distribution, warehousing, as well as wholesalers, furniture stores, and car, RV, and boat sales. The uses along the southern edge of SR-210 include retail centers and office complexes. The Town Center, which is located at the southwest intersection of Ayala and SR-210, accommodates an array of commercial uses. The corporate office center is envisioned as an intense office center, reinforcing the business corridor gateway into Rialto, maximizing freeway visibility, and strengthening the surrounding commercial uses. The uses at the intersection of Renaissance Parkway and Alder Avenue are retail and entertainment related.

The Rialto City Council adopted resolutions declaring the intent to relocate the Rialto Airport operations to other nearby facilities. With the closure of the Rialto Airport, the Renaissance Specific Plan serves as the long-range plan for the development of the 1,439 acres that was previously the Airport and the area surrounding SR-210. The Renaissance Specific Plan provides detailed text and exhibits describing the various land uses, amenities, and infrastructure improvements envisioned for the community. The Specific Plan also provides clear provisions for development and use as well as incentives and flexibility to achieve both the City's needs and those of the business community.

### ***Circulation Element***

The following goals relate to the Specific Plan.

*Goal 3.2.2: Confine trucking to designated, efficient, and convenient routes within and through the City.*

*Goal 3.2.3: Maintain Level of Service D or better on all Rialto arterial roadways.*

*Goal 3.2.4: Residential neighborhoods in Rialto shall be protected from the noise, pollution and danger of excessive vehicular traffic.*

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*Goal 4.1.1: Safe, convenient and adequate parking throughout the City.*

*Goal 7.1.2: Safe pedestrian access throughout Rialto.*

### ***Specific Plan Response***

The truck routes in and around Renaissance are intended to direct on-site truck traffic westward to Alder Avenue and away from residential areas. Truck traffic is accommodated along Baseline Road, Alder Avenue, Ayala Drive, Laurel Avenue, Walnut Street, and Miro Way. All roadways south of Miro Way and north of Renaissance Parkway accommodate truck traffic. On-site truck traffic will be directed to Alder Avenue through the use of directional signage at business driveways. Truck trips are restricted to local deliveries (no through trips) on those portions of: 1) Linden Avenue and Renaissance Parkway that are adjacent to residentially designated areas, and 2) Casmalia Street, Renaissance Parkway, and Baseline Road west of Alder Avenue.

The circulation plan for Renaissance is designed to meet and exceed the City's LOS standards. The EIR, which accompanies the Renaissance Specific Plan, contains the detailed traffic study and analysis in this regard.

The Village is located and designed in such a manner as to minimize through-traffic and exposure to excessive noise. The Renaissance Parkway, Locust Avenue, and Ayala Drive form the perimeter of the project and provide separation from non-residential uses to the north and the west. The 16-acre public park and open space buffer help provide separation from non-residential uses to the south and west. Linden Avenue, which is a Secondary Arterial, is the only through-street to cut through the Village residential area. The street sections and development standards contained in the Specific Plan detail the separations and setbacks from the roadways that are designed to protect residences from excessive vehicular noise.

Detailed parking standards, which address residential and non-residential uses, are contained in the Renaissance Specific Plan. Within the Village, off-street (paseo) and on-street (sidewalks) pedestrian systems are provided to allow convenient and safe non-vehicular circulation. In addition, sidewalks in the non-residential areas of the plan, which are detailed on each of the street sections contained in the Specific Plan, provide a comprehensive system of pedestrian movement.

Given the Council's action to close the Rialto Airport, Goals 8.1.1 and 8.1.2 and their associated policies regarding the Rialto Airport are no longer valid and must be amended to reflect the provisions and direction of the Renaissance Specific Plan.

### ***Housing Element***

The following goals from the Housing Element relate to the Specific Plan.

*Goal 4: To create a housing market environment where all households have adequate housing within their economic means.*

*Goal 5: To encourage the continued construction of new affordable housing units.*

### *Specific Plan Response*

In all, the Specific Plan accommodates 1,667 residential units and a population of approximately 5,167 residents. The Specific Plan accommodates a wide array of housing types and densities to attract different lifestyles and life stages. Residential products found within Renaissance include single-family detached homes, small-lot detached homes, townhomes, apartments, courtyard clusters, and condominiums. These residential products range from 3 to 30 units per acre and are intended to serve a wide range of the residential market—families, single professionals, first-time homebuyers, those looking to move up, empty-nesters looking to downsize, and seniors.

The Low Density Residential land use designation provides for 446 detached, single family units. The Medium Density Residential designation provides for 290 small-lot detached, duplexes, and townhomes. The Medium High Density Residential designation provides for 818 attached units, duplexes, and townhomes. Finally, the High Density Residential designation provides for 113 attached units.

While units in Renaissance are not specifically targeted to the very low, low, and moderate income ranges, the higher density products can be expected to be priced at a level that is more affordable to the workforce.

### *Open Space and Recreation Element*

The following goals of the Open Space and Recreation Element relate to the Specific Plan.

*Goal 3.1: Optimal use of the Cactus Basin area.*

*Goal 7.1: Meet adopted City standards for the provision of park lands and open space.*

### *Specific Plan Response*

Renaissance includes a wealth of open spaces that serve multiple functions, including recreational opportunities, community separators, buffers, visual landmarks, theme setters, and as an interconnecting system of trails. The parks are easily accessible to residents within Renaissance. Parks are located to ensure that all homes are within one-quarter mile of a park.

The portion of Cactus Basin within the Renaissance Specific Plan area is preserved as a public park.

Maximum buildout of the Renaissance Specific Plan would accommodate 1,667 units and a population of approximately 5,167 residents. Based on the City's standard of 3 acres of parkland per 1,000 residents, full buildout of the Specific Plan would result in the need to provide 15.5 acres of parkland or an equivalent fee in lieu of dedicated parkland. Renaissance exceeds the City's requirement and provides 24.7 total acres of public and private parkland and paseos (does not include the 3.3 acre existing portion of the Jerry Eaves Park, which is developed as a parking lot).

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Given the Council's action to close the Rialto Airport, Goal 6.2 and its associated policies regarding the Rialto Airport are no longer valid and must be amended to reflect the provisions and direction of the Renaissance Specific Plan.

### *Community Design*

The following goals relate to the Specific Plan.

*Goal 1.3: Improve the quality of planned development in the City of Rialto.*

*Goal 1.4: Improve the architectural quality of development within Rialto to achieve harmony without monotony in the built environment.*

*Goal 2.1: Promote well planned design of residential land uses within the City.*

*Goal 3.1: Promote commercial and/or industrial development which is well designed, people oriented, sensitive to the needs of the visitor or resident, and functionally efficient for its purpose.*

*Goal 3.3: Minimize the visual impact of vehicles on the landscape and community design of parking lots.*

*Goal 3.4: Special design consideration shall be given to such uses as service stations, car washes, convenience markets, and fast food drive through businesses.*

*Goal 5.1: Promote the "greening" of Rialto.*

*Goal 6.2: Encourage the aesthetic design and treatment of all signage within the City of Rialto.*

*Goal 6.3: New streets shall be developed to assist rather than alienate pedestrians.*

*Goal 6.5: Encourage the undergrounding of utility wires to protect scenery, enhance the appearance of major boulevards, and promote neighborhood character.*

### *Specific Plan Response*

The Renaissance Specific Plan includes detailed development standards, design guidelines, and signage and landscape provisions that address public and private residential and non-residential developments. These provisions ensure a high quality and cohesive aesthetic quality in Renaissance. The Specific Plan includes provisions that require variation of product types, garage placement, materials and color palettes, and articulation and screening of walls and facades to avoid a monotonous environment. The Specific Plan also includes landscape and setback provisions that screen parking lots and provisions that address drive-through facilities.

The mobility plan contained in the Specific Plan includes street sections that detail the street-adjacent pedestrian systems and minimum sidewalk widths. The street sections depict the appropriate landscape treatment for each street section.

The Renaissance Specific Plan requires that utility lines shall be placed underground per the requirements of Chapter 15.32 Underground Utilities of the Municipal Code and the utility companies.

### ***Cultural and Historic Resources***

The following goals relate to the Specific Plan.

*Goal 2.1 All significant archaeological resources in Rialto shall be surveyed, recorded and, where feasible, protected.*

### ***Specific Plan Response***

Renaissance is a partially developed area with an airport, industrial uses, residences, roadways, utilities, and some open lands. There are no known historical or archaeological resources within the Renaissance Specific Plan area. As detailed in the accompanying EIR, any historical or archaeological resources discovered during demolition and earth work shall be appropriately surveyed and recorded.

### ***Conservation***

The following goals relate to the Specific Plan.

*Goal 2.1: Protect and enhance Rialto's surface waters and groundwater basins.*

*Goal 2.2: Conserve scarce water resources.*

*Goal 3.1: Conserve Rialto top soils by eliminating erosion from wind and water.*

*Goal 5.2: Improve the balance between jobs and housing in order to create more efficient urban form and/or reduce the Vehicle Miles Traveled (VMT).*

*Goal 5.8: Minimize the practicable particulate emissions from the construction and operation of roads and buildings.*

*Goal 5.9: Reduce emissions through reduced energy consumption.*

*Goal 7.1: Conserve scarce energy resources.*

### ***Specific Plan Response***

Renaissance includes a drainage plan and water plan that will achieve the City's Storm Water Pollution Prevention Program (SWPPP) and conform to the Water and Drainage Master Plans. All drainage facilities will be designed and constructed in accordance with the City and county flood control district standards and specifications. In addition, a Water Quality Management Plan (WQMP) and an

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SWPPP are required in accordance with the requirements of the National Pollutant Discharge Elimination System (NPDES) standards.

All construction and grading within Renaissance will be required to comply with the uniform Building Code, grading permits, SCAQMD requirements, and appropriate NPDES construction permit.

No significant biologic or cultural resources were discovered on the site. A water supply assessment was prepared for Renaissance and found that there is adequate water supply to accommodate full buildout of the project.

The Renaissance Specific Plan includes provisions and landscape guidelines designed to minimize water and energy consumption. The Specific Plan includes incentives designed to encourage the development of LEED certified (or equivalent, measurable, and verifiable rating system) projects. The landscape palette is designed with native and drought tolerant plants. Energy efficient lighting is also required.

The Specific Plan accommodates approximately 14,468 jobs (850 existing and expected to remain) which equates to more than eight jobs for every housing unit. This employment base improves both the City's and region's jobs-to-housing balance.

Given the Council's action to close the Rialto Airport, Goal 5.7 and its associated policies regarding the Rialto Airport are no longer valid and must be amended to reflect the provisions and direction of the Renaissance Specific Plan.

### ***Noise Element***

The following goals of the Noise Element relate to the Specific Plan.

*Objective 1.0: Incorporate noise considerations into land use planning decisions.*

### ***Specific Plan Response***

Development within Renaissance will comply with the City's interior and exterior noise standards. In conjunction with plan review, the City will review noise attenuation prior to issuance of building permits.

The residential properties in Renaissance are setback and protected from noise originating on SR-210. The Village is located and designed in such a manner as to minimize through-traffic and exposure to excessive noise. As shown in Figure 3-14, *Truck Routes*, Renaissance Parkway, Locust Avenue, and Ayala Drive form the perimeter of the Village and provide separation from non-residential uses to the north and the west. As also shown in Figure 3-14, truck trips are restricted to local deliveries (no through trips) on those portions of: 1) Linden Avenue and Renaissance Parkway that are adjacent to residentially designated areas, and 2) Casmalia Street, Renaissance Parkway, and Baseline Road west of Alder Avenue.

As shown in Figure 2-2, *Land Use Diagram*, the 16-acre public park and open space buffers helps provide separation from non-residential uses to the south and west.

Additionally, as shown on Figure 2-2, the Specific Plan also includes the option to develop paseos within the buffers on the edges of the Village (Planning Areas 61a, 61b, 82a, and 82b) to allow a functional use of these open spaces. The street sections and development standards contained in the Specific Plan detail the separations and setbacks from the roadways that are designed to protect residences from excessive vehicular noise.

The Specific Plan also includes provisions to protect sensitive receptors, such as residences, from excessive noise, including setbacks, restrictions on hours of operation, and design considerations.

Given the Council's action to close the Rialto Airport, background information, tables, and graphics and policies regarding the Rialto Airport are no longer valid and must be amended to reflect the provisions and direction of the Renaissance Specific Plan.

### ***Safety Element***

The following goals relate to the Specific Plan.

*Goal 1.1: Minimize hazards to public health, safety, and welfare resulting from natural and man-made hazards.*

*Goal 2.1: Minimize hazards to public health, safety, and welfare resulting from natural and geotechnical hazards.*

*Goal 2.2: Encourage urbanization only in those areas without significant risk to life and property.*

*Goal 3.1: Minimize risk and damage from flood hazards within the City.*

*Goal 3.2: Minimize the adverse effects of urbanization upon drainage and flood control facilities.*

*Goal 4.1: Fire prevention regulations and standards to minimize potential fire hazards and fire losses.*

*Goal 5.4: Ensure that all businesses in the City of Rialto that use hazardous materials and generate hazardous waste properly manage these substances.*

### ***Specific Plan Response***

Renaissance is not located in known natural or man-made hazard areas. The business related land uses allow uses that may utilize hazardous materials.

Renaissance includes a drainage plan that conforms to the City's Drainage Master Plan. All drainage facilities will be designed and constructed in accordance the City and county flood control district standards and specifications. A WQMP and an SWPPP will be required in accordance with the requirements of the NPDES standards for all applicable development.

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All construction in Renaissance will be required to obtain a building permit, which includes review and approval of the Fire Department. The uses of any listed hazardous materials in the City are also subject to review and approval by the Fire Department.

Given the Council's action to close the Rialto Airport, Goal 6.0 and its associated policies regarding the Rialto Airport are no longer valid and must be amended to reflect the provisions and direction of the Renaissance Specific Plan.

## Consistency with draft General Plan Update

The Renaissance Specific Plan is consistent with and supportive of the goals and policies of the draft General Plan Update to reflect the comprehensive and unified master plan for the project site. A discussion of the relationship of this Specific Plan to the applicable goals of the draft General Plan Update follows.

### *Land Use, Community Design, Open Space and Conservation Element*

The goals that directly relate to the Specific Plan are presented below.

#### *Land Use*

*Goal 2-9: Protect residential, schools, parks, and other sensitive land uses from the impacts associated with industrial and trucking-related land uses, as well as commercial and retail areas.*

#### *Specific Plan Response*

The Village, which contains the sensitive land uses (residential, school, and park and open space), is located and designed in such a manner as to minimize through-traffic and impacts from industrial and commercial land uses that would be permitted to the north, west, and south. As shown in Figure 3-14, *Truck Routes*, Renaissance Parkway, Locust Avenue, and Ayala Drive form the perimeter of the Village and provide separation from non-residential uses to the north and the west. As also shown in Figure 3-14, truck trips are restricted to local deliveries (no through trips) on those portions of: 1) Linden Avenue and Renaissance Parkway that are adjacent to residentially designated areas, and 2) Casmalia Street, Renaissance Parkway, and Baseline Road west of Alder Avenue.

As shown in Figure 2-2, *Land Use Diagram*, the 16-acre public park and the open space buffers helps provide separation from non-residential uses to the south and west. Additionally, as shown on Figure 2-2, the Specific Plan also includes the option to develop paseos within the buffers on the edges of the Village (Planning Areas 61a, 61b, 82a, and 82b) to allow a functional use of these open spaces.

The street sections and development standards contained in the Specific Plan detail the provisions for separations and setbacks from the roadways and the architectural design and landscape standards that are designed to help buffer sensitive land uses from non-residential land uses. Other provisions of the Specific Plan that would help protect sensitive land uses include the restrictions on hours of operation for non-residential land uses.

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## *Community Design*

Goal 2-10: Create distinctive gateways at all entry points into Rialto and for individual districts or neighborhoods.

Goal 2-11: Design streetscapes in Rialto to support and enhance the City's image as a desirable place to live, work, shop, and dine.

Goal 2-13: Achieve quality aesthetic design of all signage in the City of Rialto.

Goal 2-16: Improve the architectural and design quality of development in Rialto.

Goal 2-17: Provide high-quality and environmentally sustainable landscaping.

Goal 2-20: Require high-quality multi-unit design, landscaping, and architecture.

Goal 2-21: Ensure high-quality planned developments in Rialto.

Goal 2-22: Promote commercial and/or industrial development that is well designed, people-oriented, environmentally sustainable, sensitive to the needs of the visitor or resident, and functionally efficient for its purpose.

Goal 2-23: Minimize the visual impact of parking lots.

## *Specific Plan Response*

Sections 3, *Development Criteria*, 4, *Design Guidelines*, and 5, *Signs*, of the Specific Plan set forth the detailed development standards and design guidelines for high-quality site planning, architecture and design, landscaping, streetscape elements, signage, and lighting that would ensure the creation of a unique and special destination place for the City of Rialto. The Specific Plan includes residential provisions that require variation of product types, garage placement, materials and color palettes, and articulation and screening of walls and facades to avoid a monotonous environment. The development standards outlined in the Specific Plan also contain the necessary requirements for non-residential building intensity, setbacks, and height needed for the development of high-quality commercial, office, retail and industrial land uses. Additionally, the Specific Plan outlines design standards and guidelines for streetscape elements such as signage, lighting, street furnishings, walls, fences, gates, and paving materials. The standards involve a high level of planning and design and require that the design elements be consistent and complementary throughout the project area. The landscape standards of the Specific Plan also establish landscape treatments in addition to the streetscape elements to provide unique design elements for public rights of way and open spaces. Furthermore, the Specific Plan also includes landscape and setback provisions that screen parking lots and provisions that address drive-through facilities.

Collectively, the provisions outlined in the Specific Plan will foster the creation of a recognizable, landmark development for the City of Rialto that appeals to the City's population, stands out from other developments, and provides a unique environment for people to live, work, shop, walk, and gather.

### ***Open Space and Recreation***

*Goal 2-24: Take advantage of opportunities to increase and enhance open spaces throughout Rialto.*

*Goal 2-26: Maximize open spaces in urban areas.*

*Goal 2-27: Provide a variety of park facilities that meet the diverse needs and interest of the community.*

*Goal 3-11: Provide community facilities that adequately support established programs, can accommodate future needs, and are accessible to all members of the community.*

*Goal 3-15: Promote a social environment in Rialto that enhances the dignity of the individual and improves the quality of life.*

*Goal 3-16: Ensure integration and participation of seniors in mainstream community life through accessible social services.*

### ***Specific Plan Response***

Maximum buildout of the Renaissance Specific Plan accommodates approximately 1,667 units and a population of approximately 5,167 residents. Based on the City's standard of 3 acres of parkland per 1,000 residents, full buildout of the Specific Plan would result in the need to provide 15.5 acres of parkland or an equivalent fee in-lieu of dedicated parkland.

The Renaissance Specific Plan exceeds the City's requirement and provides 24.7 total acres of public and private parkland and paseos (21.4 acres of new parklands that does not include a 3.3 acre existing portion of the Jerry Eaves Park, which is developed as a parking lot). As shown in Figure 3-16, *Parks and Open Space Plan*, open space and recreation uses include a 16-acre park, three private parks, a private clubhouse, and paseos. The open spaces within Renaissance will serve multiple functions: as recreational opportunities, as community separators, as buffers, as visual landmarks, as theme setters, and as an interconnecting system of trails. Aside from the aforementioned functions, the open space and recreation uses will also promote a social environment in Renaissance that enhances the health and well being of individuals and improves the quality of life.

As shown in Figure 3-16, the open space and recreation uses are easily accessible to residents within Renaissance. Parks are located within one-quarter mile of all homes. Additionally, as shown in Figure 3-16, the location of the 16-acre public park makes it conveniently accessible to the proposed school and to the employees of the surrounding commercial and industrial land uses.

### ***Conservation***

*Goal 2-28: Protect and enhance Rialto's surface waters and groundwater basins.*

*Goal 2-29: Conserve water resources.*

## Appendices

*Goal 2-30: Incorporate green building and other sustainable building practices into development projects.*

*Goal 2-31: Conserve energy resources.*

*Goal 2-34: Achieve waste recycling levels that meet or exceed State mandates. Achieve maximum waste recycling in all sectors of the community: residential, commercial, industrial, institutional, and construction.*

*Goal 3-10: Minimize the volume of solid waste that enters local and regional landfills.*

*Goal 2-35: Reduce air pollution emissions from both mobile and stationary sources in the City.*

*Goal 2-36: Reduce the amount of fugitive dust released into the atmosphere.*

*Goal 2-38: Mitigate against climate change.*

*Goal 2-39: Conserve and enhance Rialto's biological resources.*

### ***Specific Plan Response***

The Renaissance Specific Plan includes a drainage and water plan that will achieve the City's SWPPP and conform to the Water and Drainage Master Plans. All drainage facilities will be designed and constructed in accordance with the City and county flood control district standards and specifications. In addition, a WQMP and SWPPP are required for individual development projects within Renaissance in accordance with the requirements of the NPDES standards. Additionally, Sections 4, *Design Guidelines*, and 6, *Sustainability*, outline the provisions for maintaining water quality and efficient and sustainable landscaping that will assist in water conservation. Some of these provisions include:

- ❖ The use of medians and parkways for water treatment and to reduce runoff.
- ❖ Collecting rainwater onsite through the use of stormwater management practices, such as the incorporation of infiltration basins and bioswales.
- ❖ The minimizing of water usage through the planting of native and low-water species and the use of water-efficient and drip irrigation systems.
- ❖ Installation of high efficiency, low-water using irrigation systems to reduce the amount of water devoted to landscaped areas, such as drip and bubbler irrigation and low-angle, low-flow nozzles on sprayheads.
- ❖ Installation of only low water consumption, EnergyStar-compliant appliances and fixtures.

The Renaissance Specific Plan includes provisions and guidelines designed to minimize energy consumption. For example, the Specific Plan includes incentives designed to encourage the development of LEED certified (or equivalent, measurable, and verifiable rating system) projects. Other provisions include:

- ❖ Installation of only energy-efficient windows, such as models with spectrally selective low-e glass and with wood, vinyl, or fiberglass frames.
- ❖ Participation in energy efficiency rebate programs offered by utility providers and government agencies.
- ❖ The installation of insulation with at least 75 percent recycled content, such as cellulose, newspaper, or recycled cotton.
- ❖ Provision of low-contrast lighting, and use low-voltage fixtures and energy-efficient bulbs, such as compact fluorescent (CFL) and light emitting diode (LED) bulbs.

Section 6, *Sustainability*, includes provisions that would assist in minimizing solid-waste impacts and helping the City achieve waste recycling levels that meet or exceed State mandates. For example, provisions include:

- ❖ Use of rapidly renewable or recycled building materials and products for at least 5 percent of the total value of materials.
- ❖ Installation of insulation with at least 75 percent recycled content, such as cellulose, newspaper, or recycled cotton.

Additionally, project applicants would be required to comply with the waste and recycling provisions outlined in the City's Municipal Code.

The proposed project would provide a mix of residential and non-residential land uses within walking distance of each other. By allowing residential uses to be mixed in with retail, office, entertainment, employment, and other support uses and services, the dependency on the motor vehicle would be greatly reduced and would in turn, reduce pollution emissions from mobile sources. Additionally, Section 6, *Sustainability*, outlines the provisions for alternative energy resources and design features/elements that would ensure minimal impacts on climate change and energy resources. Additionally, a detailed air quality analysis was completed for the Specific Plan and is included in the accompanying EIR. Furthermore, all construction and grading within Renaissance will be required to comply with the uniform Building Code, grading permits, SCAQMD requirements, and appropriate NPDES construction permit.

No significant biologic or cultural resources were discovered on the site.

### ***Economic Development, Redevelopment, and Infrastructure Element***

## Appendices

The goals that directly relate to the Specific Plan are presented below.

### *Economic Development*

*Goal 3-1: Strengthen and diversify the economic base and employment opportunities, and maintain a positive business climate.*

### *Specific Plan Response*

Renaissance provides a broad base of employment, retail, and services to Rialto. The Specific Plan accommodates a mixture of retail shopping, restaurants, entertainment, regional shopping, professional office, research and development, medical uses, light industrial, business park, light manufacturing, assembly, distribution, and manufacturing uses. In all, Renaissance accommodates 16.2 million square feet of business and commercial uses (835,200 square feet of which is existing and expected to remain). The Specific Plan accommodates approximately 14,468 jobs (850 existing and expected to remain), which equates to more than eight jobs for every housing unit. This employment base greatly improves both the City's and region's jobs-to-housing balance.

SR-210 runs through the northern portion of Renaissance with on-/off-ramps at Ayala and Alder Avenues. Renaissance takes advantage of the adjacent SR-210 and provides employment, retail, and offices uses along both sides of the freeway and at or near the freeway on-ramps. The uses north of SR-210 include employment-generating uses such as manufacturing, light industrial, distribution, warehousing, as well as wholesalers, furniture stores, and car, RV, and boat sales. The uses along the southern edge of SR-210 include retail centers and office complexes. The Town Center, which is located at the southwest intersection of Ayala and SR-210, accommodates an array of commercial uses. The corporate office center is envisioned as an intense office center, reinforcing the business corridor gateway into Rialto, maximizing freeway visibility, and strengthening the surrounding commercial uses. The uses at the intersection of Renaissance Parkway and Alder Avenue are retail and entertainment related.

### *Redevelopment*

*Goal 3-4: Revitalize aging and underperforming commercial and industrial areas.*

### *Specific Plan Response*

The Rialto City Council adopted resolutions declaring the intent to relocate the Rialto Airport operations to other nearby facilities. Consistent with the adopted resolutions and the goals and policies of the draft General Plan Update, The Specific Plan focuses on the infill of the closed and underutilized Rialto Airport. With the closure of the Rialto Airport, the Renaissance Specific Plan serves as the long-range plan for the development of the 1,439 acres that consist of the airport and the area north of the airport, on both sides of SR-210. Implementation of the Specific plan would introduce commercial, retail, office and industrial uses adjacent to and around the existing commercial and industrial uses that would remain as a part of the Specific Plan development. The introduction of these new uses would help enhance and revitalize the existing commercial and industrial uses located

within the boundaries of the Specific Plan, and also those located north of SR-210. Additionally, the Renaissance Specific Plan provides detailed development standards and design guidelines that will ensure that high-quality architecture and landscaping are provided throughout the Specific Plan area, thereby ensuring that the visibility, aesthetic quality, and viability of the existing commercial and industrial uses would be further enhanced.

### ***Infrastructure***

*Goal 3-6: Require that all developed areas within Rialto are adequately served with essential public services and infrastructure.*

*Goal 3-8: Promote affordable and quality water service capable of adequately meeting normal and emergency water demands to all areas in Rialto.*

*Goal 3-9: Upgrade and maintain an improved wastewater system with adequate plant efficiency and capacity to protect the health and safety of all Rialto residents, businesses, and institutions.*

*Goal 3-12: Improve access to high-quality education opportunities for all Rialto residents and for all stages of life.*

*Goal 3-13: Provide high-quality library resources to meet the educational, cultural, and life-long learning needs of all residents.*

### ***Specific Plan Response***

The Specific Plan outlines the phasing and infrastructure plans necessary to provide for the cohesive and organized development of the project area. More specifically, Section 3, *Development Criteria*, of the Specific Plan outlines the necessary water, sewer, and drainage infrastructure that would be required to support development of Renaissance. Additionally, infrastructure improvements would be phased in accordance with the proposed project's phasing plan, as outlined in Section 7, *Implementation Plan*. Furthermore, Section 7 outlines the means of financing the necessary infrastructure improvements.

As shown in Figure 2-2, *Land Use Diagram*, the Specific Plan includes the development of a school site within the boundaries of the Specific Plan that would serve the residents of Renaissance. Fire services would be provided by the fire station in the western edge of Renaissance along Ayala Drive. Additionally, individual project applicants within Renaissance would be required to pay all applicable development impact fees set forth by the City of Rialto, including school, library, police, and fire fees, to mitigate impacts on public services.

### ***Circulation Element***

The goals that directly relate to the Specific Plan are presented below.

*Goal 2-12: Design new streets to be pedestrian friendly.*

## Appendices

*Goal 4-1: Provide transportation improvements to reduce traffic congestion associated with regional and local trip increases.*

*Goal 4-2: Protect residential neighborhoods from through traffic impacts.*

*Goal 4-4: Protect school children and others from traffic hazards around schools.*

*Goal 4-5: Ensure the provision of adequate, convenient, and safe parking for all land uses.*

*Goal 4-6: Provide for all residents and businesses to have equal access to reliable and convenient public transit services.*

*Goal 4-8: Establish and maintain a comprehensive system of pedestrian trails and bicycle routes that provide viable connections throughout the City.*

*Goal 4-9: Promote walking.*

*Goal 4-10: Provide a circulation system that supports Rialto's position as a logistics hub.*

### ***Specific Plan Response***

The Village, which contains the sensitive land uses (residential, school, and park and open space), is located and designed in such a manner as to minimize through-traffic. As shown in Figure 3-14, *Truck Routes*, truck trips are restricted to local deliveries (no through trips) on those portions of: 1) Linden Avenue and Renaissance Parkway that are adjacent to residentially designated areas, and 2) Casmalia Street, Renaissance Parkway, and Baseline Road west of Alder Avenue. Linden Avenue, which is a Secondary Arterial, is the only through-street to cut through the Village residential area. Onsite truck traffic will be directed to Alder Avenue through the use of directional signage at business driveways.

Figure 3-15, *Bicycle and Pedestrian Circulation Plan*, shows the comprehensive system of pedestrian and bicycle routes and trails that would provide viable connections throughout Renaissance and would also connect to the City's routes and trails system. Within the Village, off-street (paseo, sidewalks) and on-street (bicycle lanes and routes) pedestrian systems are provided to allow convenient and safe non-vehicular circulation. In addition, sidewalks in the non-residential areas of the Specific Plan, which are detailed on each of the street sections contained in the Specific Plan, provide a comprehensive system of pedestrian movement. The street sections and development standards contained in the Specific Plan detail the separations (e.g., parkways, buffers) and setbacks from the roadways that are designed to protect pedestrians from vehicular activity. The roadway system is also designed to be pedestrian friendly and walkable. Additionally, the mix of residential and non-residential uses allows residents of Renaissance to live, work, shop, walk, and gather, thereby reducing the need for personal or public transportation.

Detailed parking standards, which address residential and non-residential uses, are contained in the Specific Plan.

As outlined in Section 3, *Development Criteria*, one Omnitrans bus line provides service to Renaissance. Route 10 operates along Baseline Road and provides connections between downtown San Bernardino and the Metrolink station in Fontana. While it is not within the control of the City or developer to dictate the location of future Omnitrans transit routes and stops, it is believed that, at buildout of Renaissance, expanded transit service would be merited within the Specific Plan. Ideally, transit service would be provided in the Town Center/Village area, Corporate Center, as well as to areas designated as Employment and Business Center.

### ***Safety and Noise Element***

The goals that directly relate to the Specific Plan are presented below.

*Goal 5-1: Minimize hazards to public health, safety, and welfare associated with geotechnical hazards.*

*Goal 5-2: Minimize the risk and damage from flood hazards.*

*Goal 5-3: Increase the City's fire protection capabilities, and implement fire prevention regulations and standards that minimize potential fire hazards and fire losses.*

*Goal 5-4: Protect the health and welfare of the public, environment, and economy by providing for the safe and responsible management of hazardous materials and wastes.*

*Goal 5-5: Minimize the generation of hazardous waste in Rialto.*

*Goal 5-8: Provide effective and comprehensive policing services that meet the safety needs of Rialto.*

*Goal 5-10: Minimize the impact of point source and ambient noise levels throughout the community.*

*Goal 5-11: Minimize the impacts of transportation-related noise.*

*Goal 5-12: Minimize the risks associated with wind hazards.*

### ***Specific Plan Response***

Renaissance includes a drainage plan that conforms to the City's Drainage Master Plan. All drainage facilities will be designed and constructed in accordance the City and county flood control district standards and specifications. Additionally, development of individual projects will be required to conform to the provisions of the City's Municipal Code and California Building Code.

## Appendices

Renaissance is not located in known natural or man-made hazard areas. All construction in Renaissance will be required to obtain a building permit, which includes review and approval of the Fire Department. The business related land uses allow uses that may utilize hazardous materials. The uses of any listed hazardous materials in the City are also subject to review and approval by the Fire Department.

Adequate fire and police services will be provided to Renaissance. Fire services would be provided by the fire station in the western edge of Renaissance along Ayala Drive. Police services would be provided by the Rialto Police Department. Additionally, individual project applicants within Renaissance would be required to pay all applicable development impact fees set forth by the City of Rialto, including school, library, police, and fire fees, to mitigate impacts on public services.

Development within Renaissance will comply with the City's interior and exterior noise standards. In conjunction with plan review, the City will review noise attenuation prior to issuance of building permits. The Specific Plan also includes provisions to protect sensitive receptors, such as residences, from excessive noise, including setbacks, restrictions on hours of operation, and design considerations.

The residential properties in Renaissance are setback and protected from noise originating on SR-210. The Village is located and designed in such a manner as to minimize through-traffic and exposure to excessive noise. The Renaissance Parkway, Locust Avenue, and Ayala Drive form the perimeter of the Village and provide separation from non-residential uses to the north and the west. The 16-acre public park and open space buffer help along the southern edge of the Village provide separation from non-residential uses to the south. Linden Avenue, which is a Secondary Arterial, is the only through-street to cut through the Village residential area. The street sections and development standards contained in the Specific Plan detail the separations and setbacks from the roadways that are designed to protect residences from excessive vehicular noise.

### ***Housing Element***

The goals that directly relate to the Specific Plan are presented below.

*Goal 3-5: Assist in the preservation, improvement, and production of housing stock available to lower- and moderate-income residents.*

*Goal 6-1: Maintain and improve the quality of existing housing and neighborhoods in Rialto.*

*Goal 6-2: Promote and encourage housing development that adequately meets the needs of all socioeconomic segments of the community and region.*

*Goal 6-5: Promote equal opportunity for all residents to reside in the housing of their choice.*

### ***Specific Plan Response***

Consistent with the adopted resolutions and the goals and policies of the draft General Plan Update, the Renaissance Specific Plan serves as the long-range plan for the development of the 1,439 acres that consist of the airport and the area north of the airport, on both sides of SR-210. Implementation of the Specific plan would introduce a range of residential densities and rental and for-sale unit types within the Village to attract different lifestyles and life stages. A maximum of 1,667 residential units are proposed, which would include high-quality single-family detached homes, small-lot detached homes, townhomes, apartments, courtyard clusters, and condominiums. The Low Density Residential land use designation provides for 446 detached, single-family units. The Medium Density Residential designation provides for 290 small-lot detached, duplexes, and townhomes. The Medium High Density Residential designation provides for 818 attached units, duplexes, and townhomes. Finally, the High Density Residential designation provides for 113 attached units.

These residential products range from 3 to 30 units per acre and are intended to serve a wide range of the residential market—families, single professionals, first-time homebuyers, empty-nesters looking to downsize, and seniors. The Specific Plan would also include attractive, modern housing that would provide “move-up” opportunities for residents in the project vicinity and the City of Rialto. While units in Renaissance are not specifically targeted to the very low, low, and moderate income ranges, the higher density products can be expected to be priced at a level that is more affordable to the workforce.

Additionally, the design standards outlined in the Specific Plan (e.g., permitted residential uses, densities, setbacks, and building heights) would ensure that a diversity of housing types be developed, consistent with the City’s vision of creating an exemplary community environment for the Renaissance community.

### ***Cultural and Historical Resources Element***

The goals that directly relate to the Specific Plan are presented below.

## Appendices

*Goal 7-3: Identify, document, and protect significant archaeological resources in Rialto.*

### *Specific Plan Response*

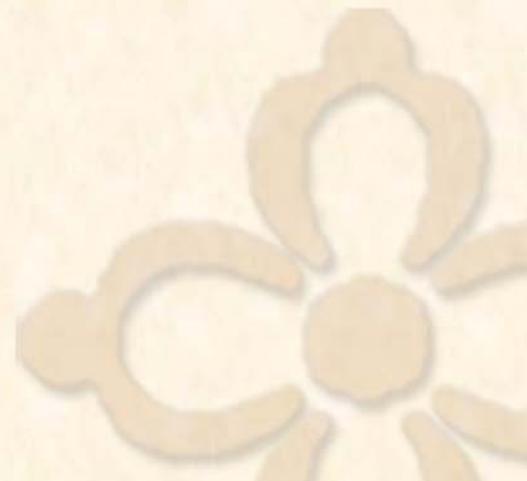
The project area is partially developed and includes an airport, industrial uses, scattered residences, roadways, utilities, and some vacant and undeveloped areas. There are no known historical or archaeological resources within the Renaissance Specific Plan area. The Cultural Resources section of the accompanying EIR evaluated the potential for impacts on cultural resources as a result of the project development. As detailed in the accompanying EIR, any historical or archaeological resources discovered during demolition and earth work shall be appropriately surveyed and recorded. Development of individual projects throughout the Renaissance Specific Plan would require some degree of ground-disturbing monitoring from a qualified archeologist, which would minimize the potential to disturb significant archeological resources. If cultural resources are discovered, they would be addressed through the necessary testing, archiving, and recovery prior to development of the site.

# Appendix C



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DEVELOPMENT TRACKING SYSTEM



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# Appendices

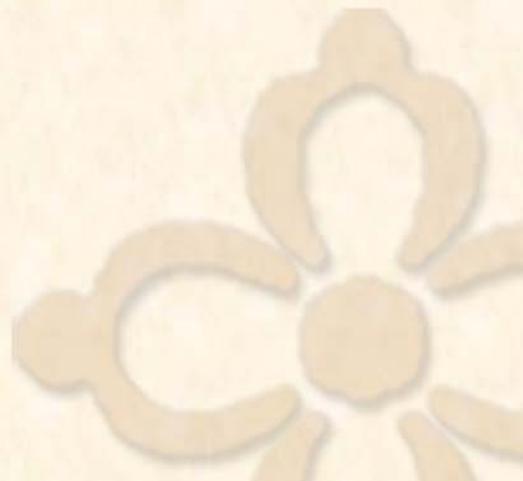
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# Appendix D



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ENVIRONMENTAL IMPACT REPORT



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## **APPENDIX D: ENVIRONMENTAL IMPACT REPORT**

Under a separate cover. Please see the City of Rialto Department of Development Services.

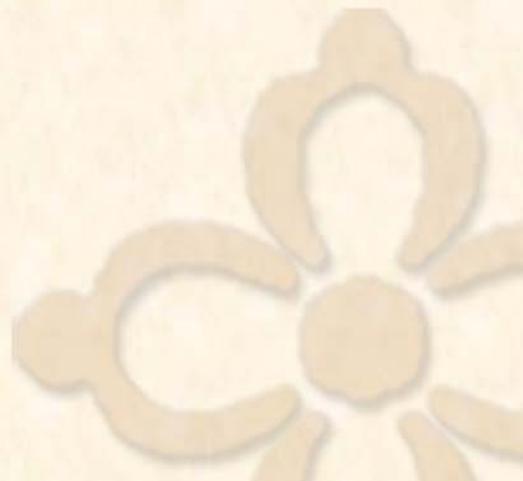
# Appendices

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# Appendix E

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## REVISED RIALTO AIRPORT SPECIFIC PLAN



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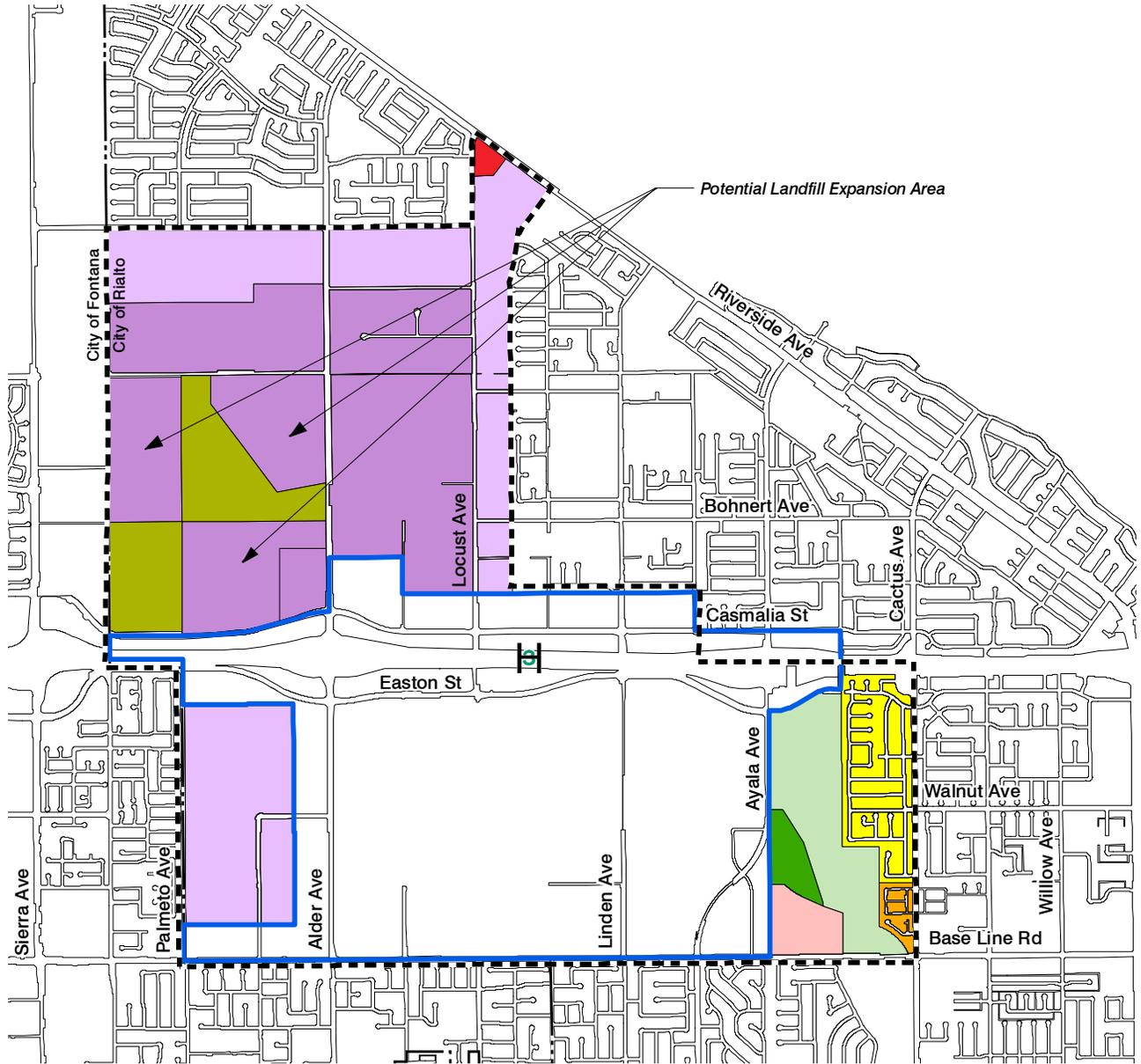
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## **APPENDIX E: REVISED RIALTO AIRPORT SPECIFIC PLAN**

On December 6, 2005, the City Council adopted resolutions declaring the intent to relocate the Rialto Airport operations to other nearby facilities. With the closure of the Rialto Airport, Renaissance was able to be planned. With the adoption of the Renaissance Specific Plan, the Rialto Airport Specific Plan is required to be amended to “take out” the 1,439 acres comprising Renaissance, leaving the remainder of the Rialto Airport Specific Plan unchanged. The following revisions to the Rialto Airport Specific Plan land use plan (Exhibits 3 and 8) and special overlay districts (Exhibit 4) will result from the adoption of the Renaissance Specific Plan.

# Appendices

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**Residential**

- R-1-C Single-Family (1-6 DU's/Ac)  
(Single-family detached)
- R-CL Single-Family (7-12 DU's/Ac)  
(Small lot, clustered, single-family detached)

**Commercial**

- NC Neighborhood Commercial  
(Less than 10 acres)

**Industrial**

- I-AR Airport-Related Planned Industrial Development  
(Airport-related office and light industrial uses)
- I-PID Planned Industrial Development  
(Office/industrial/light manufacturing uses)
- I-GM General Manufacturing  
(Most intensive industrial uses)

**Recreation**

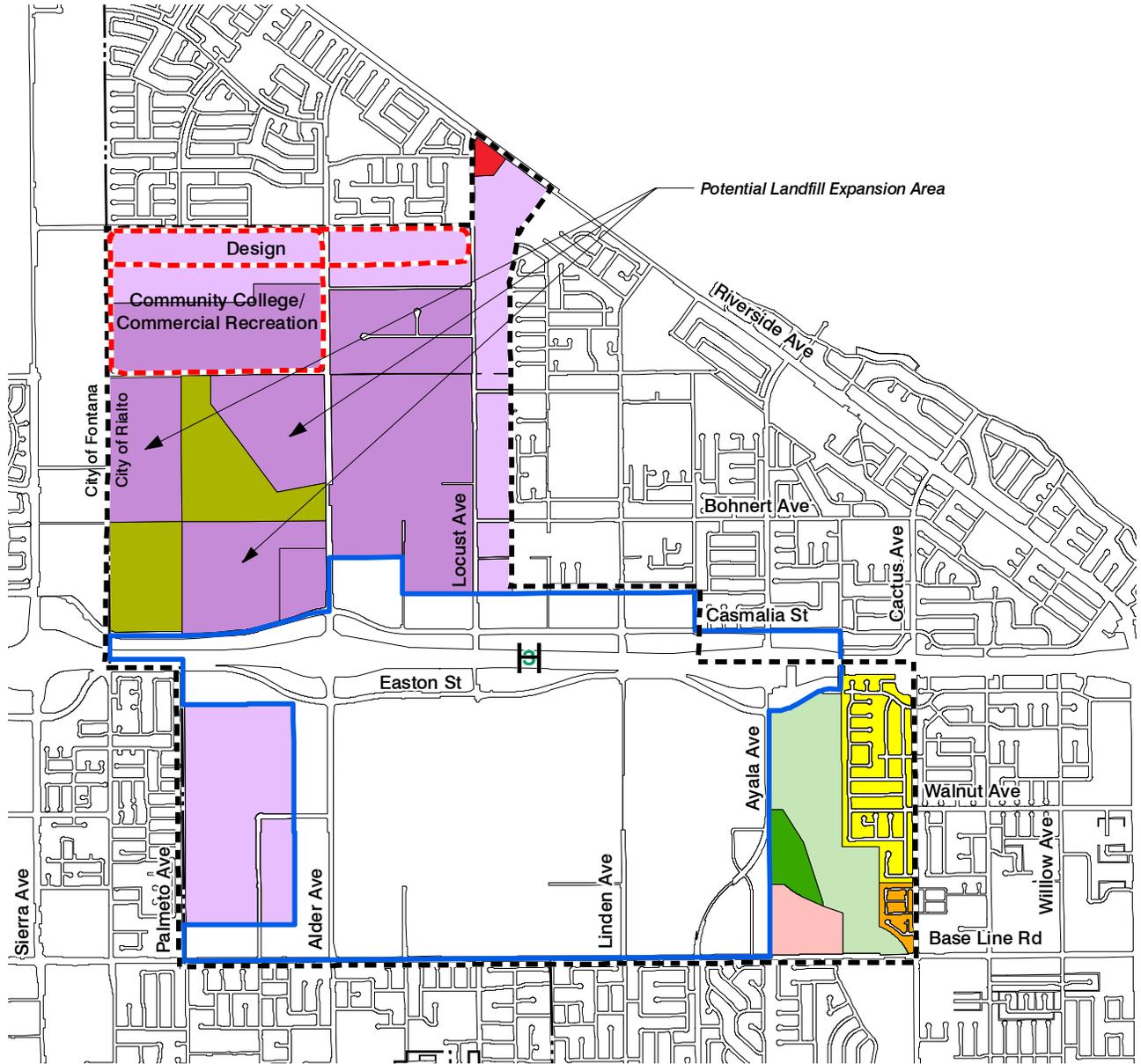
- R-P Park (Eves Park)

**Open Space**

- OS-CB Cactus Basin
- OS-L Landfill (To become open space or  
used for recreation after closure of landfill)

- Rialto Airport Specific Plan Boundary
- Renaissance Specific Plan Boundary (not a part)





**Residential**

- R-1-C Single-Family (1-6 DU's/Ac)  
(Single-family detached)
- R-CL Single-Family (7-12 DU's/Ac)  
(Small lot, clustered, single-family detached)

**Commercial**

- NC Neighborhood Commercial  
(Less than 10 acres)

**Industrial**

- I-AR Airport-Related Planned Industrial Development  
(Airport-related office and light industrial uses)
- I-PID Planned Industrial Development  
(Office/industrial/light manufacturing uses)
- I-GM General Manufacturing  
(Most intensive industrial uses)

**Recreation**

- R-P Park (Eves Park)

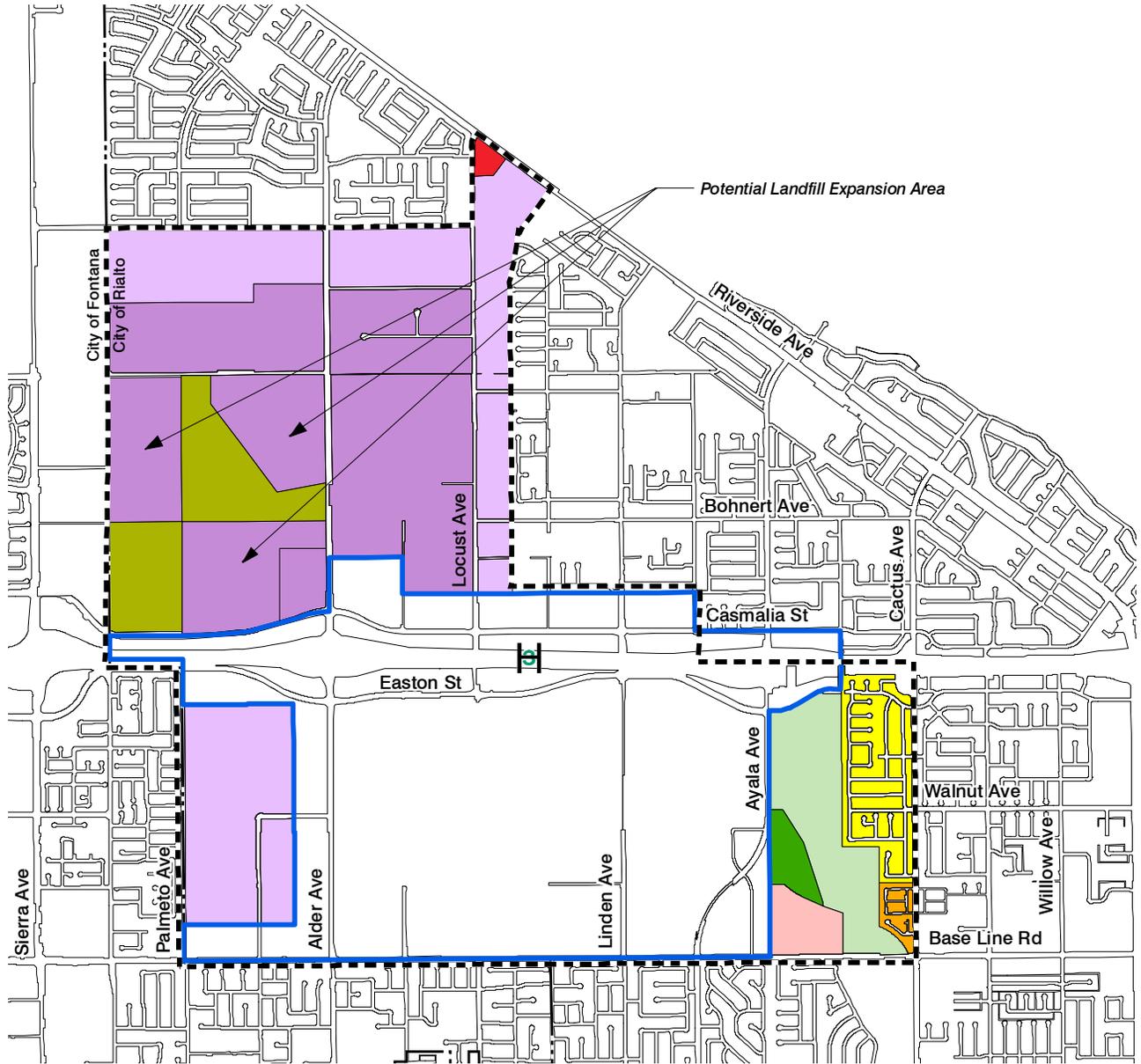
**Open Space**

- OS-CB Cactus Basin
- OS-L Landfill (To become open space or  
used for recreation after closure of landfill)

- Rialto Airport Specific Plan Boundary
- Special Overlay Districts
- Renaissance Specific Plan Boundary (not a part)



RIALTO AIRPORT SPECIFIC PLAN  
**Special Overlay Districts**



**Residential**

- R-1-C Single-Family (1-6 DU's/Ac)  
(Single-family detached)
- R-CL Single-Family (7-12 DU's/Ac)  
(Small lot, clustered, single-family detached)

**Commercial**

- NC Neighborhood Commercial  
(Less than 10 acres)

**Industrial**

- I-AR Airport-Related Planned Industrial Development  
(Airport-related office and light industrial uses)
- I-PID Planned Industrial Development  
(Office/industrial/light manufacturing uses)
- I-GM General Manufacturing  
(Most intensive industrial uses)

**Recreation**

- R-P Park (Eves Park)

**Open Space**

- OS-CB Cactus Basin
- OS-L Landfill (To become open space or  
used for recreation after closure of landfill)

- Rialto Airport Specific Plan Boundary
- Renaissance Specific Plan Boundary (not a part)

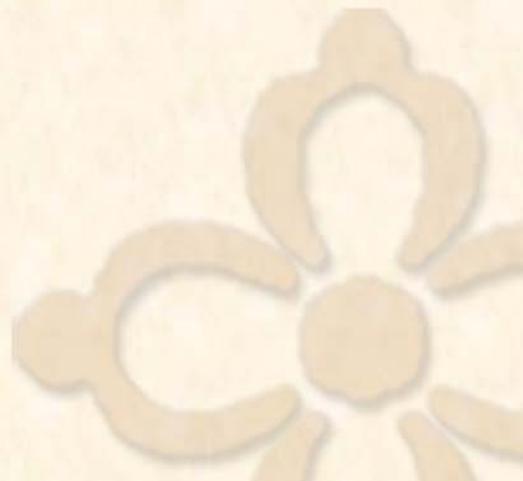


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# Appendix F

ASSESSOR'S PARCEL NUMBERS



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## **APPENDIX F: ASSESSOR'S PARCEL NUMBERS**

## Appendices

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<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0239-211-15-0000	1,722,580	39.55
0239-211-30-0000	1,306,800	30.00
0239-211-31-0000	3,783,186	86.85
0240-171-02-0000	311,018	7.14
0240-171-30-0000	8,500	0.20
0240-171-27-0000	79,279	1.82
0240-171-21-0000	73,181	1.68
0240-171-20-0000	158,123	3.63
0240-171-03-0000	130,680	3.00
0240-181-13-0000	31,640	0.73
0240-181-14-0000	33,139	0.76
0240-181-16-0000	32,956	0.76
0240-181-17-0000	32,956	0.76
0240-181-22-0000	217,800	5.00
0240-181-23-0000	42,000	0.96
0240-181-26-0000	39,000	0.90
0240-181-27-0000	36,000	0.83
0240-181-28-0000	14,766	0.34
0240-181-29-0000	17,824	0.41
0240-181-30-0000	18,500	0.42
0240-181-32-0000	8,400	0.19
0240-181-33-0000	180,774	4.15
0240-181-34-0000	21,939	0.50
0240-181-35-0000	67,518	1.55
0240-191-41-0000	14,416	0.33
0240-191-38-0000	5,261	0.12
0240-191-37-0000	4,500	0.10
0240-191-07-0000	52,708	1.21
0240-191-36-0000	8,733	0.20
0240-191-35-0000	156,816	3.60
0240-191-34-0000	135,907	3.12
0240-191-33-0000	49,658	1.14
0240-191-32-0000	3,520	0.08
0240-191-31-0000	217,800	5.00
0240-191-30-0000	217,800	5.00
0240-191-29-0000	132,422	3.04
0240-191-28-0000	16,500	0.38
0240-191-27-0000	143,748	3.30
0240-191-26-0000	21,780	0.50
0240-191-25-0000	13,068	0.30
0240-191-23-0000	21,780	0.50
0240-191-22-0000	43,560	1.00

## Appendices

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0240-191-18-0000	43,560	1.00
0240-191-16-0000	217,800	5.00
0240-191-15-0000	196,020	4.50
0240-191-09-0000	62,272	1.43
0240-191-08-0000	67,520	1.55
0240-191-42-0000	39,690	0.91
0240-191-43-0000	43,996	1.01
0240-201-01-0000	217,800	5.00
0240-201-42-0000	56,628	1.30
0240-201-44-0000	60,984	1.40
0240-201-45-0000	16,200	0.37
0240-201-46-0000	16,200	0.37
0240-201-47-0000	19,035	0.44
0240-201-02-0000	217,800	5.00
0240-201-20-0000	40,838	0.94
0240-201-04-0000	41,540	0.95
0240-201-05-0000	83,080	1.91
0240-201-23-0000	82,764	1.90
0240-201-08-0000	108,900	2.50
0240-201-29-0000	204,350	4.69
0240-201-30-0000	41,520	0.95
0240-201-32-0000	87,120	2.00
0240-201-14-0000	60,112	1.38
0240-201-35-0000	108,900	2.50
0240-201-38-0000	31,050	0.71
0240-201-40-0000	42,848	0.98
0240-201-09-0000	52,272	1.20
0240-201-10-0000	52,272	1.20
0240-201-12-0000	47,916	1.10
0240-201-13-0000	47,916	1.10
0240-201-18-0000	47,916	1.10
0240-201-21-0000	48,351	1.11
0240-201-22-0000	47,916	1.10
0240-201-34-0000	104,544	2.40
0240-201-41-0000	82,764	1.90
0240-201-31-0000	41,520	0.95
0240-201-15-0000	52,272	1.20
0240-201-43-0000	54,886	1.26
0240-211-13-0000	98,010	2.25
0240-211-14-0000	219,152	5.03
0240-211-15-0000	759,686	17.44
0240-211-16-0000	439,956	10.10

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0240-211-17-0000	81,892	1.88
0240-211-20-0000	20,460	0.47
0240-211-21-0000	123,710	2.84
0240-211-22-0000	88,427	2.03
0240-211-23-0000	130,680	3.00
0240-211-24-0000	237,402	5.45
0240-211-25-0000	88,427	2.03
0240-211-26-0000	138,956	3.19
0240-211-29-0000	13,173	0.30
0240-211-30-0000	17,700	0.41
0240-211-31-0000	12,543	0.29
0240-211-32-0000	22,170	0.51
0240-211-33-0000	204,296	4.69
0240-211-34-0000	181,645	4.17
0240-221-02-0000	38,645	0.89
0240-221-03-0000	77,383	1.78
0240-221-04-0000	83,853	1.93
0240-221-05-0000	784,080	18.00
0240-221-06-0000	207,346	4.76
0240-221-07-0000	207,346	4.76
0240-221-08-0000	435,600	10.00
0240-221-09-0000	43,560	1.00
0240-221-10-0000	355,449	8.16
0240-221-19-0000	43,560	1.00
0240-221-21-0000	43,560	1.00
0240-221-22-0000	16,093	0.37
0240-221-24-0000	43,995	1.01
0240-221-26-0000	38,544	0.88
0240-221-27-0000	43,995	1.01
0240-221-14-0000	43,560	1.00
0240-221-15-0000	87,120	2.00
0240-221-18-0000	43,560	1.00
0240-221-12-0000	296,208	6.80
0240-221-13-0000	43,560	1.00
0240-221-23-0000	69,696	1.60
0240-221-28-0000	308,840	7.09
0240-221-16-0000	43,560	1.00
0240-221-17-0000	43,560	1.00
0240-231-02-0000	435,600	10.00
0240-231-06-0000	81,840	1.88
0240-231-09-0000	435,600	10.00
0240-231-10-0000	87,120	2.00

## Appendices

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0240-231-11-0000	43,560	1.00
0240-231-13-0000	87,120	2.00
0240-231-16-0000	21,780	0.50
0240-231-17-0000	18,480	0.42
0240-231-18-0000	348,480	8.00
0240-231-20-0000	221,720	5.09
0240-231-21-0000	213,879	4.91
0240-231-22-0000	292,723	6.72
0240-231-23-0000	142,876	3.28
0240-231-25-0000	83,199	1.91
0240-231-27-0000	165,092	3.79
0240-231-28-0000	43,560	1.00
0240-231-29-0000	38,333	0.88
0240-231-30-0000	43,560	1.00
0240-241-01-0000	87,120	2.00
0240-241-02-0000	87,120	2.00
0240-241-05-0000	51,150	1.17
0240-241-06-0000	47,916	1.10
0240-241-07-0000	47,916	1.10
0240-241-10-0000	184,694	4.24
0240-241-11-0000	84,400	1.94
0240-241-14-0000	61,404	1.41
0240-241-15-0000	61,404	1.41
0240-241-16-0000	81,840	1.88
0240-241-17-0000	81,840	1.88
0240-241-19-0000	19,200	0.44
0240-241-20-0000	83,200	1.91
0240-241-21-0000	40,933	0.94
0240-241-22-0000	40,933	0.94
0240-241-23-0000	42,112	0.97
0240-241-24-0000	42,112	0.97
0240-241-29-0000	54,450	1.25
0240-241-30-0000	43,428	1.00
0240-241-32-0000	40,920	0.94
0240-241-33-0000	31,668	0.73
0240-241-34-0000	43,428	1.00
0240-241-37-0000	76,237	1.75
0240-241-38-0000	207,345	4.76
0240-241-40-0000	43,428	1.00
0240-241-41-0000	43,428	1.00
0240-241-42-0000	42,940	0.99
0240-241-43-0000	130,680	3.00

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0240-241-44-0000	117,176	2.69
0240-241-45-0000	54,450	1.25
0240-241-46-0000	54,450	1.25
0240-241-47-0000	94,960	2.18
0240-241-48-0000	114,562	2.63
0240-241-49-0000	54,450	1.25
0240-241-50-0000	50,529	1.16
0240-241-51-0000	786,258	18.05
0240-251-04-0000	435,600	10.00
0240-251-06-0000	94,089	2.16
0240-251-09-0000	871,200	20.00
0240-251-10-0000	871,200	20.00
0240-251-11-0000	435,600	10.00
0240-251-14-0000	814,136	18.69
0240-251-21-0000	2,469,852	56.70
0240-251-22-0000	639,896	14.69
0240-251-23-0000	39,366	0.90
0240-251-24-0000	174,240	4.00
0240-251-25-0000	87,120	2.00
0240-251-28-0000	1,337,292	30.70
0240-251-29-0000	1,133,431	26.02
0240-251-30-0000	47,044	1.08
0240-251-32-0000		0.00
0240-251-33-0000	348,203	7.99
0240-251-34-0000	522,997	12.01
0240-261-22-0000	46,173	1.06
0240-261-02-0000	19,200	0.44
0240-261-03-0000	42,760	0.98
0240-261-04-0000	42,760	0.98
0240-261-08-0000	42,760	0.98
0240-261-12-0000	211,200	4.85
0240-261-13-0000	49,658	1.14
0240-261-18-0000	39,680	0.91
0240-261-19-0000	46,173	1.06
0240-261-20-0000	46,080	1.06
0240-261-21-0000	46,173	1.06
0240-261-49-0000	64,033	1.47
0240-261-23-0000	21,170	0.50
0240-261-25-0000	117,612	2.70
0240-261-28-0000	76,665	1.76
0240-261-30-0000	789,742	18.13
0240-261-32-0000	42,760	0.98

## Appendices

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0240-261-39-0000		0.00
0240-261-40-0000	26,000	0.60
0240-261-44-0000	31,001	0.71
0240-261-60-0000	44,610	1.02
0240-261-52-0000	99,752	2.29
0240-261-54-0000	24,829	0.57
0240-261-56-0000	24,829	0.57
0240-261-58-0000	104,544	2.40
0240-261-59-0000	24,662	0.57
0240-261-65-0000	67,005	1.54
0240-261-62-0000	44,431	1.02
0240-261-64-0000	148,540	3.41
0240-261-66-0000	103,673	2.38
0240-261-36-0000	36,015	0.83
0240-261-45-0000	24,080	0.55
0240-261-53-0000	12,000	0.28
0240-261-31-0000	42,760	0.98
0240-261-63-0000	13,475	0.31
0240-261-34-0000	42,760	0.98
0240-261-35-0000	87,120	2.00
0240-261-05-0000	42,760	0.98
0240-261-06-0000	42,760	0.98
0240-261-07-0000	42,760	0.98
0240-261-55-0000	24,640	0.57
0240-261-67-0000	43,995	1.01
0240-261-57-0000	24,640	0.57
0240-261-16-0000	47,916	1.10
0240-261-17-0000	56,628	1.30
0240-261-43-0000	56,628	1.30
0240-261-50-0000	15,125	0.35
0240-261-61-0000	18,750	0.43
0240-261-51-0000	94,960	2.18
0240-261-01-0000	66,320	1.52
0264-151-45-0000	238,709	5.48
0264-151-49-0000	27,007	0.62
0264-151-50-0000	1,119,056	25.69
0264-151-51-0000	11,326	0.26
0264-151-52-0000	1,888,761	43.36
0264-151-53-0000	871,200	20.00
0264-151-57-0000	328,442	7.54
0264-151-59-0000	24,829	0.57
0264-151-62-0000	209,523	4.81

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0264-151-63-0000	288,367	6.62
0264-151-65-0000		0.00
0264-151-66-0000	1,873,516	43.01
0264-151-67-0000		0.00
0264-151-68-0000		0.00
0264-151-72-0000	1,679,238	38.55
0264-151-73-0000	189,050	4.34
0264-211-05-0000	109,335	2.51
0264-211-06-0000	94,961	2.18
0264-211-07-0000	94,961	2.18
0264-211-08-0000	108,900	2.50
0264-211-09-0000	108,900	2.50
0264-211-10-0000	108,900	2.50
0264-211-11-0000	109,335	2.51
0264-211-12-0000	109,335	2.51
0264-211-15-0000	147,668	3.39
0264-211-16-0000	142,441	3.27
0264-211-17-0000	75,358	1.73
0264-211-18-0000	125,017	2.87
0264-211-19-0000	135,036	3.10
0264-211-20-0000	337,154	7.74
0264-211-21-0000		0.00
0264-212-05-0000	218,671	5.02
0264-212-06-0000	218,671	5.02
0264-212-07-0000	218,236	5.01
0264-212-08-0000	218,236	5.01
0264-212-09-0000	208,216	4.78
0264-212-12-0000	215,622	4.95
0264-212-15-0000	187,308	4.30
0264-212-17-0000	134,164	3.08
0264-212-18-0000	66,211	1.52
0264-212-19-0000	57,063	1.31
0264-212-20-0000	147,232	3.38
0264-212-27-0000	133,729	3.07
0264-212-28-0000	170,775	3.92
0264-212-29-0000	123,275	2.83
0264-212-30-0000	16,000	0.37
0264-212-31-0000	47,916	1.10
0264-212-32-0000	57,063	1.31
0264-212-33-0000	49,222	1.13
0264-212-34-0000	49,222	1.13
0264-212-35-0000	47,916	1.10

## Appendices

<b>Assessor's Parcel Numbers</b>		
<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
0264-212-36-0000	47,916	1.10
0264-212-37-0000	108,029	2.48
0264-212-38-0000	46,609	1.07
0264-212-39-0000	44,866	1.03
0264-212-40-0000	44,866	1.03
0264-212-42-0000	134,164	3.08
0264-212-43-0000	132,858	3.05
0264-212-44-0000	87,120	2.00
0264-212-45-0000	317,552	7.29
0264-212-46-0000	300	0.01
0264-212-47-0000		0.00
0264-212-51-0000	30,812	0.71
0264-212-52-0000	70,523	1.62
0264-212-53-0000	47,816	1.10
0264-213-35-0000	108,028	2.48
0264-213-36-0000	47,916	1.10
0264-213-37-0000	47,916	1.10
0264-213-38-0000	49,222	1.13
0264-213-39-0000	49,222	1.13
0264-213-40-0000	49,222	1.13
0264-213-41-0000	49,658	1.14
0264-213-42-0000	61,419	1.41
0264-213-43-0000	67,518	1.55
0264-213-44-0000	69,260	1.59
0264-361-01-0000	65,340	1.50
0264-361-02-0000	53,143	1.22
0264-361-03-0000	46,609	1.07
0264-361-04-0000	47,044	1.08
0264-361-05-0000	43,560	1.00
0264-361-06-0000	44,866	1.03
1133-181-05-0000	217,800	5.00
1133-181-06-0000	217,800	5.00
1133-181-14-0000	719,935	16.53
1133-181-15-0000	402,581	9.24
1133-191-01-0000	235,224	5.40
1133-191-20-0000	43,524	1.00
1133-191-21-0000	43,524	1.00
1133-191-22-0000	43,803	1.01
1133-191-11-0000	38,368	0.88
1133-191-12-0000	43,648	1.00
1133-191-13-0000	43,648	1.00
1133-191-15-0000	43,425	1.00

**Assessor's Parcel Numbers**

<b>APN</b>	<b>S.F.</b>	<b>AC.</b>
1133-191-16-0000	43,524	1.00
1133-191-17-0000	43,368	1.00
1133-191-18-0000	43,524	1.00
1133-191-19-0000	43,524	1.00
1133-191-14-0000	333,669	7.66
1133-241-04-0000	85,378	1.96
1133-241-05-0000	201,247	4.62
1133-241-06-0000	103,672	2.38
1133-251-02-0000		0.00
1133-251-03-0000	601,563	13.81
1133-261-01-0000	802,811	18.43
1133-271-01-0000	378,972	8.70
1133-491-03-0000	198,154	4.55
1133-491-02-0000	204,732	4.70
1133-491-04-0000	118,483	2.72
1133-491-05-0000	79,714	1.83
1133-491-01-0000	204,732	4.70
1133-511-01-0000	315,810	7.25
1133-521-02-0000	157,844	3.62
1133-521-04-0000	98,445	2.26
1133-591-01-0000	64,469	1.48
1133-591-04-0000	38,428	0.88
1133-591-07-0000	32,850	0.75
1133-591-09-0000	11,500	0.26

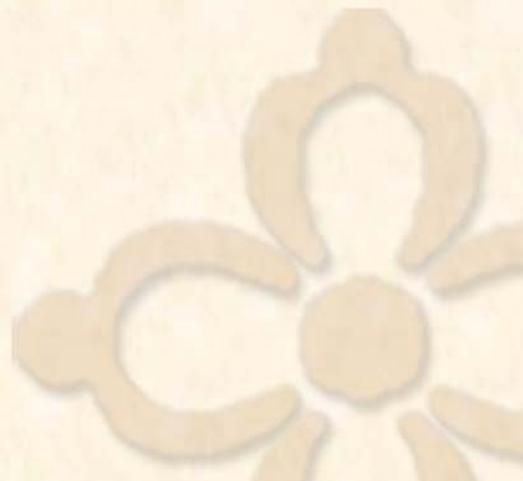
## Appendices

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# Appendix G

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## ADOPTING ORDINANCES



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## **APPENDIX G: ADOPTING ORDINANCE**



1 submitted by the applicant, it was determined that this project could have potentially significant impacts.  
2 Based on this determination, an EIR (Environmental Assessment Review No. 06-03 has been prepared in  
3 accordance with CEQA.

4 **SECTION 3.** That the Land use Map of the Rialto General Plan will be changed from: General  
5 Industrial, General Commercial, Public Use-Reservoir/Well/Energy, Industrial Park, Open Space Flood  
6 Control, Public Use-Airport and Community Commercial to "Specific Plan" overlay for the 1,4453 acre site  
7 legally described in the attached Exhibit 'A'. The text, tables and charts of other elements in the General  
8 Plan including Land Use, Open Space and Circulation will also change as a result of the proposed project.

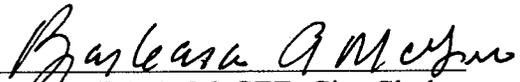
9 **SECTION 4.** The City Clerk shall certify adoption of this Ordinance and cause the same to be  
10 published once in a local newspaper and the same shall be in force and effect thirty (30) days after  
11 passage and adoption.

12  
13 PASSED, APPROVED AND ADOPTED this 23rd day of November, 2010.

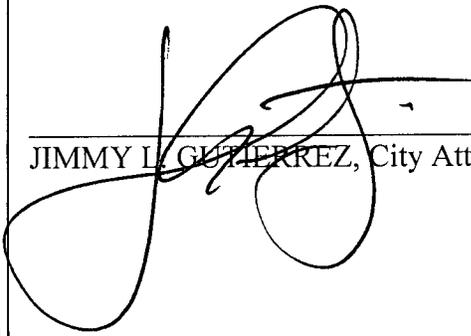
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18 GRACE VARGAS, Mayor

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ATTEST:

  
BARBARA A. McGEE, City Clerk

APPROVED AS TO FORM:

  
JIMMY L. GUTIERREZ, City Attorney

1 STATE OF CALIFORNIA )  
2 COUNTY OF SAN BERNARDINO )ss  
3 CITY OF RIALTO )

4 I, Barbara A. McGee, City Clerk of the City of Rialto, do hereby certify that the foregoing  
5 Ordinance No. 1477 was duly passed and adopted at a regular meeting of the City Council of the  
6 City of Rialto held on the 23rd day of November, 2010.  
7

8 Upon motion of Councilmember Baca Jr., seconded by Councilmember Scott, the foregoing  
9 Ordinance No. 1477 was duly passed and adopted.  
10

11 Vote on the Motion:

12 AYES: Mayor Vargas, Council Members: Baca Jr., Robertson, Scott and Palmer  
13

14 NOES: None

15 ABSTAIN: None  
16

17 ABSENT: None  
18

19 IN WITNESS WHEREOF, I have hereunto set my hand and the Official Seal of the City of  
20 Rialto, this 14th day of December, 2010.  
21

22   
23 BARBARA A. MCGEE, CITY CLERK  
24  
25  
26  
27  
28

# EXHIBIT ‘A’

## Renaissance Specific Plan

## Assessor’s Parcel Numbers

<b>Parcel Number</b>	<b>Square Feet</b>	<b>Lot Size</b>
0240-171-02	0	7.14 AC
0240-171-03	0	3 AC
0240-171-20	0	3.63 AC
0240-171-21	0	1.68 AC
0240-171-27	0	1.82 AC
0240-171-30	0	8500 SF
0240-171-31	881820	39.535 AC
0240-171-32	0	51.06 AC
0240-181-04	0	2.07 AC
0240-181-05	0	2.07 AC
0240-181-08	1170	2.5 AC
0240-181-13	984	31640 SF
0240-181-14	0	33139 SF
0240-181-16	1064	32956 SF
0240-181-17	0	32956 SF
0240-181-18	0	5 AC
0240-181-22	0	5 AC
0240-181-23	3200	42000 SF
0240-181-24	0	39000 SF
0240-181-26	1282	39000 SF
0240-181-27	1077	36000 SF
0240-181-28	1658	14766 SF
0240-181-29	0	17824 SF
0240-181-30	1450	18500 SF
0240-181-32	0	8400 SF
0240-181-33	1350	4.09 AC
0240-181-34	1027	21939 SF
0240-181-35	480	1.55 AC
0240-181-36	2651	7080 SF
0240-181-37	0	1.45 AC
0240-181-48	0	5.95 AC
0240-181-49	543400	28.64 AC
0240-181-50	0	10 AC
0240-191-07	0	1.21 AC
0240-191-08	0	1.55 AC
0240-191-09	0	1.43 AC
0240-191-15	0	4.5 AC
0240-191-16	0	5 AC
0240-191-18	0	1 AC
0240-191-22	0	1 AC
0240-191-23	0	21780 SF
0240-191-25	0	13068 SF
0240-191-26	0	21780 SF
0240-191-27	0	3.3 AC
0240-191-28	0	16500 SF
0240-191-29	0	3.04 AC
0240-191-30	0	5 AC
0240-191-31	0	5 AC
0240-191-32	0	3520 SF
0240-191-33	0	1.14 AC
0240-191-34	0	1.726 AC

0240-191-35	0 2.57 AC
0240-191-38	0 4465 SF
0240-191-41	0 14416 SF
0240-191-43	0 1.014 AC
0240-191-44	0 4869 SF
0240-191-45	0 13548 SF
0240-201-02	0 5 AC
0240-201-04	0 40588 SF
0240-201-05	0 1.907 AC
0240-201-08	0 2.5 AC
0240-201-09	1390 1.2 AC
0240-201-10	600 1.2 AC
0240-201-12	0 1.1 AC
0240-201-13	0 1.1 AC
0240-201-14	0 1.38 AC
0240-201-15	1840 1.2 AC
0240-201-18	1005 1.1 AC
0240-201-20	0 40838 SF
0240-201-21	3944 1.11 AC
0240-201-22	1520 1.1 AC
0240-201-23	0 1.9 AC
0240-201-24	0 5 AC
0240-201-26	0 5 AC
0240-201-28	4048 1.902 AC
0240-201-29	0 4.691 AC
0240-201-30	0 40602 SF
0240-201-31	1704 41520 SF
0240-201-32	0 2 AC
0240-201-34	1056 2.4 AC
0240-201-35	0 2.5 AC
0240-201-36	1036 2.485 AC
0240-201-37	0 2.5 AC
0240-201-38	0 31050 SF
0240-201-40	0 42848 SF
0240-201-41	1588 1.9 AC
0240-201-42	0 1.3 AC
0240-201-43	1718 1.26 AC
0240-201-44	0 1.4 AC
0240-201-45	0 16200 SF
0240-201-46	0 16200 SF
0240-201-47	0 19035 SF
0240-201-48	0 1.143 AC
0240-201-49	3005 1.653 AC
0240-201-55	22131 1.404 AC
0240-201-56 01	0
0240-201-57	16054 38693 SF
0240-211-13	0 1.738 AC
0240-211-14	0 4.468 AC
0240-211-15	0 16.6 AC
0240-211-16	0 8.645 AC
0240-211-17	0 1.88 AC
0240-211-20	0 20460 SF

0240-211-21	0 2.84 AC
0240-211-22	0 5.27 AC
0240-211-23	0 3 AC
0240-211-24	0 5.27 AC
0240-211-25	0 2.03 AC
0240-211-26	0 3.19 AC
0240-211-29	0 13173 SF
0240-211-30	0 22920 SF
0240-211-31	0 12543 SF
0240-211-32	0 22500 SF
0240-211-33	0 4.69 AC
0240-211-34	0 4.17 AC
0240-211-35	0 20911 SF
0240-211-36	0 17837 SF
0240-221-02	0 38674 SF
0240-221-03	0 1.776 AC
0240-221-04	0 1.925 AC
0240-221-05	0 18 AC
0240-221-06	0 4.76 AC
0240-221-07	0 4.76 AC
0240-221-08	0 10 AC
0240-221-09	0 1 AC
0240-221-10	0 8.16 AC
0240-221-12	1632 6.8 AC
0240-221-13	605 1 AC
0240-221-14	0 1 AC
0240-221-15	0 2 AC
0240-221-16	1200 1 AC
0240-221-17	1492 1 AC
0240-221-18	0 1 AC
0240-221-19	0 1 AC
0240-221-21	0 1 AC
0240-221-22	0 16093 SF
0240-221-23	1115 1.6 AC
0240-221-24	0 1.01 AC
0240-221-26	0 38581 SF
0240-221-27	0 1.01 AC
0240-221-28	0 7.09 AC
0240-231-02	0 10 AC
0240-231-06	1900 1.879 AC
0240-231-09	0 10 AC
0240-231-10	1376 1.881 AC
0240-231-11	0 40970 SF
0240-231-13	0 2 AC
0240-231-16	0 21780 SF
0240-231-17	0 19019 SF
0240-231-18	0 8 AC
0240-231-20	0 5.09 AC
0240-231-21	0 4.91 AC
0240-231-22	0 6.72 AC
0240-231-23	0 3.28 AC
0240-231-25	0 1.91 AC

0240-231-27	0
0240-231-28	0 15.25 AC
0240-231-29	0
0240-231-30	2351 1 AC
0240-241-01	0 2 AC
0240-241-02	0 2 AC
0240-241-05	0 1.174 AC
0240-241-06	0 1.1 AC
0240-241-07	0 1.1 AC
0240-241-10	0 4.24 AC
0240-241-14	551 1.41 AC
0240-241-15	0 1.41 AC
0240-241-16	0 1.97 AC
0240-241-17	0 1.879 AC
0240-241-19	0 19200 SF
0240-241-20	1274 1.91 AC
0240-241-21	1452 40933 SF
0240-241-22	2007 40933 SF
0240-241-23	650 42112 SF
0240-241-24	1020 42112 SF
0240-241-29	0 1.25 AC
0240-241-30	1547 43428 SF
0240-241-32	0 40920 SF
0240-241-33	0 31668 SF
0240-241-34	0 43428 SF
0240-241-37	0 1.837 AC
0240-241-38	0 4.76 AC
0240-241-40	1206 43428 SF
0240-241-41	0 43428 SF
0240-241-43	0 3 AC
0240-241-44	0 2.69 AC
0240-241-45	0 1.25 AC
0240-241-46	0 1.25 AC
0240-241-47	0 2.18 AC
0240-241-48	0 2.63 AC
0240-241-49	0 1.25 AC
0240-241-50	0 1.16 AC
0240-241-52	0 8.45 AC
0240-241-53	100089 9.97 AC
0240-241-55	1222 1.025 AC
0240-241-56	0 1.96 AC
0240-251-04	0 10 AC
0240-251-06	0 2.16 AC
0240-251-09	0 20 AC
0240-251-10	0 20 AC
0240-251-11	0 10 AC
0240-251-14	0 18.69 AC
0240-251-21	0 56.7 AC
0240-251-21-W	27900 4 AC
0240-251-22	0 14.69 AC
0240-251-22-W	0
0240-251-22-W-006	0

0240-251-22-W-007	0
0240-251-22-W-008	0
0240-251-22-W-012	0
0240-251-22-W-013	0
0240-251-22-W-019	0
0240-251-22-W-020	0
0240-251-22-W-021	0
0240-251-22-W-022	0
0240-251-22-W-023	0
0240-251-22-W-027	0
0240-251-22-W-028	0
0240-251-23	0
0240-251-24	0 4 AC
0240-251-24-W-002	39560 6 AC
0240-251-25	0 2 AC
0240-251-28	0
0240-251-29	0 22.318 AC
0240-251-30	0 1.08 AC
0240-251-32	0
0240-251-33	0 7.994 AC
0240-251-34	0 12.006 AC
0264-151-45	0 5.48 AC
0264-151-49	0
0264-151-50	0
0264-151-51	0
0264-151-52	0 43.36 AC
0264-151-53	0
0264-151-53-W-002	0
0264-151-53-W-003	0
0264-151-53-W-004	0
0264-151-53-W-005	0
0264-151-53-W-006	0
0264-151-53-W-007	0
0264-151-53-W-008	0
0264-151-53-W-009	0
0264-151-53-W-010	0
0264-151-53-W-011	0
0264-151-53-W-012	1000
0264-151-53-W-013	0
0264-151-53-W-014	0
0264-151-53-W-015	1000
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0264-151-53-W-017	0
0264-151-53-W-018	0
0264-151-53-W-020	0
0264-151-53-W-021	0
0264-151-53-W-022	0
0264-151-53-W-023	0
0264-151-53-W-024	0
0264-151-53-W-025	0
0264-151-53-W-026	0
0264-151-53-W-027	0

0264-151-53-W-028	0
0264-151-53-W-029	0
0264-151-53-W-030	0
0264-151-57	0
0264-151-59	0 7881 SF
0264-151-62	0 4.81 AC
0264-151-65	0
0264-151-66	0 39.302 AC
0264-151-67	0
0264-151-68	0
0264-151-72	0
0264-151-73	0
0264-151-74	0 2.327 AC
0264-151-75	0 4.295 AC
0264-151-76	0 2.233 AC
0264-151-77	0 842 SF
0264-151-78	0 3.885 AC
0264-151-79	0 13863 SF
0264-151-80	0 1749 SF
0264-151-81	0 2.725 AC
0264-211-07	0
0264-211-08	0
0264-211-09	0
0264-211-10	0
0264-211-11	0
0264-211-12	0 2.51 AC
0264-211-15	0 3.39 AC
0264-211-16	57541 3.27 AC
0264-211-17	56408 1.73 AC
0264-211-18	56408 2.87 AC
0264-211-19	86675 3.1 AC
0264-211-20	0
0264-211-21	0
0264-211-22	0 1.348 AC
0264-211-23	0 1.118 AC
0264-211-24	1024 1.878 AC
0264-212-05	0 5.02 AC
0264-212-06	0
0264-212-07	0
0264-212-08	0
0264-212-09	0 4.78 AC
0264-212-12	0 4.95 AC
0264-212-15	102087 4.3 AC
0264-212-17	0 3.08 AC
0264-212-18	0 1.52 AC
0264-212-19	0 1.31 AC
0264-212-20	0 3.38 AC
0264-212-27	0 3.07 AC
0264-212-28	0
0264-212-28-W	27560 3.91 AC
0264-212-29	14210 2.83 AC
0264-212-30	0 16000 SF

0264-212-31	0 1.1 AC
0264-212-32	0 1.31 AC
0264-212-33	11920 1.13 AC
0264-212-34	11920 1.13 AC
0264-212-35	19866 1.1 AC
0264-212-36	19866 1.1 AC
0264-212-37	0
0264-212-38	17500 1.07 AC
0264-212-39	0 1.03 AC
0264-212-40	3641 1.03 AC
0264-212-42	67085 3.08 AC
0264-212-44	0 2 AC
0264-212-45	0
0264-212-46	0 300 SF
0264-212-47	0
0264-212-48	4032 1 AC
0264-212-49	2224 35143 SF
0264-212-50	3056 1.011 AC
0264-212-51	6270 30812 SF
0264-212-52	0 1.619 AC
0264-212-53	2940 1.098 AC
0264-213-16	0
0264-213-17	0
0264-213-18	0 30.87 AC
0264-213-32	0
0264-213-35	0 2.48 AC
0264-213-36	19733 1.1 AC
0264-213-37	11920 1.1 AC
0264-213-38	11920 1.13 AC
0264-213-39	11920 1.13 AC
0264-213-40	11920 1.13 AC
0264-213-41	0 1.14 AC
0264-213-42	0 1.41 AC
0264-213-43	0 1.55 AC
0264-213-44	0 1.59 AC
0264-213-45	0
0264-213-46	0
0264-213-47	0
0264-213-48	0
0264-213-49	0
0264-213-50	0
0264-213-51	0 37987 SF
0264-361-01	10655 1.5 AC
0264-361-02	0 1.22 AC
0264-361-03	0 1.07 AC
0264-361-04	18960 1.08 AC
0264-361-05	0 1 AC
0264-361-06	0 1.03 AC

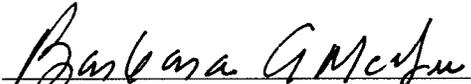
<u>Parcel Number</u>	<u>Square Feet</u>	<u>Lot Size</u>
1119-241-01 Partial		
1119-241-02 Partial		
1119-241-02-W		0
1133-181-05		0 5 AC
1133-181-06		0 5 AC
1133-181-12		0 7.12 AC
1133-181-13	13390	12.16 AC
1133-181-15		0 9.242 AC
1133-181-16		0 13.224 AC
1133-181-17		0 3.302 AC
1133-191-11		0 38368 SF
1133-191-12		0 1.002 AC
1133-191-13		0 1.002 AC
1133-191-14	16000	7.66 AC
1133-191-15		0 43425 SF
1133-191-16		0 43524 SF
1133-191-17		0 43368 SF
1133-191-18		0 43524 SF
1133-191-19		0 43524 SF
1133-191-20		0 43524 SF
1133-191-21		0 43524 SF
1133-191-22		0 1.006 AC
1133-241-04		0 1.96 AC
1133-241-05		0 4.62 AC
1133-241-06	7758	2.38 AC
1133-251-02		0 10086 SF
1133-251-04		0 1.54 AC
1133-251-05		0 3.07 AC
1133-251-06		0 9.2 AC
1133-261-01		0 18.43 AC
1133-271-01		0 6.69 AC
1133-491-01	69404	4.7 AC
1133-491-02		0 4.7 AC
1133-491-03		0 4.549 AC
1133-491-04		0 2.64 AC
1133-491-05	23926	1.825 AC
1133-511-03	198437	4.56 AC
1133-511-04	158754	3.64 AC
1133-521-04	92747	2.129 AC
1133-521-06	162687	3.73 AC
1133-591-01	1172	1.48 AC
1133-591-04	3264	38428 SF
1133-591-09	1500	11500 SF



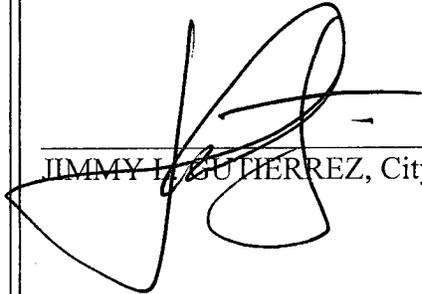


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ATTEST:

  
BARBARA A. McGEE, City Clerk

APPROVED AS TO FORM:

  
JIMMY L. GUTIERREZ, City Attorney

1 STATE OF CALIFORNIA )  
2 COUNTY OF SAN BERNARDINO )ss  
3 CITY OF RIALTO )

4 I, Barbara A. McGee, City Clerk of the City of Rialto, do hereby certify that the foregoing  
5 Ordinance No. 1478 was duly passed and adopted at a regular meeting of the City Council of the  
6 City of Rialto held on the 23rd day of November, 2010.  
7

8 Upon motion of Councilmember Baca Jr., seconded by Councilmember Scott, the foregoing  
9 Ordinance No. 1478 was duly passed and adopted.  
10

11 Vote on the Motion:

12 AYES: Mayor Vargas, Council Members: Baca Jr., Robertson, Scott and Palmer  
13

14 NOES: None  
15

16 ABSTAIN: None  
17

18 ABSENT: None  
19

20 IN WITNESS WHEREOF, I have hereunto set my hand and the Official Seal of the City of  
21 Rialto, this 14th day of December, 2010.  
22

23   
24 BARBARA A. MCGEE, CITY CLERK  
25  
26  
27  
28

# EXHIBIT ‘A’

## Renaissance Specific Plan

## Assessor’s Parcel Numbers

<b>Parcel Number</b>	<b>Square Feet</b>	<b>Lot Size</b>
0240-171-02	0	7.14 AC
0240-171-03	0	3 AC
0240-171-20	0	3.63 AC
0240-171-21	0	1.68 AC
0240-171-27	0	1.82 AC
0240-171-30	0	8500 SF
0240-171-31	881820	39.535 AC
0240-171-32	0	51.06 AC
0240-181-04	0	2.07 AC
0240-181-05	0	2.07 AC
0240-181-08	1170	2.5 AC
0240-181-13	984	31640 SF
0240-181-14	0	33139 SF
0240-181-16	1064	32956 SF
0240-181-17	0	32956 SF
0240-181-18	0	5 AC
0240-181-22	0	5 AC
0240-181-23	3200	42000 SF
0240-181-24	0	39000 SF
0240-181-26	1282	39000 SF
0240-181-27	1077	36000 SF
0240-181-28	1658	14766 SF
0240-181-29	0	17824 SF
0240-181-30	1450	18500 SF
0240-181-32	0	8400 SF
0240-181-33	1350	4.09 AC
0240-181-34	1027	21939 SF
0240-181-35	480	1.55 AC
0240-181-36	2651	7080 SF
0240-181-37	0	1.45 AC
0240-181-48	0	5.95 AC
0240-181-49	543400	28.64 AC
0240-181-50	0	10 AC
0240-191-07	0	1.21 AC
0240-191-08	0	1.55 AC
0240-191-09	0	1.43 AC
0240-191-15	0	4.5 AC
0240-191-16	0	5 AC
0240-191-18	0	1 AC
0240-191-22	0	1 AC
0240-191-23	0	21780 SF
0240-191-25	0	13068 SF
0240-191-26	0	21780 SF
0240-191-27	0	3.3 AC
0240-191-28	0	16500 SF
0240-191-29	0	3.04 AC
0240-191-30	0	5 AC
0240-191-31	0	5 AC
0240-191-32	0	3520 SF
0240-191-33	0	1.14 AC
0240-191-34	0	1.726 AC

0240-191-35	0 2.57 AC
0240-191-38	0 4465 SF
0240-191-41	0 14416 SF
0240-191-43	0 1.014 AC
0240-191-44	0 4869 SF
0240-191-45	0 13548 SF
0240-201-02	0 5 AC
0240-201-04	0 40588 SF
0240-201-05	0 1.907 AC
0240-201-08	0 2.5 AC
0240-201-09	1390 1.2 AC
0240-201-10	600 1.2 AC
0240-201-12	0 1.1 AC
0240-201-13	0 1.1 AC
0240-201-14	0 1.38 AC
0240-201-15	1840 1.2 AC
0240-201-18	1005 1.1 AC
0240-201-20	0 40838 SF
0240-201-21	3944 1.11 AC
0240-201-22	1520 1.1 AC
0240-201-23	0 1.9 AC
0240-201-24	0 5 AC
0240-201-26	0 5 AC
0240-201-28	4048 1.902 AC
0240-201-29	0 4.691 AC
0240-201-30	0 40602 SF
0240-201-31	1704 41520 SF
0240-201-32	0 2 AC
0240-201-34	1056 2.4 AC
0240-201-35	0 2.5 AC
0240-201-36	1036 2.485 AC
0240-201-37	0 2.5 AC
0240-201-38	0 31050 SF
0240-201-40	0 42848 SF
0240-201-41	1588 1.9 AC
0240-201-42	0 1.3 AC
0240-201-43	1718 1.26 AC
0240-201-44	0 1.4 AC
0240-201-45	0 16200 SF
0240-201-46	0 16200 SF
0240-201-47	0 19035 SF
0240-201-48	0 1.143 AC
0240-201-49	3005 1.653 AC
0240-201-55	22131 1.404 AC
0240-201-56 01	0
0240-201-57	16054 38693 SF
0240-211-13	0 1.738 AC
0240-211-14	0 4.468 AC
0240-211-15	0 16.6 AC
0240-211-16	0 8.645 AC
0240-211-17	0 1.88 AC
0240-211-20	0 20460 SF

0240-211-21	0 2.84 AC
0240-211-22	0 5.27 AC
0240-211-23	0 3 AC
0240-211-24	0 5.27 AC
0240-211-25	0 2.03 AC
0240-211-26	0 3.19 AC
0240-211-29	0 13173 SF
0240-211-30	0 22920 SF
0240-211-31	0 12543 SF
0240-211-32	0 22500 SF
0240-211-33	0 4.69 AC
0240-211-34	0 4.17 AC
0240-211-35	0 20911 SF
0240-211-36	0 17837 SF
0240-221-02	0 38674 SF
0240-221-03	0 1.776 AC
0240-221-04	0 1.925 AC
0240-221-05	0 18 AC
0240-221-06	0 4.76 AC
0240-221-07	0 4.76 AC
0240-221-08	0 10 AC
0240-221-09	0 1 AC
0240-221-10	0 8.16 AC
0240-221-12	1632 6.8 AC
0240-221-13	605 1 AC
0240-221-14	0 1 AC
0240-221-15	0 2 AC
0240-221-16	1200 1 AC
0240-221-17	1492 1 AC
0240-221-18	0 1 AC
0240-221-19	0 1 AC
0240-221-21	0 1 AC
0240-221-22	0 16093 SF
0240-221-23	1115 1.6 AC
0240-221-24	0 1.01 AC
0240-221-26	0 38581 SF
0240-221-27	0 1.01 AC
0240-221-28	0 7.09 AC
0240-231-02	0 10 AC
0240-231-06	1900 1.879 AC
0240-231-09	0 10 AC
0240-231-10	1376 1.881 AC
0240-231-11	0 40970 SF
0240-231-13	0 2 AC
0240-231-16	0 21780 SF
0240-231-17	0 19019 SF
0240-231-18	0 8 AC
0240-231-20	0 5.09 AC
0240-231-21	0 4.91 AC
0240-231-22	0 6.72 AC
0240-231-23	0 3.28 AC
0240-231-25	0 1.91 AC

0240-231-27	0
0240-231-28	0 15.25 AC
0240-231-29	0
0240-231-30	2351 1 AC
0240-241-01	0 2 AC
0240-241-02	0 2 AC
0240-241-05	0 1.174 AC
0240-241-06	0 1.1 AC
0240-241-07	0 1.1 AC
0240-241-10	0 4.24 AC
0240-241-14	551 1.41 AC
0240-241-15	0 1.41 AC
0240-241-16	0 1.97 AC
0240-241-17	0 1.879 AC
0240-241-19	0 19200 SF
0240-241-20	1274 1.91 AC
0240-241-21	1452 40933 SF
0240-241-22	2007 40933 SF
0240-241-23	650 42112 SF
0240-241-24	1020 42112 SF
0240-241-29	0 1.25 AC
0240-241-30	1547 43428 SF
0240-241-32	0 40920 SF
0240-241-33	0 31668 SF
0240-241-34	0 43428 SF
0240-241-37	0 1.837 AC
0240-241-38	0 4.76 AC
0240-241-40	1206 43428 SF
0240-241-41	0 43428 SF
0240-241-43	0 3 AC
0240-241-44	0 2.69 AC
0240-241-45	0 1.25 AC
0240-241-46	0 1.25 AC
0240-241-47	0 2.18 AC
0240-241-48	0 2.63 AC
0240-241-49	0 1.25 AC
0240-241-50	0 1.16 AC
0240-241-52	0 8.45 AC
0240-241-53	100089 9.97 AC
0240-241-55	1222 1.025 AC
0240-241-56	0 1.96 AC
0240-251-04	0 10 AC
0240-251-06	0 2.16 AC
0240-251-09	0 20 AC
0240-251-10	0 20 AC
0240-251-11	0 10 AC
0240-251-14	0 18.69 AC
0240-251-21	0 56.7 AC
0240-251-21-W	27900 4 AC
0240-251-22	0 14.69 AC
0240-251-22-W	0
0240-251-22-W-006	0

0240-251-22-W-007	0
0240-251-22-W-008	0
0240-251-22-W-012	0
0240-251-22-W-013	0
0240-251-22-W-019	0
0240-251-22-W-020	0
0240-251-22-W-021	0
0240-251-22-W-022	0
0240-251-22-W-023	0
0240-251-22-W-027	0
0240-251-22-W-028	0
0240-251-23	0
0240-251-24	0 4 AC
0240-251-24-W-002	39560 6 AC
0240-251-25	0 2 AC
0240-251-28	0
0240-251-29	0 22.318 AC
0240-251-30	0 1.08 AC
0240-251-32	0
0240-251-33	0 7.994 AC
0240-251-34	0 12.006 AC
0264-151-45	0 5.48 AC
0264-151-49	0
0264-151-50	0
0264-151-51	0
0264-151-52	0 43.36 AC
0264-151-53	0
0264-151-53-W-002	0
0264-151-53-W-003	0
0264-151-53-W-004	0
0264-151-53-W-005	0
0264-151-53-W-006	0
0264-151-53-W-007	0
0264-151-53-W-008	0
0264-151-53-W-009	0
0264-151-53-W-010	0
0264-151-53-W-011	0
0264-151-53-W-012	1000
0264-151-53-W-013	0
0264-151-53-W-014	0
0264-151-53-W-015	1000
0264-151-53-W-016	0
0264-151-53-W-017	0
0264-151-53-W-018	0
0264-151-53-W-020	0
0264-151-53-W-021	0
0264-151-53-W-022	0
0264-151-53-W-023	0
0264-151-53-W-024	0
0264-151-53-W-025	0
0264-151-53-W-026	0
0264-151-53-W-027	0

0264-151-53-W-028	0
0264-151-53-W-029	0
0264-151-53-W-030	0
0264-151-57	0
0264-151-59	0 7881 SF
0264-151-62	0 4.81 AC
0264-151-65	0
0264-151-66	0 39.302 AC
0264-151-67	0
0264-151-68	0
0264-151-72	0
0264-151-73	0
0264-151-74	0 2.327 AC
0264-151-75	0 4.295 AC
0264-151-76	0 2.233 AC
0264-151-77	0 842 SF
0264-151-78	0 3.885 AC
0264-151-79	0 13863 SF
0264-151-80	0 1749 SF
0264-151-81	0 2.725 AC
0264-211-07	0
0264-211-08	0
0264-211-09	0
0264-211-10	0
0264-211-11	0
0264-211-12	0 2.51 AC
0264-211-15	0 3.39 AC
0264-211-16	57541 3.27 AC
0264-211-17	56408 1.73 AC
0264-211-18	56408 2.87 AC
0264-211-19	86675 3.1 AC
0264-211-20	0
0264-211-21	0
0264-211-22	0 1.348 AC
0264-211-23	0 1.118 AC
0264-211-24	1024 1.878 AC
0264-212-05	0 5.02 AC
0264-212-06	0
0264-212-07	0
0264-212-08	0
0264-212-09	0 4.78 AC
0264-212-12	0 4.95 AC
0264-212-15	102087 4.3 AC
0264-212-17	0 3.08 AC
0264-212-18	0 1.52 AC
0264-212-19	0 1.31 AC
0264-212-20	0 3.38 AC
0264-212-27	0 3.07 AC
0264-212-28	0
0264-212-28-W	27560 3.91 AC
0264-212-29	14210 2.83 AC
0264-212-30	0 16000 SF

0264-212-31	0 1.1 AC
0264-212-32	0 1.31 AC
0264-212-33	11920 1.13 AC
0264-212-34	11920 1.13 AC
0264-212-35	19866 1.1 AC
0264-212-36	19866 1.1 AC
0264-212-37	0
0264-212-38	17500 1.07 AC
0264-212-39	0 1.03 AC
0264-212-40	3641 1.03 AC
0264-212-42	67085 3.08 AC
0264-212-44	0 2 AC
0264-212-45	0
0264-212-46	0 300 SF
0264-212-47	0
0264-212-48	4032 1 AC
0264-212-49	2224 35143 SF
0264-212-50	3056 1.011 AC
0264-212-51	6270 30812 SF
0264-212-52	0 1.619 AC
0264-212-53	2940 1.098 AC
0264-213-16	0
0264-213-17	0
0264-213-18	0 30.87 AC
0264-213-32	0
0264-213-35	0 2.48 AC
0264-213-36	19733 1.1 AC
0264-213-37	11920 1.1 AC
0264-213-38	11920 1.13 AC
0264-213-39	11920 1.13 AC
0264-213-40	11920 1.13 AC
0264-213-41	0 1.14 AC
0264-213-42	0 1.41 AC
0264-213-43	0 1.55 AC
0264-213-44	0 1.59 AC
0264-213-45	0
0264-213-46	0
0264-213-47	0
0264-213-48	0
0264-213-49	0
0264-213-50	0
0264-213-51	0 37987 SF
0264-361-01	10655 1.5 AC
0264-361-02	0 1.22 AC
0264-361-03	0 1.07 AC
0264-361-04	18960 1.08 AC
0264-361-05	0 1 AC
0264-361-06	0 1.03 AC

<u>Parcel Number</u>	<u>Square Feet</u>	<u>Lot Size</u>
1119-241-01 Partial		
1119-241-02 Partial		
1119-241-02-W		0
1133-181-05		0 5 AC
1133-181-06		0 5 AC
1133-181-12		0 7.12 AC
1133-181-13	13390	12.16 AC
1133-181-15		0 9.242 AC
1133-181-16		0 13.224 AC
1133-181-17		0 3.302 AC
1133-191-11		0 38368 SF
1133-191-12		0 1.002 AC
1133-191-13		0 1.002 AC
1133-191-14	16000	7.66 AC
1133-191-15		0 43425 SF
1133-191-16		0 43524 SF
1133-191-17		0 43368 SF
1133-191-18		0 43524 SF
1133-191-19		0 43524 SF
1133-191-20		0 43524 SF
1133-191-21		0 43524 SF
1133-191-22		0 1.006 AC
1133-241-04		0 1.96 AC
1133-241-05		0 4.62 AC
1133-241-06	7758	2.38 AC
1133-251-02		0 10086 SF
1133-251-04		0 1.54 AC
1133-251-05		0 3.07 AC
1133-251-06		0 9.2 AC
1133-261-01		0 18.43 AC
1133-271-01		0 6.69 AC
1133-491-01	69404	4.7 AC
1133-491-02		0 4.7 AC
1133-491-03		0 4.549 AC
1133-491-04		0 2.64 AC
1133-491-05	23926	1.825 AC
1133-511-03	198437	4.56 AC
1133-511-04	158754	3.64 AC
1133-521-04	92747	2.129 AC
1133-521-06	162687	3.73 AC
1133-591-01	1172	1.48 AC
1133-591-04	3264	38428 SF
1133-591-09	1500	11500 SF

# Appendices